

**LOCATION CHANGE – The meeting will be held at the Brighton Center of Performing Arts at the Brighton High School located at 7878 Brighton Road, Brighton**

**GENOA CHARTER TOWNSHIP  
PLANNING COMMISSION PUBLIC HEARING  
SEPTEMBER 17, 2024  
TUESDAY  
6:30 P.M.  
AGENDA**

**CALL TO ORDER:**

**PLEDGE OF ALLEGIANCE:**

**APPROVAL OF AGENDA:**

**DECLARATION OF CONFLICT OF INTEREST:**

**CALL TO THE PUBLIC: (Note: The Board reserves the right to not begin new business after 10:00 p.m.)**

**OPEN PUBLIC HEARING #1...** Consideration of an amendment to the Latson Road Innovation Interchange PUD Agreement, Environmental Impact Assessment and conceptual PUD Plan. The PUD amendment includes amendments to the list of uses, PUD design guidelines and the conceptual PUD plan. The parcels included in the request are as follows: 1882 Latson Road (4711-08-400-020), 1896 Latson Road (4711-08-400-006), 1908 Latson Road (4711-08-400-004), 3799 Clover Bend Ct. (4711-08-400-012), 3796 Clover Bend Ct, (4711-08-400-013), 3854 Clover Bend Ct. (4711-08-400-014), 3912 Clover Bend Ct. (4711-08-400-015) and vacant parcel #4711-17-200-008 which are located south of the CSX Rail Line on the west side of Latson Road. Also includes 1895 Latson Road (4711-09-300-031) located east of Latson Road and south of the CSX Rail Line and vacant parcel #4711-09-300-044 located on the north side of Beck Road, south of expressway ramp. The request also includes the removal of parcel 4711-09-300-040 (5.74 acres) that is currently zoned ICPUD and is proposed to be removed from the existing Innovation Interchange PUD and incorporated into the proposed Latson Road/I-96 Interchange Commercial PUD . The request is petitioned by Todd Wyett.

- A. Recommendation of PUD Agreement Amendment
- B. Recommendation of Environmental Impact Assessment
- C. Recommendation of amended Conceptual Plan

**OPEN PUBLIC HEARING #2...** Consideration of a rezoning application, PUD Agreement, Environmental Impact Assessment and conceptual PUD Plan requesting approval for the Latson Road/I-96 Interchange Commercial PUD. The proposed development involves rezoning parcel 4711-09-300-046 (7.44 acres) from CE to ICPUD. Parcel 4711-09-300-040 (5.74 acres) is currently zoned ICPUD and is proposed to be removed from the existing Innovation Interchange/Latson Road PUD and incorporated into the proposed Latson Road/I-96 Interchange Commercial PUD with parcel 4711-09-300-046. The parcels are located east of Latson Road, between Beck Road and the CSX rail line. The request is petitioned by Todd Wyett.

- A. Recommendation of Rezoning
- B. Recommendation of Environmental Impact Assessment
- C. Recommendation of PUD Agreement
- D. Recommendation of Conceptual PUD Plan

**ADMINISTRATIVE BUSINESS:**

- Adjournment

\*Citizen's Comments- In addition to providing the public with an opportunity to address the Township Board at the beginning of the meeting, opportunity to comment on individual agenda items may be offered by the Chairman as they are presented. Anyone speaking on an agenda item will be limited to 2 minutes.



Application to Amend Existing PUD  
GENOA CHARTER TOWNSHIP APPLICATION  
Planned Unit Development (PUD)

APPLICANT NAME: Todd Wyett

APPLICANT EMAIL: todd@versacos.com

APPLICANT ADDRESS & PHONE: 29201 Telegraph Rd, Ste 410, Southfield, MI 48034 , ( 248 ) 771-8484

OWNER'S NAME: Todd Wyett

OWNER ADDRESS & PHONE: 29201 Telegraph Rd, Ste 410, Southfield, MI 48034 , ( 248 ) 771-8484

TAX CODE(S): 11-09-300-031, See PUD for full list of parcels

**QUALIFYING CONDITIONS (To be filled out by applicant)**

- 1. A PUD zoning classification may be initiated only by a petition.
- 2. It is desired and requested that the foregoing property be rezoned to the following type of PUD designation:

- Residential Planned Unit Development (RPUD)
- Planned Industrial District (PID)
- Mixed Use Planned Unit Development (MUPUD)
- Redevelopment Planned Unit Development (RDPUD)
- Non-residential Planned Unit Development (NRPUD)
- Town Center Planned Unit Development (TCPUD)
- CAPUD
- ICPUD
- Existing PUD Amendment

- 3. The planned unit development site shall be under the control of one owner or group of owners and shall be capable of being planned and developed as one integral unit.

EXPLAIN The property is currently under single ownership via three separate entities:

Latson Partners LLC, Latson Farms LLC, and Covenant Faith LLC who's address is

29201 Telegraph Rd, Ste 410, Southfield, MI 48034

- 4. The site shall have a minimum area of twenty (20) acres of contiguous land, provided such minimum may be reduced by the Township Board as follows:
  - A. The minimum area requirement may be reduced to five (5) acres for sites served by both public water and public sewer.
  - B. The minimum lot area may be waived for sites zoned for commercial use (NSD, GCD or RCD) where the site is occupied by a nonconforming commercial, office or industrial building, all buildings on such site are proposed to be removed and a new use permitted within the underlying zoning district is to be established. The Township Board shall only permit the PUD on the smaller site where it finds that the flexibility in dimensional standards is necessary to allow for innovative design in redeveloping the site and an existing blighted situation will be eliminated. A parallel plan shall be provided showing how the site could be redeveloped without the use of the PUD to allow the Planning Commission to evaluate whether the modifications to dimensional standards are the

minimum necessary to allow redevelopment of the site, while still meeting the spirit and intent of the ordinance.

- C. The PUD site plan shall provide one or more of the following benefits not possible under the standards of another zoning district, as determined by the Planning Commission:
  - preservation of significant natural or historic features
  - a complementary mixture of uses or a variety of housing types
  - common open space for passive or active recreational use
  - mitigation to offset impacts
  - redevelopment of a nonconforming site where creative design can address unique site constraints.
  
- D. The site shall be served by public sewer and water. The Township may approve a residential PUD that is not served by public sewer or water, provided all lots shall be at least one (1) acre in area and the requirements of the County Health Department shall be met.

Size of property is +/- 193 \_\_\_\_\_ acres.

DESCRIBE BELOW HOW THE REQUESTED PUD DESIGNATION COMPLIES WITH AFOREMENTIONED MINIMUM LOT SIZE REQUIREMENTS.

The total project area exceeds the minimum 20 acre requirement.

**STANDARDS FOR REZONING TO PLANNED UNIT DEVELOPMENT (RESPOND HERE OR WITHIN THE IMPACT STATEMENT)**

- 1. How would the PUD be consistent with the goals, policies and future land use map of the Genoa Township Master Plan, including any subarea or corridor studies. If conditions have changed since the Master Plan was adopted, the consistency with recent development trends in the area;

The proposed amendment removes several commercial uses from the parcel east of Latson and south of the rail (Mixed Use Area) and now provides for similar uses as the development west of Latson with a smaller maximum size per building

Describe how this is consistent with the Master Plan

- 2. The compatibility of all the potential uses in the PUD with surrounding uses and zoning in terms of land suitability, impacts on the environment, density, nature of use, traffic impacts, aesthetics, infrastructure and potential influence on property values;

The proposed amendment removes several commercial uses from the parcel east of Latson and south of the rail (Mixed Use Area) and now provides for similar uses as the development west of Latson with a smaller maximum size per building

Please revise the response to address the compatibility of the uses as indicated in the heading.

- 3. The capacity of infrastructure and services sufficient to accommodate the uses permitted in the requested district without compromising the “health, safety and welfare” of the Township;

The development team has worked closely with the Township, MHOG and County to fund the design and construction of water and sewer utility extensions to serve the area. The capacity of the public utility system to serve development in this area has been studied and planned for.

Address infrastructure and services other than water and sewer.

Recent improvements to the waste water treatment facility have also been performed to accomodate development of the area.

4. The apparent demand for the types of uses permitted in the PUD;  
Given the newly constructed interchange on I-96, which is a highly traveled express way  
transportation corridor, and proximity to Ann Arbor, Lansing and metro Detroit  
market, there is significant demand for the uses proposed.

There should be evidence of demand. The interchange has been in place for over 10 years and there have been no requests for development.

**AFFIDAVIT**

The undersigned says that they are the owner (owner, lessee, or other specified interest) involved in this petition and that the foregoing answers and statements herein contained and the information herewith submitted are in all respects true and correct to the best of his/her knowledge and belief.

BY: \_\_\_\_\_

ADDRESS: 29201 Telegraph Rd, Ste 410, Southfield, MI 48034

*Contact Information - Review Letters and Correspondence shall be forwarded to the following:*  
Todd Wyett of Versa Real Estate at todd@versacos.com  
*Name Business Affiliation E-mail*



**FEE EXCEEDANCE AGREEMENT**  
As stated on the site plan review fee schedule, all site plans are allocated two (2) consultant reviews and one (1) Planning Commission meeting. If additional reviews or meetings are necessary, the applicant will be required to pay the actual incurred costs for the additional reviews. If applicable, additional review fee payment will be required concurrent with submittal to the Township Board. By signing below, applicant indicates agreement and full understanding of this policy.  
PROJECT NAME: Innovation Park  
PROJECT LOCATON & DESCRIPTION: Latson Road south of I-96  
SIGNATURE:  DATE: 2/28/2024  
PRINT NAME: Todd Wyett PHONE: (248) 770-8484  
COMPANY NAME & ADDRESS: Versa Real Estate, 29201 Telegraph Rd, Ste 410, Southfield, MI 48034

**GENOA CHARTER TOWNSHIP BOARD**  
**Regular Meeting & Public Hearing**  
**August 3, 2020**

**MINUTES**

Supervisor Rogers called the Regular Meeting of the Genoa Charter Township Board to order at 6:30 p.m. at the Township Hall with the Pledge of Allegiance. The following members were present constituting a quorum for the transaction of business: Bill Rogers, Paulette Skolarus, Jean Ledford, Terry Croft, Jim Mortensen and Diana Lowe. Also present were Township Manager Michael Archinal; Township Assistant Manager Kelly VanMarter and six persons in the audience. Absent - Robin Hunt.

A Call to the Public was made with the following response:

- Ty Cole – I just want to clarify my intent from the last board meeting that may have caused some concern or upset. I would like to achieve a collaborative relationship with the township and work through some issues related to lake traffic and violations on the lake.
- Philip Casteleyn – The speed limit on Hughes Road is 25 M.P.H. and many exceed that limit. I have put out speed trailers, 16 crosswalk signs, and contacted the Livingston County Sheriff and now ask for your assistance.

**Approval of Consent Agenda:**

Moved by Mortensen and supported by Lowe to approve all items listed under the Consent Agenda as requested. The motion carried unanimously.

**1. Payment of Bills.**

**2. Request to Approve Minutes: July 20, 2020**

**Approval of Regular Agenda:**

Moved by Ledford and supported by Lowe to approve for action all items listed under the Regular Agenda. The motion carried unanimously.

**3. Consideration of a recommendation for approval of a rezoning (adoption of Ordinance Z-20-03), PUD Agreement, Impact Assessment, and conceptual PUD Plan for a proposed rezoning request from Country Estates (CE) to Interchange Campus Planned Unit Development (CAPUD) and Interchange Commercial Planned Unit Development (ICPUD) for approximately 195 acres along S. Latson Road south of I-96. The subject property includes 177 acres on the west side of S. Latson Road, 10 acres on the east side of S. Latson**

**Road and 6 acres on Beck Road east of S. Latson Road. The properties include the following parcels requested to be rezoned to CAPUD: 4711-08-400-004, 006, 012, 013, 014, 015, 020, 4711-09-300-031 and 4711-17-200-008. Parcel 4711-09-300-040 (formerly 001) is requested to be rezoned to ICPUD. The request is petitioned by Todd Wyett.**

**A. Call the Public was made with the following response:** Brenda Daniels – I have lived in this community for 20 years. We need a larger buffer between our property and the proposed development. Please address the building height, signage and lighting. Please also consider an easement on our property to allow access to the new traffic light for future development.

**B. Disposition of Rezoning Ordinance Z-20-03**

Moved by Skolarus and supported by Lowe to approve and adopt Ordinance No. Z-20-03. This approval is made because the proposed amendment to the Zoning Map and reclassification as Interchange Campus and Interchange Commercial Planned Unit Development (CAPUD and ICPUD) with the related development agreement including use restrictions, design guidelines, utility extensions and conceptual plan has been found to comply with the criteria stated in Sections 10.02, 10.03.06 and 22.04 of the Township Zoning Ordinance. This finding includes the following supporting statements:

1. The rezoning promotes comprehensive and long term planning of appropriate land uses, innovative architectural design, high quality building materials, and a walkable environment for pedestrians;
2. The rezoning encourages innovative and beneficial land uses with streetscape, building and site design elements which are consistent with the goals, objectives, and land use map of the master plan and are compatible with surrounding uses and zoning in terms of land suitability, impacts on the environment, density, nature of use, traffic impacts, aesthetics, infrastructure and potential influence on property values;
3. The rezoning is compatible with the site's physical, geological, hydrological and other environmental features with the host of uses permitted in the proposed zoning district and will serve to protect the large wooded wetland located west of S. Latson Road;
4. The rezoning will provide the required utility extensions necessary to serve the proposed development and will further promote efficient provision of public services and utilities without compromising the "health, safety and welfare" of the Township;
5. Safe, convenient, uncongested, and well-defined vehicular and pedestrian circulation within and to the site is provided. The development provides for inter-connection of roads and the future integration of circulation between adjacent sites which will reduce adverse vehicular and pedestrian traffic impacts.

The motion carried by roll call vote as follows: Ayes – Ledford, Croft, Lowe, Mortensen, Skolarus and Rogers. Nays – None. Absent – Hunt.

**C. Disposition of PUD Agreement last updated on July 20, 2020.**

Mortenson raised the issue of \$10,000.00 for an easement acquisition with the Township being responsible for additional costs. VanMarter advised the board that there is only one easement left to get for the water-main. The sewer easement is already complete.

Moved by Lowe and supported by Croft to approve the PUD Agreement received on July 20, 2020 subject to the following:

1. The comments from staff and the Township Attorney in the marked up Agreement on 7/29/20 shall be incorporated with the exception of changes to Section 20 related to Timing of Development which shall be further negotiated and approved by Township staff and the Township Attorney prior to signing.
2. The comments from staff and the Township Attorney on the marked up Utility Construction Agreement (Exhibit 12) referenced in PUD Agreement shall be incorporated into the document. Any changes shall be reviewed and approved by the Township Attorney and staff.
3. The PUD Agreement final draft with all Exhibits shall be reviewed and approved by Township staff and Township Attorney prior to signing.
4. The fully executed document including all Exhibits shall be recorded at the Livingston County Register of Deeds office and a copy of the recorded document shall be provided to the Township.

The motion carried unanimously.

**D. Disposition of Environmental Impact Assessment dated July 30, 2019.**

Moved by Lowe and supported by Croft to approve the Environmental Impact Assessment as submitted. The motion carried unanimously.

**E. Disposition of Conceptual PUD Plan dated May 20, 2020**

Moved by Skolarus and supported by Ledford to approve the Conceptual PUD Plan dated May 20, 2020 subject to the following:

1. The requirements of the Township Engineer's letter dated June 3, 2020 shall be met.
2. The requirements of the Brighton Area Fire Authority's letter dated March 26, 2020 shall be met
3. Easements will be required to allow cross access for vehicular and pedestrian traffic in each of the project areas and to adjacent parcels.
4. Details will be required for the highway sign, uses, dimensional standards, building and site design, etc. prior to development of the north area.

The motion carried unanimously.

**Correspondence:**

- The Cromaine District Library provided minutes of their board meeting
- The Livingston County sheriff's Office provided an overview of work being done within the township

**Member Discussion:**

- Mortensen asked that the sheriff speak to the board about their responses to possible catastrophic issues.
- Archinal provided an overview of work being done within the township.

Moved by Mortensen and supported by Croft to adjourn the meeting at 7:15 p.m.

Paulette A. Skolarus, Clerk  
Genoa Charter Township Board



This Meeting was Conducted Via Zoom Meeting

**GENOA CHARTER TOWNSHIP  
PLANNING COMMISSION  
PUBLIC HEARING  
JUNE 11, 2020  
6:30 P.M.  
MINUTES**

CALL TO ORDER: Chairman Grajek called the meeting of the Genoa Charter Township Planning Commission to order at 6:33 p.m. Present were Marianne McCreary, Chris Grajek, Eric Rauch, Jim Mortensen, Jeff Dhaenens, Jill Rickard and Glynis McBain. Also present was Kelly VanMarter, Community Development Director/Assistant Township Manager, Joseph Seward, Township Attorney, Shelby Scherdt and Gary Markstrom of Tetra Tech, and Brian Borden of Safebuilt Studio.

PLEDGE OF ALLEGIANCE: The pledge of allegiance was recited.

Chairman Grajek reviewed the process for this evening's Planning Commission meeting and how public comment can be given via Zoom Meeting.

APPROVAL OF AGENDA:

**Moved** by Commissioner Dhaenens, seconded by Commissioner Mortensen, to approve the agenda as presented. **The motion carried unanimously with a roll call vote.**

CALL TO THE PUBLIC: The call to the public was made at 6:38 pm with no response.

CONFLICT OF INTEREST

There were no members with a conflict of interest this evening.

OPEN PUBLIC HEARING #1... Review of a request of a rezoning application, Planned Unit Development application, PUD agreement, impact assessment and conceptual PUD plan. The rezoning request is from Country Estates (CE) to Interchange Campus Planned Unit Development (CAPUD) and Interchange Commercial Planned Unit Development (ICPUD) for approximately 195 acres along S. Latson Road south of I-96. The subject property includes 177 acres on the west side of S. Latson Road, 10 acres on the east side of S. Latson Road and 6 acres on Beck Road east of S. Latson Road. The properties include the following parcels requested to be rezoned to CAPUD: 4711-08-400-004, 006, 012, 013, 014, 015, 020, 4711-09-300-031 and 4711-17-200-008. Parcel 4711-09-300-001 is requested to be rezoned to ICPUD. The request is petitioned by Todd Wyett.

- A. Recommendation of Rezoning and PUD Application
- B. Recommendation of PUD Agreement
- C. Recommendation of Impact Assessment (6-19-19)
- D. Recommendation of Conceptual PUD Plan (5-20-20)

Mr. Todd Wyett, the property owner, Eric Lord, the engineer, Alan Greene, the developer's attorney, and Brad Strader, the landscape architect, were present.

Mr. Strader showed a map of the parcels proposed to be rezoned and what uses are anticipated to be developed on the sites. A concept plan video was shown.

He provided a review of the details of their plan, which included the open space concept, the Sweet Road intersection, commercial area layout, hotel setback study, permitted and prohibited uses, detailed design and architectural guidelines, and two separate options for the Latson Road design, one of which would include a boulevard, but narrower than the one that was originally proposed. He showed a site-line study of their proposed hotel and how what they are proposing would be compatible with the area.

Mr. Eric Lord reviewed the Impact Assessment. They focused on the impacts of the development on the topography and natural features of the site, the public utilities, and the traffic. He provided details of the impact of their development on these three areas and how they plan to address and ease these impacts. He noted that this project will take many years to complete so all of these improvements will be done when they are needed based on what is developed and at what time.

Mr. Alan Greene started by noting that the Township's Master Plan was updated to include this type of development in this area after the Latson Road / I-96 Interchange was installed. This was not planned to be residential neighborhoods. He reviewed the PUD Agreement stating that all of the details of the plan are included in the PUD. It is a legal and binding document that runs with the land, regardless of who owns or develops the property. The applicant has worked with Township staff and the Township attorney on the PUD and he believes it is mostly complete. There is one portion that needs to be addressed, which is regarding the construction of the utilities. The developer is going to finance all of the water and sewer upgrades, but it needs to be determined if they will reimburse the Township for the upgrades or pay up front to have them installed.

Mr. Borden reviewed his letter dated June 3, 2020.

- The ordinance standards for the PUD are generally met, though utility extensions will be required as part of this project.
- The proposed zoning designations are consistent with the Master Plan and Future Land Use Map. He believes the rezoning is appropriate and necessary to implement the vision and goals of the I-96/Latson Road Subarea Plan. It cannot be accomplished under the current Country Estates Zoning.
- The applicant seeks deviations from the conventional use requirements, dimensional standards, lighting intensity, and building material standards of the Zoning Ordinance. He provided details of what deviations the applicant is proposing.
- Proposed building heights and internal setbacks are subject to approval by the Planning Commission.
- Easements are required to allow cross-access for vehicular and pedestrian traffic in each of the project areas.
- Aside from the highway sign, details (uses, dimensional standards, building and site design, etc.) will be needed prior to future development in the North Area.

Mr. Markstrom reviewed his letter dated June 3, 2020.

They do not have any objections to the Impact Assessment that has been developed from an engineering perspective. The site plan provided is very conceptual and all future developments within the PUD will need to have their own site plan review and approval.

The Impact Assessment notes that the PUD will be serviced by water and sewer services through MHOG and GO-SWATH. The extension of water and sanitary sewer to the south side of the CSX railroad is accurately described in the Impact Assessment and corresponds with the plans that have been created for the Township by Tetra Tech. Furthermore, the conceptual plan for the PUD is consistent with the assumptions made on the basis of design for the South Latson Road Water and Sewer improvements design.

The installation of a sanitary pump station will eventually be necessary to provide sanitary service to the PUD. The petitioner added language in the PUD Agreement to note that landscaping and existing trees will be used to screen the pump station site and that building materials may consist of block, metal siding, or other materials used on the nearby research and industrial structures. The Agreement also notes that all building and landscaping plans will be submitted to the Township for review and approval, and he finds these changes acceptable.

The Impact Assessment states that a storm water management plan will be prepared for the entire development. The master plan will have central detention facilities. The detention sizing should be determined based on the entire site to ensure that there will be proper storm management as the property develops rather than developing individual storm water management plans for each new building. The site naturally drains to the Marion Genoa Drain, which is maintained and operated by Livingston County. The Livingston County Drain Commissioner's office will need to be included in the storm water master plan development process.

The developer has prepared a traffic impact study and a traffic improvement timing analysis that have been provided in this submittal. The general layout of the on-site roadways and intersections with Latson Road appear to be well thought out and provide for circulation through the site. The final layout may vary from this concept once end users of the sites are determined.

Improvements to Latson Road are subject to Livingston County Road Commission approval and should be submitted for review and comment by the Township. Since this parcel is the first major development on the south side of Latson Road and, as such, is the gateway to Genoa Township, he recommends additional concepts be considered to promote the Township with either monument signage or landscaping details as part of the overall development plan.

Chairman Grajek asked the applicant if they have seen the Brighton Area Fire Authority's review letter. Mr. Lord stated they have reviewed the letter and will comply with their requirements.

Commissioner Mortensen questioned the change in The PUD regarding how the improvements to the water and sewer systems will be done. It was noted that different plans were discussed between the Township and the developer and it was decided that the developer would solely finance and manage the construction of the utilities. The details of this plan are being negotiated and will be available by the time this item is presented to the Township Board. Commissioner Rickard agrees as it is common for developers to handle the upgrades and installation of water and sewer. Ms. VanMarter noted that the Township's Utilities Director, Greg Tatara, is in support of this plan as well.

Commissioner Rickard asked the petitioner why they are not able to comply with the ordinance with regard to the lighting. She would like to stay within the ordinance with regard to the pole height and the brightness. Mr. Strader stated they will have the lower pole height and “night sky” provisions when the lights are next to the residential neighborhood. He noted their proposal is consistent with other communities who have updated their lighting ordinances. If they met the Township Ordinance, they would require more poles. He also suggested that this be addressed at each site plan review process. Commissioner Rickard would like to see a proposal where the ordinance is met. Mr. Borden stated that the pole heights meet the requirements on the east side, but they are asking for a deviation from the ordinance on the height on the west side, but they have proposed smaller poles close to the existing residential areas.

Commissioner Mortensen asked the Township Attorney to review the three changes he proposed in the recent version of the PUD. Mr. Seward reviewed the reasons for his proposed changes. The Commissioners, the petitioner’s attorney, Ms. VanMarter, and Mr. Seward discussed the items. All Commissioners agreed to have the attorneys and staff determine the best language to address these issues.

Commissioner Dhaenens asked the applicant to provide details of how the residential neighborhood in the middle of this development will be protected. Mr. Strader advised they will meet the ordinance standards when industrial developments abut residential properties.

Commissioner McBain wants to ensure that all of the plants and green areas will be maintained after the development is complete. Mr. Greene stated this maintenance agreement is put in the PUD Agreement and all of the users will pay towards the maintenance of the common areas. Mr. Seward stated the maintenance of the landscaping is not defined in this PUD Agreement. Commissioner McBain would like it to be included. Mr. Borden advised that there is a section of the ordinance that requires the owners to maintain the site after it is developed; however, it can also be included in the PUD Agreement. The petitioner agreed that they will have the maintenance obligation for plantings in the right of way, but they do not want to maintain the sidewalk that is being required to be installed by the Township. Commissioner Rickard would like the developer to include the maintenance of the sidewalk in the Agreement as well. All Commissioners agreed to have the attorneys and staff to discuss and determine the best way to address the maintenance of the sidewalk.

The call to the public was made at 8:38 pm.

Ms. VanMarter stated she received an email from Alan Rankin of 3876 Clover Bend Court. He is concerned with the format of tonight’s meeting, his home being placed in the middle of a commercial area, the credibility of the developer, and his property values decreasing. He asked the Planning Commission to delay making a decision until an in-person meeting can be held.

Ms. Brenda Daniels of 1947 S. Latson Road stated that the property adjacent to -031 was not mentioned this evening. There are four properties that are not in the subject area. How are these properties going to fit into the plan? How are these residential properties going to be accommodated? She has not been approached about having her property purchased and included in this plan. Mr. Wyett advised Ms. Daniels that she could contact him as he may be interested in purchasing her property. She wants to understand what is being developed, how it will impact them, and when the development will occur. Mr. Wyett advised Ms. Daniels that the future land use map shows her property as Interchange Campus so it will increase the value of her property.

Mr. Roy Hibbs of 20919 Greenbriar Circle, South Lyon is concerned with the situation around his brother- and sister-in-law's house, who are Mr. and Mrs. Rankin of 3875 Clover Bend Court. They have their dream home in a Country Estates zoned property and will now be surrounded on three sides by light industrial uses. He knows that development happens but it doesn't make any sense that they were not made an offer to have their property purchased by the developer.

Chairman Grajek asked if Mr. Rankin was ever able to attend any meetings during the development of the I-96 Interchange and the changes in land use were discussed. This was all part of this process. Mr. Hibbs advised Mr. Rankin is with him, but he was never made aware of any meetings until this meeting this evening.

Ms. VanMarter recalls discussions with Mr. Rankin during the Master Plan process and he spoke out in opposition to the change in Campus. She noted that the interchange has been planned since the late 1990's and was in former versions of the Master Plan. Initially commercial development was supposed to stop at the railroad tracks, and when the amendment was proposed, many residents in the area were opposed to converting it to something other than Country Estates zoning.

Mr. Alan Rankin of 3875 Clover Bend Ct. is very upset because Mr. Wyett's contractor tore up Clover Bend Ct. and never repaired it. Mr. Wyett never contacted him about purchasing his property as part of this development and now he will be surrounded by industrial on three sides. He does not want to live with the construction for ten years.

Chairman Grajek advised Mr. Rankin that this did not happen overnight and Mr. Rankin lived in the area during the time the Master Plan was being revised. Mr. Rankin said he never knew about the industrial zoning until he received the letter regarding tonight's meeting. Chairman Grajek understands Mr. Rankin's concerns, but the Township staff conducted many public meetings advising the planned changes. These meeting notices are always published in the newspaper and on WHMI.

Mr. Rankin is asking the Township for consideration and help with his and his neighbors' property. He doesn't believe that he will be able to even sell his home now.

Chairman Grajek called for a five minute break at 9:10 pm to allow members of the public to call in to speak to the Commission.

The meeting resumed at 9:15 pm.

The call to the public was closed at 9:16 pm.

Commissioner Dhaenens asked the Commissioners and staff if they have reviewed the use table provided by the petitioner. Mr. Borden stated the list has been updated after the previous joint meetings held between the Planning Commission and the Township Board and has been vetted thoroughly.

**Moved** by Commissioner Dhaenens, seconded by Commissioner Rickard, to recommend to the Township Board approval of the Rezoning and PUD Application from County Estates (CE) to Interchange Campus Planned Unit Development (CAPUD) and Interchange Commercial Planned Unit Development (ICPUD) for approximately 175 acres. The properties include the following parcels requested to be rezoned to CAPUD: 4711-08-400-004, 006, 012, 013, 014,

015, 020, 4711-09-300-031, 4711-17-200-008 and Parcel 4711-09-300-001 is requested to be rezoned to ICPUD, for the following reasons:

- The rezoning criteria for Section 22.04 of the Zoning Ordinance have been met.
- The proposed zoning is consistent with the goals, policies and future land use map of the Genoa Township Master Plan, including any subarea or corridor studies. If conditions have changed since the Master Plan was adopted, the consistency with recent development trends in the area.
- The rezoning is compatible with the site's physical, geological, hydrological and other environmental features with the host of uses permitted in the proposed zoning district.
- The site is able to be reasonably developed with one of the uses permitted under the current zoning.
- All of the potential uses allowed in the proposed zoning district are compatible with surrounding uses and zoning in terms of land suitability, impacts on the environment, density, nature of use, traffic impacts, aesthetics, infrastructure and potential influence on property values.
- The Township's infrastructure and services are sufficient to accommodate the uses permitted in the requested district without compromising the "health, safety and welfare" of the Township.
- The apparent demand for the types of uses permitted in the requested zoning district in the Township in relation to the amount of land in the Township currently zoned is able to accommodate the demand.
- The rezoning is reasonable given the above criteria, a determination the requested zoning district is more appropriate than another district or amending the list of permitted or Special Land Uses within a district.
- The request has not previously been submitted within the past one year and there is a conceptual PUD Plan.

**The motion carried unanimously with a roll call vote.**

**Moved** by Commissioner Mortensen, seconded by Commissioner Dhaenens, to recommend to the Township Board approval of the PUD Agreement dated May 18, 2020 and as amended by the Township Attorney on June 11, 2020, subject to the following:

- The Township Attorney will work with the developer's attorney to clarify expiration dates of site plans.
- The Township Attorney will coordinate with the developer's attorney to develop language regarding the maintenance of the plantings in the right-of-way and the sidewalk on S. Latson Road to assure that the maintenance continues over time.
- Planning Commission shall review the requested lighting deviation at the time of the first site plan submittal where additional information and detail can be provided to the Planning Commission to see more clearly the deviation between what is being proposed and the ordinance requirements.
- The utility construction arrangements will be coordinated between the Township Attorney, the petitioner's attorney, and Township Staff prior to submission to the Township Board.

**The motion carried unanimously with a roll call vote.**

**Moved** by Commissioner Mortensen, seconded by Commissioner McBain, to recommend to the Township Board approval of the Impact Assessment dated June 19, 2019 for the following properties: 4711-08-400-004, 006, 012, 013, 014, 015, 020, 4711-09-300-031, 4711-17-200-008, and 4711-09-300-001. **The motion carried unanimously with a roll call vote.**

**Moved** by Commissioner Mortensen, seconded by Commissioner Dhaenens, to recommend to the Township Board approval of the Conceptual PUD dated May 20, 2020 for the following properties: 4711-08-400-004, 006, 012, 013, 014, 015, 020, 4711-09-300-031, 4711-17-200-008, and 4711-09-300-001, with the following conditions:

- The requirements of the Township Engineer in his letter dated June 3, 2020 shall be met.
- The requirements of the Brighton Area Fire Authority's letter dated March 26 , 2020 shall be met
- Easements will be required to allow cross access where vehicular and pedestrian traffic in each of the project areas.
- Details will be required for the highway sign, uses, dimensional standards, building and site design, etc. prior to development of the north area.

**The motion carried unanimously with a roll call vote.**

### ADMINISTRATIVE BUSINESS

#### **Staff Report**

Ms. VanMarter stated there may be an item for the July Planning Commission Meeting.

The minutes from this meeting and last week's meeting will be on the next meeting's agenda.

#### **Member Discussion**

Commissioner Rauch is excited that vacant properties are being developed; however, he would like to focus on existing buildings in the Township. He would like the Planning Commission to think about the Township becoming proactive and creative with regard to redevelopment opportunities. The Township could promote redevelopment, especially along the Grand River Corridor.

Commissioner Mortensen noted that the Master Plan is going to be updated shortly and this can be addressed during that time.

Ms. VanMarter agrees. There are organizations and associations in the area that she can reach out to for assistance.

Commissioner McBain agreed that young adults are not interested in large, expensive homes. They want to have smaller homes and be close to cities.

#### **Adjournment**

**Moved** by Commissioner Rickard, seconded by Commissioner Dhaenens, to adjourn the meeting at 9:54 pm. **The motion carried unanimously with a roll call vote.**

Respectfully Submitted,

Patty Thomas, Recording Secretary

September 10, 2024

Planning Commission  
 Genoa Township  
 2911 Dorr Road  
 Brighton, Michigan 48116

<b>Attention:</b>	Amy Ruthig, Planning Director
<b>Subject:</b>	Versa Development/Innovation Interchange – Amendment to approved PUD (2 <sup>nd</sup> Review)
<b>Location:</b>	Latson Road, south of I-96 interchange
<b>Zoning:</b>	CAPUD Interchange Campus Planned Unit Development

Dear Commissioners:

At the Township’s request, we have reviewed the revised submittal materials proposing an amendment to the approved Versa/Innovation Interchange Planned Unit Development.

The materials include amendments to the previously approved conceptual PUD plan and PUD Agreement, which includes the list of permissible uses and PUD Design Guidelines for the project.

**A. Summary**

**1. PUD Agreement:**

- a. The applicant must address any comments provided by the Township Attorney and staff.
- b. We suggest that language regarding the timing of development remain, such that development must commence on the west side of Latson Road prior to the east side.
- c. Exhibits A and B need minor corrections (as noted).

**2. Use Tables:**

- a. The applicant must provide a track changes version of the Use Table from the currently approved version.
- b. Buildings greater than 40,000 square feet in the west area should be listed as permitted (as opposed to “N/A”).
- c. We suggest that accessory outdoor storage be prohibited in the east area (as opposed to being special land use).

**3. Conceptual PUD Plan/Design Guidelines:**

- a. No changes are proposed to established standards for dimensions, building design/materials or lighting.
- b. Buffer width between industrial and residential or mixed use has been increased to 50 to 75 feet.
- c. Perimeter buffer widths are more clearly identified, including areas where the natural landscape will be preserved.

**4. Impact Assessment:**

- a. The revised Impact Assessment has corrected the discrepancies noted in our initial review letter.





*Aerial view of site and surroundings (looking east)*

## **B. Proposal/Process**

In summary, the request is to amend the previously approved PUD, as follows:

- Removal of the 5.74-acre property between the rail line and Beck Road for inclusion in a new PUD;
- Revisions to the 10.46-acre property on the east side of Latson Road, which was previously intended for commercial uses;
- Revisions to the host of permissible uses throughout the PUD, including prohibited uses; and
- Corresponding amendments to the PUD Agreement.

With the removal of the North Area, the PUD will comprise 189.88 acres – 177.27 acres on the west side of Latson Road (West Area), 10.46 acres on the east side of Latson Road (East Area) and 2.15 acres between Beck Road and I-96 (Development Sign Area).

The applicant seeks Planning Commission review of the First Amendment to the PUD.

Following a public hearing, the Commission may put forth a recommendation to the Township Board, who has final approval authority.

## **C. Review Comments**

We have reviewed the request and provide the following comments for the Commission's consideration:

### **1. PUD Agreement.**

- The applicant must address any comments provided by the Township Attorney or staff.
- We suggest that the current language regarding the timing of development remain, such that development must commence on the west side of Latson Road prior to development on the east side.
- Exhibit A (Legal Descriptions of Original PUD Properties) does not include Parcel #11-09-300-040, (which is being removed from this PUD) or Parcel #11-09-33-044. Additionally, there is a legal description for Parcel #11-09-300-001, which does not appear on the map.
- Exhibit B (Original Innovation Interchange PUD Parcel Map) has a typo for Parcel #11-09-300-031.

**2. Use Tables:**

- The applicant must provide a track changes version of the Use Table from the currently approved version in the PUD Agreement from 2020.
- Many of the more intensive land uses (distribution facilities, ambulance service/maintenance, helipads and uses with buildings greater than 200,000 square feet) will not be allowed in the east area.
- Any use greater than 40,000 square feet will require special land use review/approval in the east area. However, this line item should read as permitted (P) for the west area (instead of “N/A”).
- Asphalt plants, manufacture of automobiles, battery production, auto service/repair and self-storage are prohibited uses throughout the entire PUD.
- Accessory outdoor storage is allowed with special land use approval throughout the PUD; however, we suggest it be prohibited in the east area.

**3. Conceptual PUD Plan/Design Guidelines.**

- No changes are proposed to dimensional requirements, building design/material standards, or lighting standards.
- Page 9 includes 2 options for road improvements. The ultimate design is subject to authorization from the County.
- Page 10 increases the minimum buffer width between industrial and residential or mixed use to 50-75 feet.
- Page 12 identifies the perimeter buffer width requirements, as well as areas where the natural landscape is to remain.

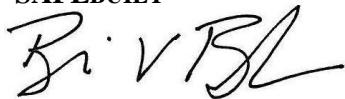
**4. Impact Assessment:**

- The revised Impact Assessment addresses our previous comments regarding acreages, parcels, uses and map corrections.

Should you have any questions concerning this matter, please do not hesitate to contact our office.

Respectfully,

**SAFE BUILT**



Brian V. Borden, AICP  
Michigan Planning Manager



September 10, 2024

Ms. Amy Ruthig  
Genoa Township  
2911 Dorr Road  
Brighton, MI 48116

**Re: Latson Road - Versa PUD Amendment  
Conceptual Site Plan Review No. 7**

Dear Ms. Ruthig:

Tetra Tech conducted a seventh site plan review of the Versa PUD conceptual plans, impact assessment, and traffic study submitted on August 27, 2024. The plans and impact assessment were prepared by MKSK, Atwell Group, and Fleis & Vandenbrink on behalf of Todd Wyett and Latson Partners LLC. The traffic impact study was prepared by Fleis & Vandenbrink. The original 200-acre PUD was previously approved, and the applicant is now proposing to modify the PUD by transferring 5.74 commercial acres east of Latson Road to a separate commercial PUD north of the railroad tracks and modifying the use of the remaining 10.46 acres to be the same as the portion of the PUD on the west side of Latson Road. We offer the following comments:

#### **GENERAL**

1. The site plan provided is still very conceptual and all future developments within the PUD will need to have their own site plan review and approval.

#### **SANITARY AND WATER SERVICES**

1. The previous impact assessment noted that the PUD will be serviced by water and sewer services through MHOG and GO-SWATH. Water and sanitary sewer extension to the south side of I-96 has been completed in anticipation of the proposed development. Furthermore, the expanded PUD area was already included in the assumptions made when completing the basis of design for the South Latson Road Water and Sewer improvements.

#### **TRAFFIC AND ROAD CONCEPTS**

1. Upon review of the Fleis & Vandenbrink executive memo for the Latson Road PUD Amendment it is apparent that light industrial use will generate less traffic than the originally proposed retail/service uses. Even if the numbers are off a bit in the end, we believe the only thing that really matters is that the total number of trips would be less than originally evaluated in the study, which is the finding of this report.
2. It should be noted that the difference in trips reported would be at the site driveways; however, the trip difference for new trips on the adjacent roadway network would be less given the relatively high portion of pass-by trips:
  - Original commercial development, total trips, AM inbound: 170
  - Proposed light industrial development, total trips, AM inbound: 51
  - Difference: 119 trips, but this is at the site driveway

3. On the adjacent roadway network, pass-by trips (drivers already on the road, but decide at last minute to make a stop as they pass by the development) is different:
  - o Original commercial development, new (total minus pass-by) trips, AM inbound: 99
  - o Proposed light industrial, new trips, AM inbound: 51
  - o Difference: 48 trips, but this is new traffic on the surrounding roads – nearly half of the “improvement” (reduction) reported.
4. The original traffic impact study includes a list of recommended improvements to mitigate the increase in traffic. These improvements will need to be considered by the Township as the PUD develops in the future. Any site drive or intersection recommendations should be included in future site plans for approval.
5. Improvements to Latson Road are subject to LCRC approval and should be submitted for review and comment by the Township. Any signage would need to be reviewed by the Township and their planning consultant.
6. The original traffic impact study was completed based on estimated traffic conditions for the proposed development. We note again that the traffic impact study should be updated as needed as part of the site plan application process. We suggest that the traffic impact study be updated when proposed development will generate over 100 trips per day per the Institute of Transportation Engineers (ITE) Trip Generation Manual, which is consistent with the Township’s Zoning Ordinance.

As noted previously, based on the conceptual nature of the PUD, it is difficult to perform an engineering review. Our general findings are presented above. These should be discussed with the applicant and planning commission and any comments incorporated in future submittals.

Sincerely,



Shelby Byrne, P.E.  
Project Engineer

Marianne McCreary  
Genoa Township Planning Commission  
2911 Dorr Road  
Genoa Township, Michigan 48116

My name is Bonnie Spicker  
and I live at 5606 Mountain Rd.

I am expressing my opposition to the proposed Latson PUD development. Genoa Township already suffers from over-development and a population explosion, and our Township cannot bear the cost, traffic, additional population increase or noise and light pollution of a large commercial or industrial development in this area.

The Proposed development is not compatible with the surrounding area, and would begin a surge of industrial and commercial development that will spread far beyond the current plan and throughout what has long been a quiet residential area of country estate homes, North Shore, and Oak Pointe, and will substantially decrease both the value and the character of these homes.

The uses permitted in this development are not conducive to the intent of the zoning ordinance, and many of the permitted uses, like a large industrial distribution warehouse, would bring significant amounts of light, noise and traffic pollution that were never the intent of the original CAPUD zoning. Moreover, Three Rivers Elementary School with over 600 elementary students would be compromised by the increased safety risk of an industrial district and a high-density housing population across the street.

I moved to Genoa Township because I was seeking a tranquil bedroom community away from urban sprawl, as did many others, and this development invites urban sprawl. Genoa Township has seen too much development in recent years and it is time we started protecting and preserving the character of our community before it disappears.

For these and many other reasons, I ask the township to deny any further rezoning of the Latson - Crooked Lake area and to reconsider significantly restricting or removing the current CAPUD - ICPUD South of Beck Road.

Sincerely,



Rec'd 5/3/24  
HARD COPY ALSO DELIVERED  
TO EACH BOARD AND  
PC MEMBER.

Kelly VanMarter, Township Manager  
Genoa Township  
2911 Dorr Road  
Genoa Township, MI 48116

May 2, 2024

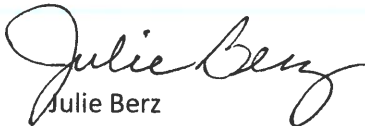
My name is Julie Berz and I live at 3093 Pineview Trail, Genoa Township.

I am expressing my opposition to the proposed Latson PUD development. Livingston County, and particularly Genoa Township, already is negatively affected by over development and an explosion of population. We cannot bear the cost, traffic, additional population increase or noise/light pollution of a large commercial or industrial development in this area.

My husband and I moved to Genoa Township from Oakland County 29 years ago. You can only imagine the urbanization of our former County, where forests and wetlands fell to the almighty god of development making living there sad, crowded and polluted. What Genoa Township lacks is more conserved natural park space, not more ugly development.

Livingston County is becoming overrun by development. I am urging you to put a stop to it. I ask the Planning Commission and Genoa Township Board to deny any further rezoning of the Latson- Crooked Lake area and to reconsider significantly restricting or removing the current CAPUD and ICPUD South of Beck Road.

Sincerely,

  
Julie Berz

Marianne McCreary  
Genoa Township Planning Commission  
2911 Dorr Road  
Genoa Township, Michigan 48116

My name is KEITH SPICHER

and I live at 5606 MOUNTAIN RD BRIGHTON.

I am expressing my opposition to the proposed Latson PUD development. Genoa Township already suffers from over-development and a population explosion, and our Township cannot bear the cost, traffic, additional population increase or noise and light pollution of a large commercial or industrial development in this area.

The Proposed development is not compatible with the surrounding area, and would begin a surge of industrial and commercial development that will spread far beyond the current plan and throughout what has long been a quiet residential area of country estate homes, North Shore, and Oak Pointe, and will substantially decrease both the value and the character of these homes.

The uses permitted in this development are not conducive to the intent of the zoning ordinance, and many of the permitted uses, like a large industrial distribution warehouse, would bring significant amounts of light, noise and traffic pollution that were never the intent of the original CAPUD zoning. Moreover, Three Rivers Elementary School with over 600 elementary students would be compromised by the increased safety risk of an industrial district and a high-density housing population across the street.

I moved to Genoa Township because I was seeking a tranquil bedroom community away from urban sprawl, as did many others, and this development invites urban sprawl. Genoa Township has seen too much development in recent years and it is time we started protecting and preserving the character of our community before it disappears.

For these and many other reasons, I ask the township to deny any further rezoning of the Latson - Crooked Lake area and to reconsider significantly restricting or removing the current CAPUD - ICPUD South of Beck Road.

Sincerely,



Genoa Township Planning Commission  
2911 Dorr Road  
Genoa Township, Michigan 48116

My name is RON KING.

and I live at 4758 TREASURE LK.

I am expressing my opposition to the proposed Latson PUD development. Livingston County, and particularly Genoa Township, already suffers from over-development and a population explosion, and we cannot bear the cost, traffic, additional population increase or noise and light pollution of a large commercial or industrial development in this area.

I moved to Livingston County because I was seeking a tranquil bedroom community away from urban sprawl, as did many others, and this development invites urban sprawl. Genoa Township and Livingston County has seen too much development in recent years and it is time we started protecting and preserving the character of our community before it disappears.

Grand River alone has dozens of vacancies for both commercial and industrial use, and no fewer than four additional housing or apartment developments are going up in Genoa Township currently.

The reasons that may have existed five or ten years ago to justify any additional development in this area do not exist today, and Livingston County is becoming overrun by development. I am urging you to put a stop to it.

For these and many other reasons, I ask the township to deny any further rezoning of the Latson - Crooked Lake area and to reconsider significantly restricting or removing the current CAPUD and ICPUD South of Beck Road.

Sincerely,

Ron King  
6/12/24



Genoa Township Planning Commission  
2911 Dorr Road  
Genoa Township, Michigan 48116

My name is Pamela King.

and I live at 4758 TREASURE LK DRIVE HOWELL.

I am expressing my opposition to the proposed Latson PUD development. Livingston County, and particularly Genoa Township, already suffers from over-development and a population explosion, and we cannot bear the cost, traffic, additional population increase or noise and light pollution of a large commercial or industrial development in this area.

I moved to Livingston County because I was seeking a tranquil bedroom community away from urban sprawl, as did many others, and this development invites urban sprawl. Genoa Township and Livingston County has seen too much development in recent years and it is time we started protecting and preserving the character of our community before it disappears.

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For these and many other reasons, I ask the township to deny any further rezoning of the Latson - Crooked Lake area and to reconsider significantly restricting or removing the current CAPUD and ICPUD South of Beck Road.

Sincerely,

*Pamela King*  
4-12-24

April 29, 2024

GENOA TOWNSHIP

Genoa Township Board of Trustees  
2911 Dorr Road  
Genoa Township, Michigan 48116

MAY 2, 2024

RECEIVED

My name is Kristine Cook Lindsey  
and I live at 3920 Highcrest Dr., Brighton, MI 48116

I am expressing my opposition to the proposed Latson PUD development. Genoa Township already suffers from over-development and a population explosion, and our Township cannot bear the cost, traffic, additional population increase or noise and light pollution of a large commercial or industrial development in this area.

The Proposed development is not compatible with the surrounding area, and would begin a surge of industrial and commercial development that will spread far beyond the current plan and throughout what has long been a quiet residential area of country estate homes, North Shore, and Oak Pointe, and will substantially decrease both the value and the character of these homes.

The uses permitted in this development are not conducive to the intent of the zoning ordinance, and many of the permitted uses, like a large industrial distribution warehouse, would bring significant amounts of light, noise and traffic pollution that were never the intent of the original CAPUD zoning. Moreover, Three Fires Elementary School with over 600 elementary students would be compromised by the increased safety risk of an industrial district and a high-density housing population across the street.

I live in Genoa Township because I am seeking a tranquil bedroom community away from urban sprawl, as do many others, and this development invites urban sprawl. Genoa Township has seen too much development in recent years and it is time we started protecting and preserving the character of our community before it disappears.

For these and many other reasons, I ask the township to deny any further rezoning of the Latson - Crooked Lake area and to reconsider significantly restricting or removing the current CAPUD South of Beck Road.

Sincerely,



# GENOA TOWNSHIP

APR - 0 2024

RECEIVED

March 28, 2024

To whom it may concern,

My name is Lira Lloyd and I live at 2319 E. Coon Lake Rd. I am expressing my opposition to the proposed Latson PUD development. Genoa Township already suffers from over-development and we are rapidly losing the rural and natural character of the community that we all love and cherish.

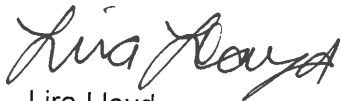
I live in Genoa Township because I do not want to live in the middle of urban sprawl, and this development invites urban sprawl. Genoa Township and Livingston County has seen too much development in recent years and it is time we started protecting and preserving the character of our community before it disappears.

I have serious concerns about the destruction and pollution of the wetlands in and around this proposed development, the Shiawasee and Huron Riversheds, the country drain that runs through this development, and the runoff that will be draining into all of these water sources. I also have significant concerns about any threatened or endangered species that may inhabit this area. Over 80 new species were added to the Endangered Species Act last Spring, and no study has been done to determine whether a development on this nearly 200 acres will destroy one or more essential habitats.

The reasons that may have existed five or ten years ago to justify any additional development in this area do not exist today, and Genoa Township is overrun by development. I am urging you to put a stop to it.

For these and many other reasons, I ask the township to deny any further rezoning of the Latson - Crooked Lake area and to reconsider significantly restricting or removing the current CAPUD South of Beck Road.

Sincerely,



Lira Lloyd

Genoa Township Planning Commission  
2911 Dorr Road  
Genoa Township, Michigan 48116

My name is Claire McCarthy.

and I live at 3222 Hillandale Dr. Howell.

I am expressing my opposition to the proposed Latson PUD development. Livingston County, and particularly Genoa Township, already suffers from over-development and a population explosion, and we cannot bear the cost, traffic, additional population increase or noise and light pollution of a large commercial or industrial development in this area.

I moved to Livingston County because I was seeking a tranquil bedroom community away from urban sprawl, as did many others, and this development invites urban sprawl. Genoa Township and Livingston County has seen too much development in recent years and it is time we started protecting and preserving the character of our community before it disappears.

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For these and many other reasons, I ask the township to deny any further rezoning of the Latson - Crooked Lake area and to reconsider significantly restricting or removing the current CAPUD and ICPUD South of Beck Road.

Sincerely,

*Claire McCarthy*

GENOA TOWNSHIP

MAY 08 2024

RECEIVED

Genoa Township Planning Commission  
2911 Dorr Road  
Genoa Township, Michigan 48116

My name is Claire McCarthy.

and I live at 3222 Hillandale Dr. Howell.

I am expressing my opposition to the proposed Latson PUD development. Livingston County, and particularly Genoa Township, already suffers from over-development and a population explosion, and we cannot bear the cost, traffic, additional population increase or noise and light pollution of a large commercial or industrial development in this area.

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For these and many other reasons, I ask the township to deny any further rezoning of the Latson - Crooked Lake area and to reconsider significantly restricting or removing the current CAPUD and ICPUD South of Beck Road.

Sincerely,

*Claire McCarthy*

GENOA TOWNSHIP

MAY 08 2024

RECEIVED

Genoa Township Planning Commission  
2911 Dorr Road  
Genoa Township, Michigan 48116

My name is Pamela King.

and I live at 4758 TREASURE LK DRIVE HOWELL.

I am expressing my opposition to the proposed Latson PUD development. Livingston County, and particularly Genoa Township, already suffers from over-development and a population explosion, and we cannot bear the cost, traffic, additional population increase or noise and light pollution of a large commercial or industrial development in this area.

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For these and many other reasons, I ask the township to deny any further rezoning of the Latson - Crooked Lake area and to reconsider significantly restricting or removing the current CAPUD and ICPUD South of Beck Road.

Sincerely, *Pamela King*  
4-12-24

Genoa Township Planning Commission  
2911 Dorr Road  
Genoa Township, Michigan 48116

My name is Clarence (Bill) Reiber

and I live at 3154 Stillriver Rd Howell, MI 48843

I am expressing my opposition to the proposed Latson PUD development. Livingston County, and particularly Genoa Township, already suffers from over-development and a population explosion, and we cannot bear the cost, traffic, additional population increase or noise and light pollution of a large commercial or industrial development in this area.

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Sincerely,



**GENOA TOWNSHIP**

**APR 18 2024**

**RECEIVED**

Genoa Township Planning Commission  
2911 Dorr Road  
Genoa Township, Michigan 48116

My name is Todd Walker,

and I live at 4758 Roundtree Drive, Brighton, MI 48116.

I am expressing my opposition to the proposed Latson PUD development. Livingston County, and particularly Genoa Township, already suffers from over-development and a population explosion, and we cannot bear the cost, traffic, additional population increase or noise and light pollution of a large commercial or industrial development in this area.

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Sincerely,



GENOA TOWNSHIP

APR 19 2024

RECEIVED



# COMMUNITY IMPACT ASSESSMENT

## INNOVATION INTERCHANGE PUD

August 27, 2024



Prepared By:



In accordance with Section 18.07 of the Genoa Township Zoning Ordinance, this impact assessment describes the Versa property, the intended land uses, the potential impacts, and design features to minimize the negative impacts. Given the size of the property and the range of potential land uses, some portions of this report are general in nature. More specific assessments will be provided when more detailed site plans are submitted for a specific project or phase.

The PUD will be designated as an employment center for office, research, light industrial and warehousing uses.

**18.07.01 Preparer.**

This statement was prepared by Bradley Strader, AICP, Principal Planner, C2G and Eric Lord, P.E., Vice President, Atwell. A traffic impact study will be submitted separately, prepared by Julie Kroll of Fleis & Vandenbrink.

**Cincar Consulting Group  
(C2G)**  
17199 N Laurel Park Drive  
Suite 204  
Livonia, MI 48152  
(313) 652-1101  
Bradley Strader, Principal  
[Brad.Strader@itsc2g.com](mailto:Brad.Strader@itsc2g.com)

**ATWELL, LLC**  
Two Towne Square, Suite 700  
Southfield, MI 48076  
(248) 447-2000  
Eric Lord, Vice President  
[elord@atwell-group.com](mailto:elord@atwell-group.com)

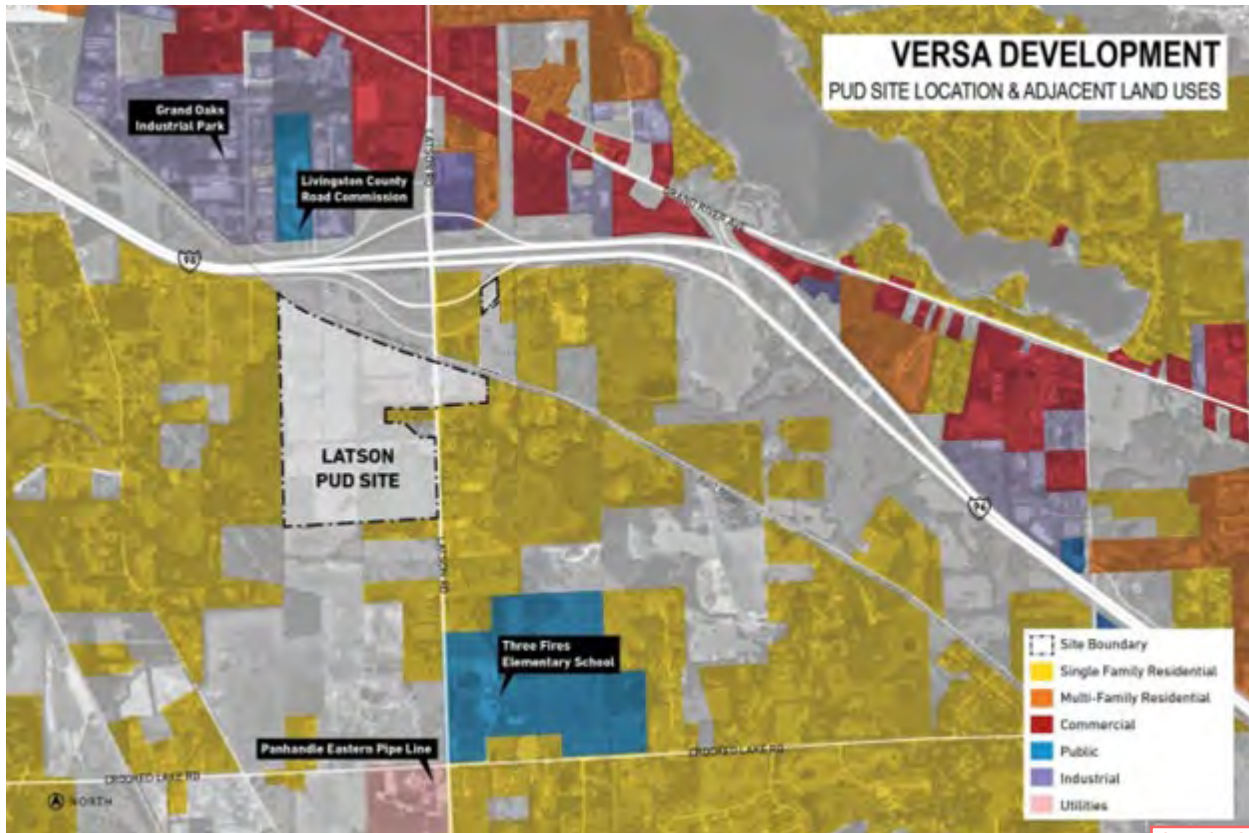
**FLEIS & VANDENBRINK**  
27725 Stansbury St #195  
Farmington Hills, MI 48334  
(248) 536-0080  
Julie Kroll, Traffic Services  
Group Manager  
[jkroll@fveng.com](mailto:jkroll@fveng.com)

**18.07.02 Location.**

The project site includes ±189.88 acres and is located south of the I-96 Interchange and the railroad tracks, primarily along the western side of Latson Road. The site wraps around several properties that front the west side of Latson Road that are not part of the PUD. A small portion of the PUD area lies on the east side of Latson Road (please refer to site location and land use map on the following page). The areas north of the site along Latson and extending along Grand River Avenue includes an extensive amount of regional type commercial developments.

The following parcels are included in the PUD:

- 11-08-400-004
- 11-08-400-006
- 11-08-400-012
- 11-08-400-013
- 11-08-400-014
- 11-08-400-015
- 11-08-400-020
- 11-09-300-031
- 11-09-300-044
- 11-17-200-008



the drain does not bisect the property

**18.07.03 Impact on Natural Features.**

The subject property is comprised of approximately 189.88 acres of land, of which 177.27 acres is situated west of Latson Road and 12.61 acres is located east of Latson Road. Much of the 177.27 acre area west of Latson Road is active farmland. The Marion Genoa Drain bisects the subject property and ultimately receives runoff from much of the site. The topography generally slopes from north to south and from south to north in the direction of the drain across approximately 50 feet of fall, with typically moderate slopes of 2-5%.

statement regarding slope is conflicting.

The primary natural feature asset of the property is a ±27-acre wooded area located along the west side of Latson Road, north of the Marion Genoa Drain. Within the wooded area is a low-lying State regulated wetland that appears to connect through the adjacent property to the south before merging with the Marion Genoa County Drain. This large area provides a natural buffer and screening from the rear of the proposed development to Latson Road. We view this wooded wetland area as a natural asset to the development that is intended to be preserved.

A second wooded area approximately six acres in size is located further west of the 27 acre wooded area, a portion of which contains a wetland. The regulatory status of this wetland is unknown currently. Topography within this wooded area slopes to the southwest, which is where a large portion of surface runoff exits the site on its way to the Marion Genoa Drain. Because this is a low point of the site, a detention basin in this general area is anticipated to contain runoff from the developed site prior to discharge. We anticipate that several of the trees will be impacted in this area as a result, though efforts will be made to maintain a buffer to the neighboring properties. The intent of the development is to avoid impacts to this wetland area.

A low-lying area also exists west of Latson Road along the west property line toward the northern middle of the site. An approximately 0.8-acre wetland of unknown regulatory status exists in this area, which collects localized runoff prior to exiting the site to the west. The intent of the development is to avoid impacts to this wetland area.

A single-family home exists on the property immediately east of Latson Road. The property is primarily open, with some evidence of prior farming activity. A few small stands of trees exist on the property, and there is no evidence of wetland. Topography generally and gradually slopes from north to southeast across the property. We anticipate this property to be developed for office/research and development/light industrial use, and as such will likely see impacts to the trees located in the interior of the site, though opportunities will be explored to preserve trees around perimeter property lines where possible.

#### **18.07.04 Impact on Stormwater Management.**

The topography west of Latson Road is such that there are three primary drainage patterns for surface runoff north of the Marion Genoa Drain. The northwest portion of the property drains south to the existing wetland pocket along the middle of the west property line. From there runoff will enter the neighboring site to the west on its way ultimately to the Marion Genoa Drain. The lower middle area of the subject property (north of the drain) contains a high point from which water is diverted to the southwest corner of the property and to the southeast corner. Both drainage patterns result in water running through adjacent parcels to the south and ultimately ending in the Marion Genoa Drain, which is under Livingston County jurisdiction.

The topography east of Latson Road generally drains from north to south and continues south to and through a series of low-lying areas and potential wetlands on adjacent property. This area is part of the drainage district for the Marion Genoa Drain.

According to the USDA Natural Resources Conservation Service Soils information, the subject area west of Latson Road is primarily comprised of Wawasee and Miami Loam soil, which is classified as a soils group C. Soils of this type experience low to moderate infiltration with stormwater typically saturating the soil before running off toward lower areas. High groundwater is not anticipated. These soil types do not generally limit development of land.

As previously described, there is a fair amount of grade change to the property particularly west of Latson Road. Development of the property will be designed to maintain similar drainage patterns to what occurs now. A stormwater management system will be designed for the development in accordance with the requirements of the Livingston County Drain Commissioner's office, which will include:

- Water quality measures
- Stormwater detention sized for the 100-year storm event
- Soil erosion control

We anticipate the detention basins will be strategically located at or near the existing low points of the property where stormwater is currently leaving the site. The basins will retain the water for a period with a restricted release to maintain the current drainage patterns from the property. As mentioned earlier, the subject area is tributary to the Marion Genoa Drainage District which is the ultimate receiving water course.

A soil erosion control permit will be obtained prior to construction from Livingston County which will require the site to be managed to control erosion created by construction activity. Examples of erosion control measures that are typically deployed during site development include:

- Silt fencing and vegetative buffer strips to keep soil contained within the construction area.
- Mud Mats at construction entrances to avoid tracking onto public roads.
- Inlet protection – silt sacks in catch basins to avoid sediment buildup in storm pipes and ponds.
- Stone Rip Rap – at culvert outlets to reduce scour and erosion.
- Seed and mulch – of graded areas to promote vegetation growth, which is key to controlling erosion<sup>36</sup> established.

limited ancillary uses such as,

large lot

medical, education,

developer should provide evidence of demand

**18.07.05 Impact on Surrounding Land Use.**

The Genoa Township Master Plan (2023) designates the Latson Road corridor south of the new I-96 Interchange as an area to concentrate new development, with a goal of an “Interchange Campus.” Uses contemplated in the Master Plan include research and development facilities, corporate offices, a conference center and hotel, and restaurants and other services that are complementary to the overall development. The site is within the Growth Boundary and designated as a “Primary Growth Area” in the Master Plan. South of the “Interchange Campus” area is what is described in the Master Plan as a “Transitional Area” which anticipates residential use and/or extension of the Interchange Campus area.

The proposed PUD accommodates those types of uses but with the addition of some light industrial and warehousing uses in the Interchange Campus area. The developer notes that there is significant demand in Livingston County for such uses, and that this location in Genoa Township is very appealing given the proximity to the well-designed I-96 interchange (as compared to many complex freeway interchanges in the county). These types of light industrial uses can also be designed to promote a campus setting, with a median along Latson Road, entryways, quality architecture, landscaping, pathways, consistent signage, and other attractive features. In addition, these types of uses can help stimulate development of some of the other uses desired by the Township, such as corporate offices and R & D centers.

As shown on the concept plan, described in the Design Guidelines, and as prescribed in the PUD Agreement, a number of provisions are included to help ensure the development is compatible with the surrounding area. These include:

- Preserved or landscaped buffers adjacent to residential areas.
- Most of the anticipated traffic to and from future development will use the I-96 interchange and higher density development will occur closer to the interchange, helping to minimize traffic impacts to the surrounding area.
- An extensive streetscape and potentially a median along Latson Road to provide an attractive gateway to the PUD and Southern Genoa Township
- Standards for high quality architectural design for facades visible to the public, including from I-96.
- Lighting standards to help preserve the existing “dark sky” environment.

All of the development is intended to comply with the operational requirements and performance measures in the Genoa Township Zoning Ordinance. More details regarding types of proposed uses, hours of operation, noise for particular uses, activity during construction periods, etc. will be provided once individual site plans are submitted for development.

**18.07.06 Impact on Public Facilities and Services.**

This section covers the anticipated broad impacts of the Development. Individual uses and site plans submitted in the future may need to provide more information on their particular impacts, depending upon the use. For example, water and sewer needs may vary for a particular use.

Generally, the main impacts will be traffic and public water and sewer, as noted in the sections below. In terms of employees, this will vary depending upon the types of sizes of the individual site plans. It is expected that the impacts on police, fire, emergency response and other Township or County services will be minimal. The tax benefits of the development will provide a high benefits-to-impact ratio, which will benefit the Township.

**18.07.07 Impact on Public Utilities.**

To provide public water and sanitary sewer service to the subject area south of I-96, public extension of those utilities is required. The initial stage to bring utilities to the south side of I-96 has already been completed in accordance with the permitted design plans prepared by Tetra Tech., which is shown on the attached utility exhibits. From there, utilities will be extended south along Latson Road as well as through the development area to service the district as reflected in conceptual utility exhibits. Water service will be provided by the Marion, Howell, Ocoola & Genoa Sewer and Water Authority (MHOG). Sanitary sewer service will be provided by the Genoa Ocoola Sewer and Water Authority (GO).

office, research & development, and

explain, how does distribution & warehouse stimulate development

developer funded

This site is not directly adjacent to the drain.

I don't believe this is accurate

A 12-inch water main, serviced by MHOG, has been extended in two locations: from Grand Oaks Drive across I-96 to the northwest corner of Latson Farm parcel south of the railroad tracks and from Kohl's across I-96 to Beck Road then west to Latson and south to the northeast corner of the Latson Farms parcel south of the railroad tracks. Once the developments in the South Latson Road area are constructed, the internal watermain will complete the loop.

Sanitary sewer within the proposed South Latson Road development area will consist of gravity sewers that flow to a proposed pump station located internal to the development on the west side of Latson Road adjacent to the Marion-Genoa Drain, the natural low point in the area. A force main will extend north from the pump station through the subject property and cross under I-96 before tapping into the existing sanitary system at Grand Oaks Drive. The area is ultimately serviced by the GO WWTP, which has recently received system capacity upgrades and is able to service the anticipated load from the South Latson Road development area.

Each development proposed within the South Latson Road area will be serviced by public water and sewer, designed to local, County and State requirements. Approximately 1,497 Residential Equivalent Units (REU) is anticipated for the South Latson Road development area with approximately 623 REUs assigned to the PUD. MHOG standards equate one REU to ~~250~~ gallons per day for average daily demand.

218

Franchise utilities serving the South Latson Road area will include gas, electric, telephone and data. Coordination with those utility providers to bring service to the area will continue as development plans progress.

Please see the Water Distribution Infrastructure and Sanitary Sewer Collection Infrastructure Maps in Appendix.

anticipated

**18.07.08 Storage and Handling of any Hazardous Materials.**

The northern development area west of Latson Road is primarily anticipated for light industrial and office use, subsequently there are no specific plans for storing of significant hazardous materials. Each development proposed within the subject area will be responsible for meeting all storage and handling requirements, as applicable.

**18.07.09 Traffic Impact Study.**

A separate traffic impact study has been prepared by Fleis and Vandenbrink. The study area and contents of this study has been coordinated with the Livingston County Road Commission with a focus on the potential cross section for Latson Road (such as a median), its design, and the preferred location for access points to the PUD along with impacted intersections in the surrounding area. Please refer to this report for a detailed analysis of traffic impacts and recommended improvements.

**18.07.10 Historic and Cultural Resources.**

Three of the homes in the proposed development area were built in 1958 and thus are more than 50 years old. However, those homes are not included on the State or National Historic Registers.

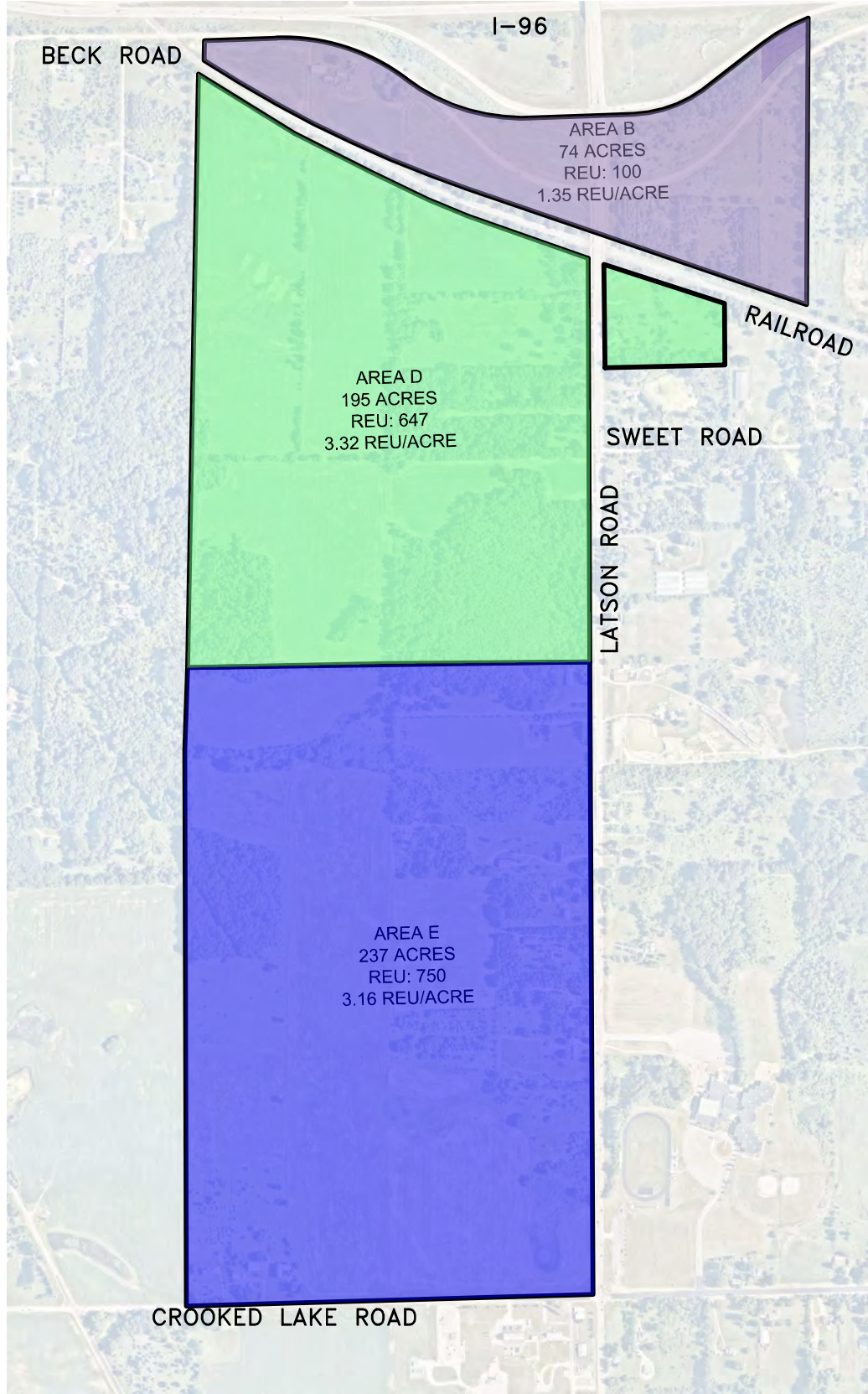
**18.07.11 Special Provisions.**

The PUD Agreement contains several provisions regarding the uses, operations, design and other standards that will apply to the Development and future site plans and owners.

- Genoa Township Master Plan
- I-96 Interchange Environmental Impact Statement
- Conversations with the Township and Livingston County Road Commission staff

**Appendix:**

- South Latson Road Service Area Map
- PUD REU Allocation Map
- Figure 1: Water Distribution Infrastructure Map
- Water Main Concept Map
- Figure 2: Sanitary Sewer Collection Infrastructure Map
- Sanitary Sewer Concept Map
- Soils and Wetlands Site Map
- Topography and Natural Features Site Map



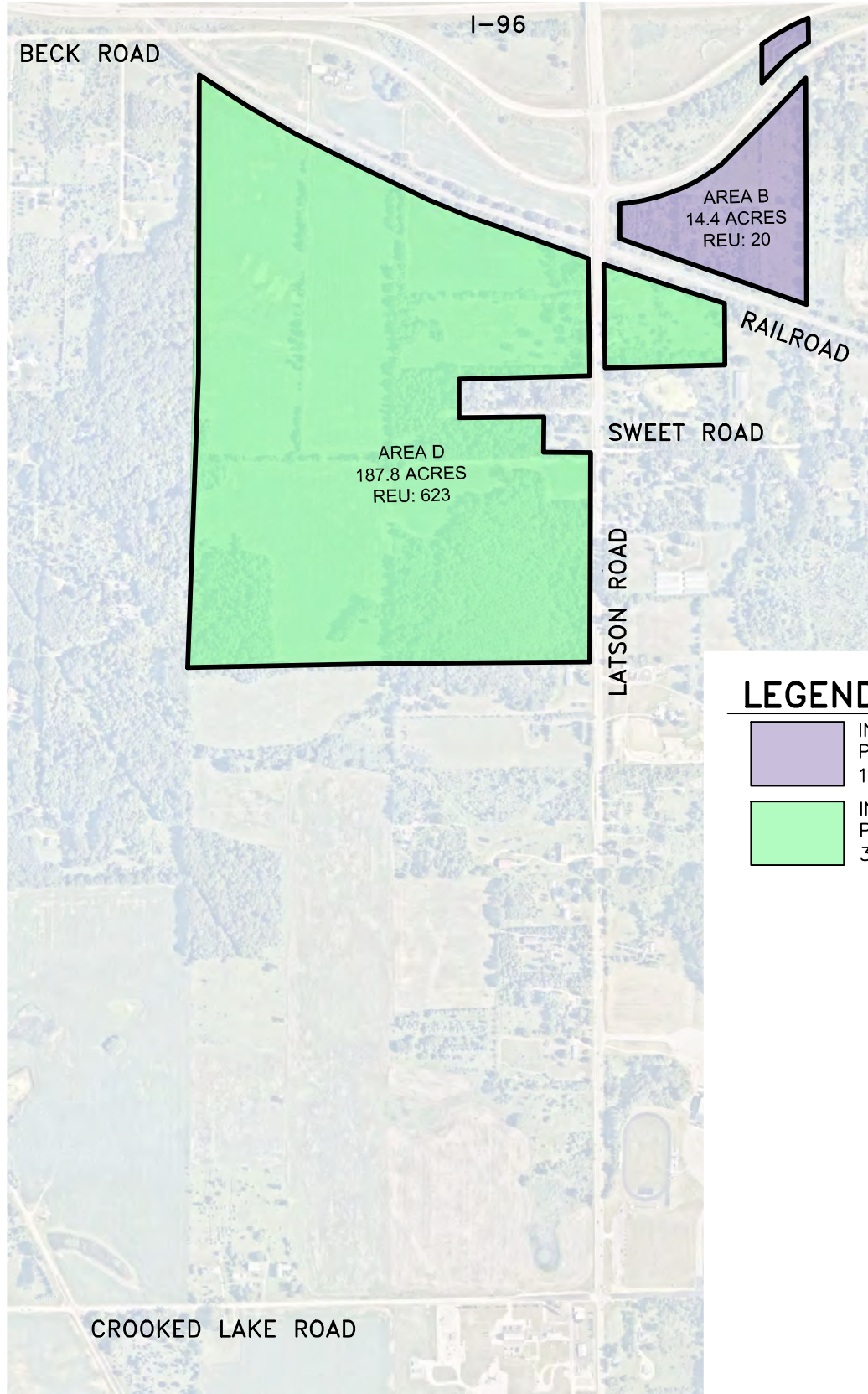
**SOUTH LATSON ROAD SERVICE AREA**

PROJECT NO.: 16001784  
DATE: 2023-08-15



**ATWELL**  
866.850.4200 [www.atwell-group.com](http://www.atwell-group.com)





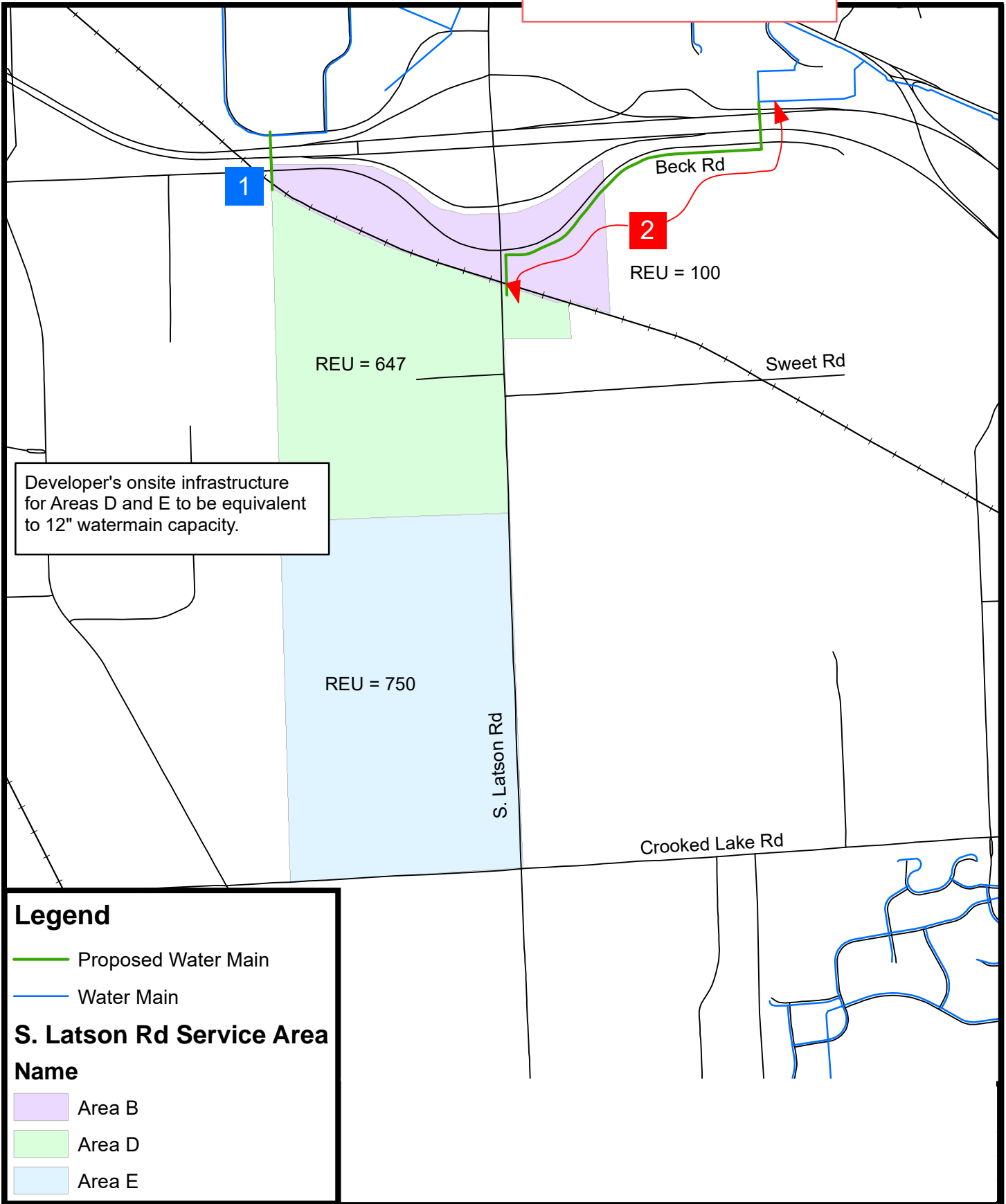
**LEGEND**

- INTERCHANGE COMMERCIAL  
PUD – AREA B  
1.35 REU/ACRE
- INNOVATION INTERCHANGE  
PUD – AREA D  
3.32 REU/ACRE

**INNOVATION INTERCHANGE PUD  
REU ALLOCATION**

PROJECT NO.: 16001784  
DATE: 2024-06-07

This exhibit is outdated



Developer's onsite infrastructure for Areas D and E to be equivalent to 12" watermain capacity.

**Legend**

- Proposed Water Main
- Water Main

**S. Latson Rd Service Area Name**

- Area B
- Area D
- Area E

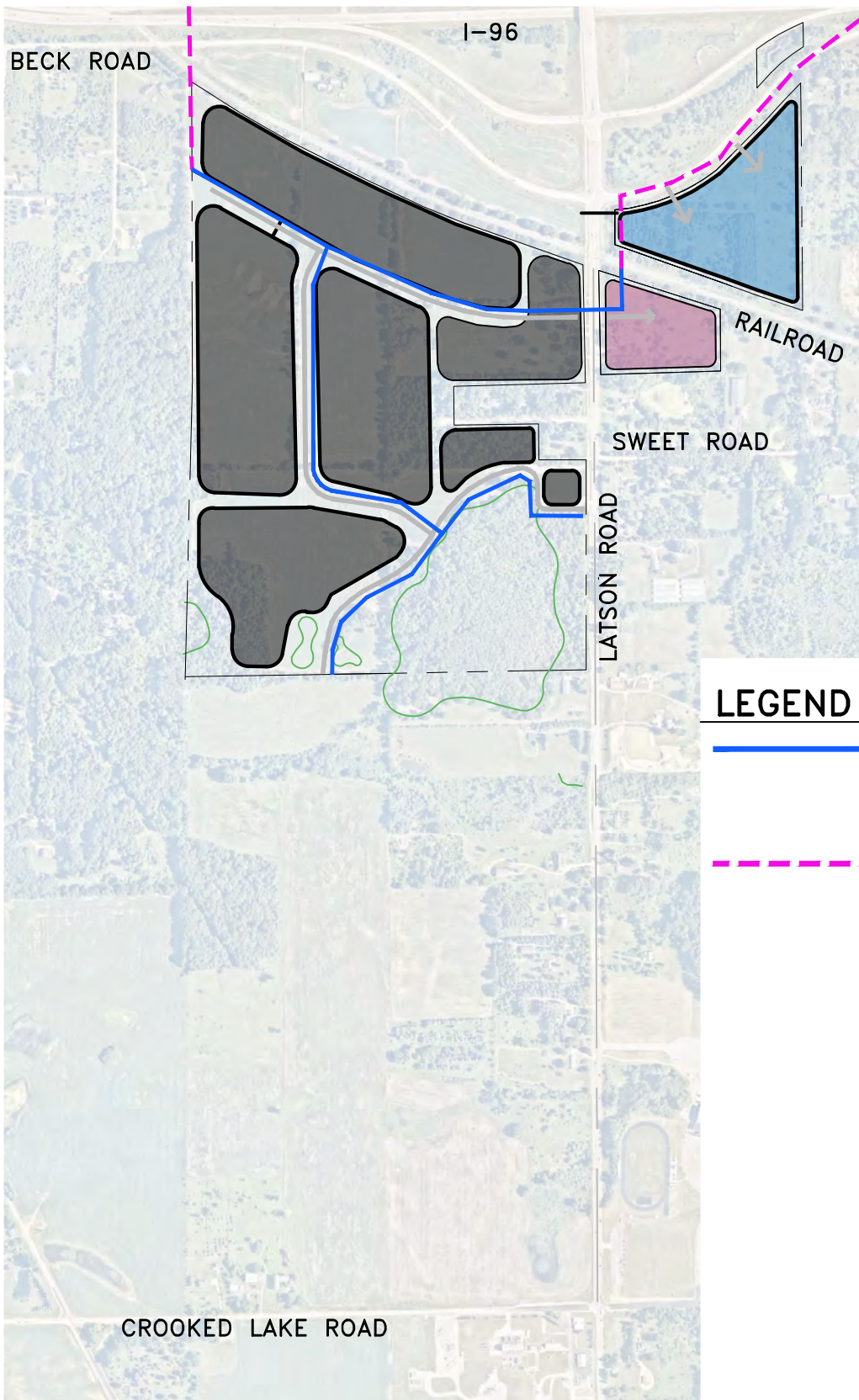
1 inch = 1,500 feet

**Figure 1**  
**Water Distribution Infrastructure**

Date: 3/2/2018

Note: This is a graphical representation of the required improvements. Final routing and location will be required during the design phase.





**LEGEND**

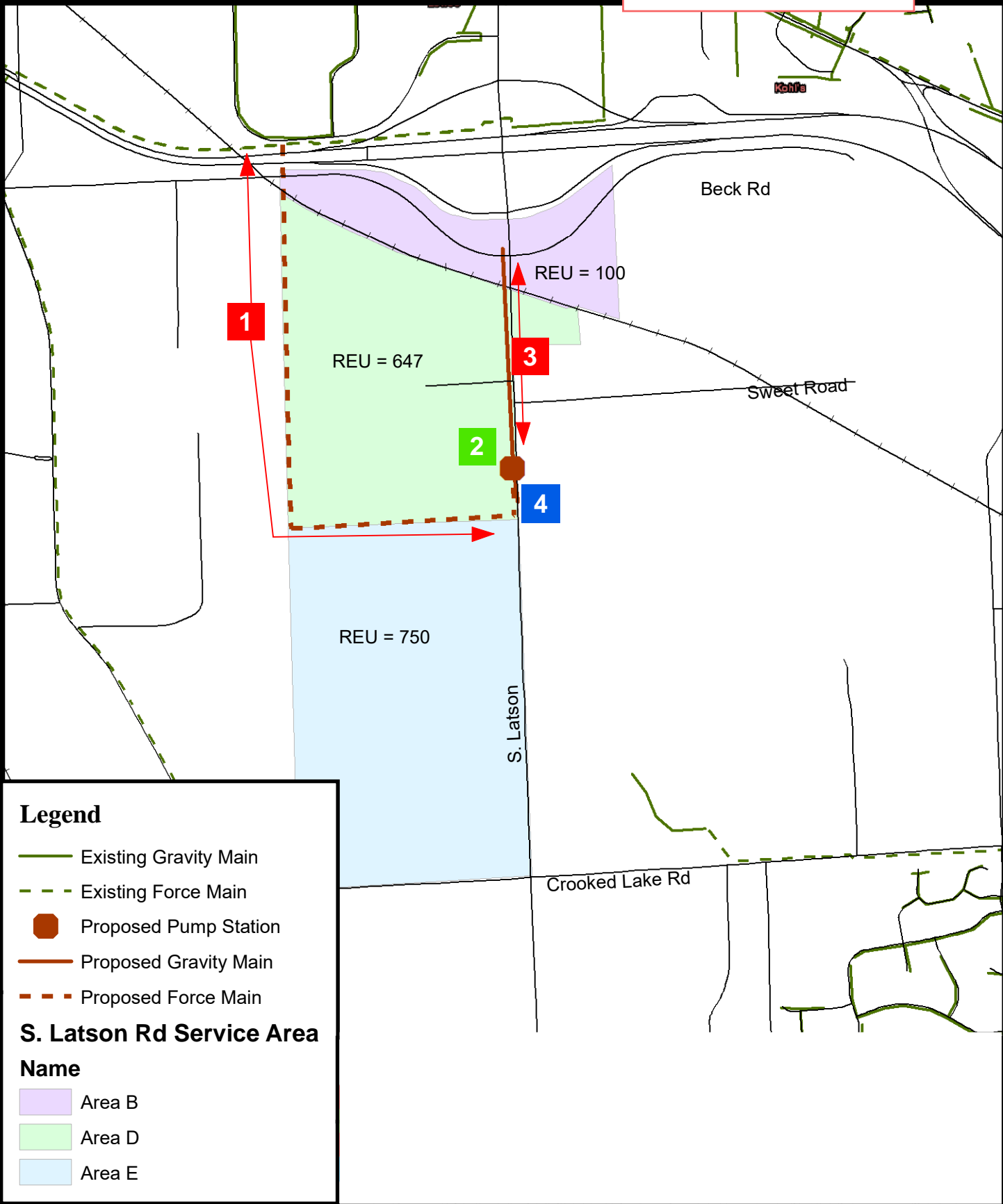
- PR WATERMAIN  
(TO BE INSTALLED  
AS PART OF THE  
INNOVATION  
INTERCHANGE PUD)
- - - - - EX WATERMAIN

**WATERMAIN CONCEPT**

NOTE:  
WATER MAIN TO BE A COMBINATION OF 8" AND  
12" IN DIAMETER

PROJECT NO.: 16001784  
DATE: 2024-06-07

This exhibit is outdated



**Legend**

- Existing Gravity Main
- - - Existing Force Main
- Proposed Pump Station
- Proposed Gravity Main
- - - Proposed Force Main

**S. Latson Rd Service Area**

**Name**

- Area B
- Area D
- Area E

1 inch = 1,500 feet

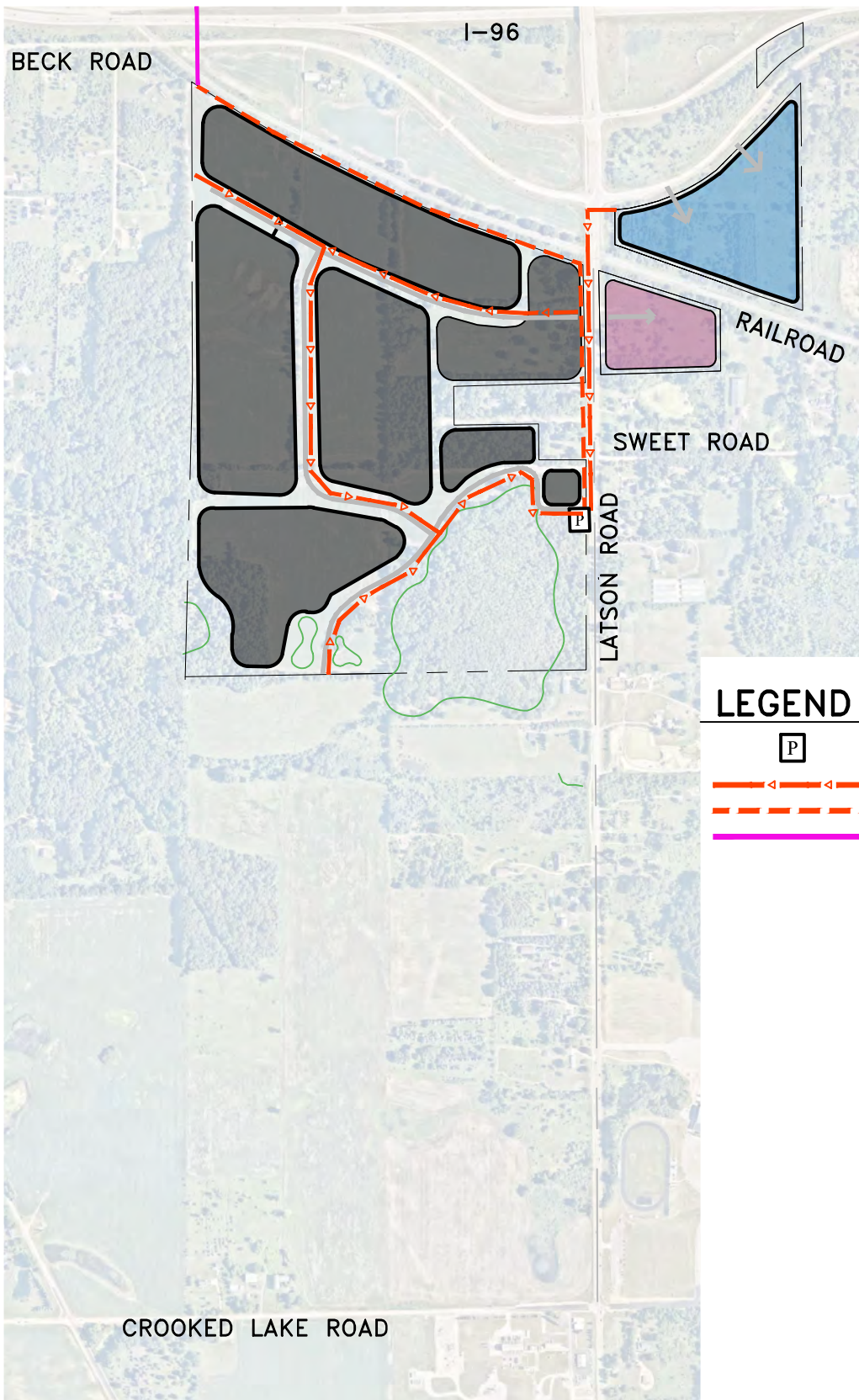
**Figure 2**

Date: 3/2/2018





**Sanitary Sewer Collection Infrastructure**

Note: This is a graphical representation of the required improvements. Final routing and location will be required during the design phase





**LEGEND**

-  PUMP STATION
-  PR GRAVITY SEWER
-  PR FORCE MAIN
-  EX 8" FORCE MAIN

**SANITARY SEWER CONCEPT**

PROJECT NO.: 16001784  
 DATE: 2024-06-07

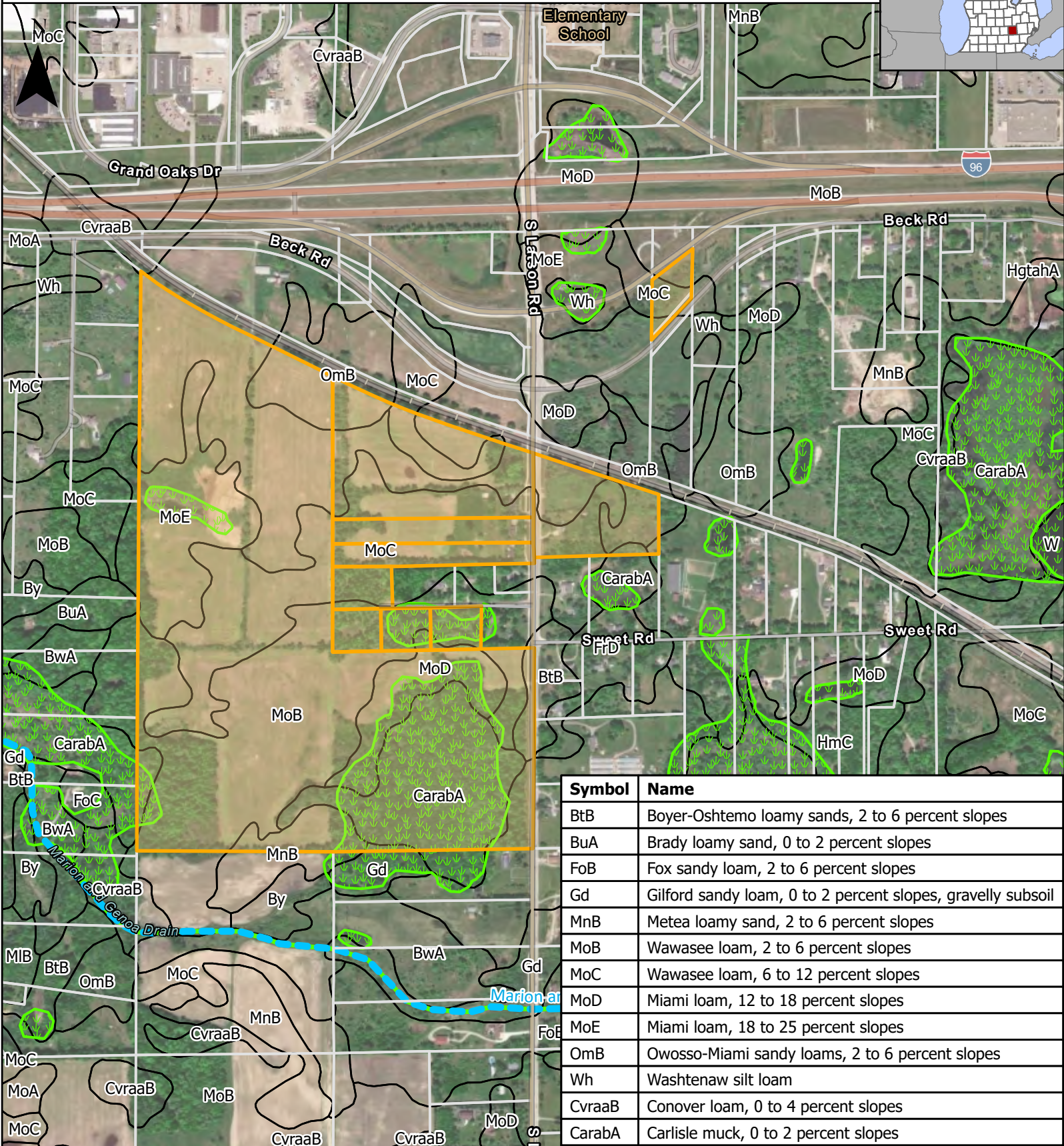
NOTE:  
 SANITARY SEWER TO BE A COMBINATION OF 8"  
 AND 10" GRAVITY SEWER AND 8" FORCE MAIN



# Innovation Interchange PUD

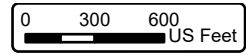
## Site Map - Soils and Wetlands

Genoa Township, Livingston County, Michigan



Symbol	Name
BtB	Boyer-Oshtemo loamy sands, 2 to 6 percent slopes
BuA	Brady loamy sand, 0 to 2 percent slopes
FoB	Fox sandy loam, 2 to 6 percent slopes
Gd	Gilford sandy loam, 0 to 2 percent slopes, gravelly subsoil
MnB	Metea loamy sand, 2 to 6 percent slopes
MoB	Wawasee loam, 2 to 6 percent slopes
MoC	Wawasee loam, 6 to 12 percent slopes
MoD	Miami loam, 12 to 18 percent slopes
MoE	Miami loam, 18 to 25 percent slopes
OmB	Owosso-Miami sandy loams, 2 to 6 percent slopes
Wh	Washtenaw silt loam
CvraaB	Conover loam, 0 to 4 percent slopes
CarabA	Carlisle muck, 0 to 2 percent slopes

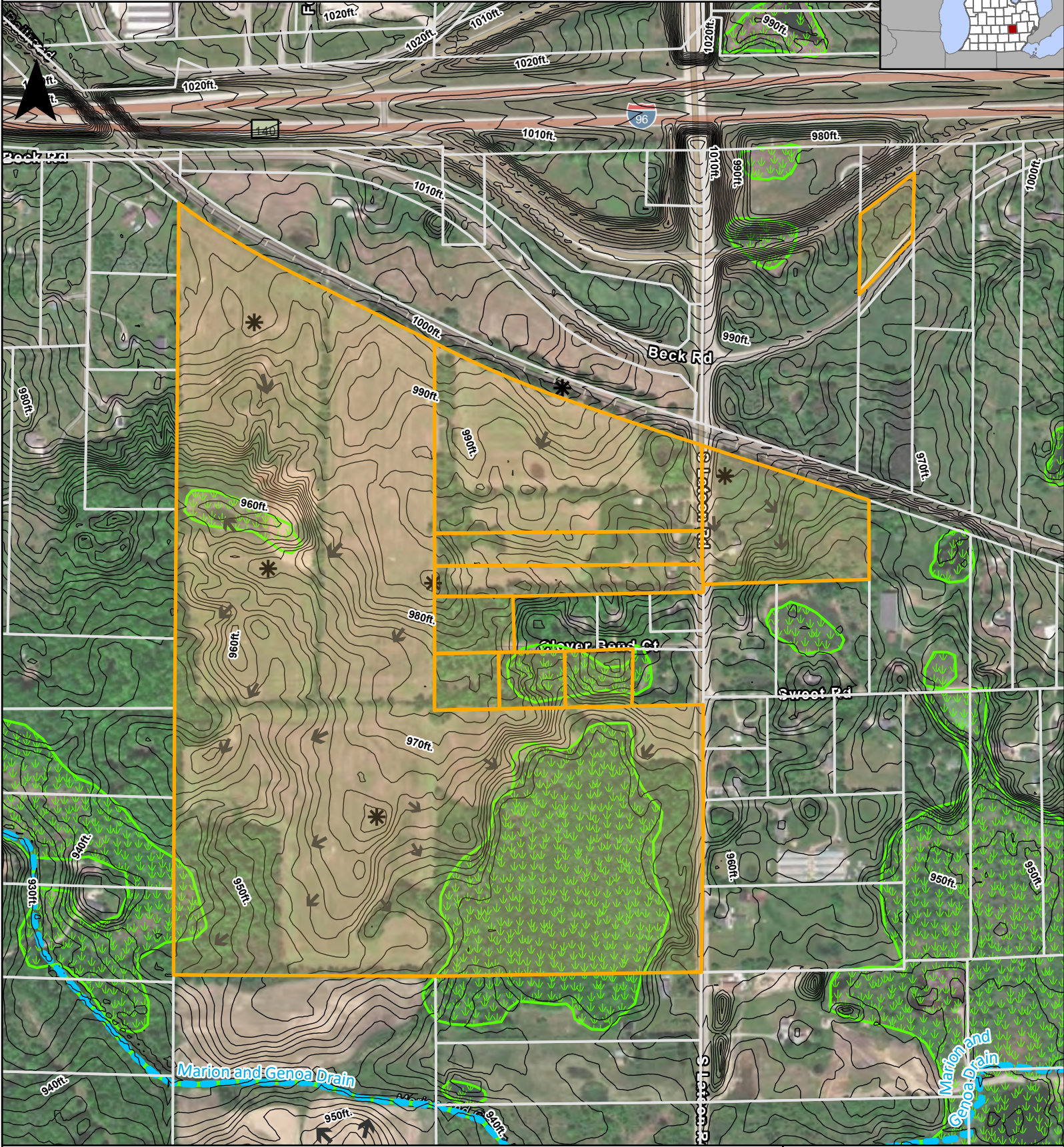
Innovation Interchange PUD  
 Parcel Boundary  
 NWI Wetlands  
 Soil Types



# Innovation Interchange PUD

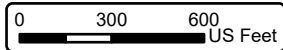
## Topography & Natural Features

Genoa Township, Livingston County, Michigan



- Innovation Interchange PUD
- Parcel Boundary
- \* Local High Point

- NWI Wetlands
- ~ 2 Ft Contour
- ➔ Estimated Flow Direction
- ~ Watercourse NHD



**FIRST AMENDMENT TO PLANNED UNIT DEVELOPMENT AGREEMENT  
(INNOVATION PARK - LATSON ROAD)**

This **First Amendment to Planned Unit Development Agreement** (the “Amendment”) is made as of the \_\_\_ day of \_\_\_\_\_, 2024 (the “Effective Date”), by and between Latson Partners, LLC, Latson Farms, LLC and Covenant of Faith, LLC (collectively, the “Developer”), whose address is 29201 Telegraph Road, Suite 410, Southfield, Michigan 48034, on the one hand, and the Charter Township of Genoa (the “Township”), whose address is 2911 Dorr Road, Brighton, Michigan 48116, on the other hand.

**RECITATIONS**

A. The Developer is the owner of approximately 200 acres of land located on the west and east sides of Latson Road, south of the I-96 expressway, as legally described on **Exhibit A** attached hereto and depicted on the Original Innovation Interchange PUD Parcel Map attached as **Exhibit B** (the “PUD Property”).

B. In order to carry out a proposed long-term development for coordinated, well-planned research, office, light industrial, high tech, commercial with consistent high-quality design standards, natural resource preservation, public amenities and improvements and inter-connectivity of land uses, Developer submitted a request for approval of a planned unit development (“PUD”) and to rezone approximately 177 acres of land located on the west side of Latson Road and another 10 acres on the east side of Latson Road to CAPUD; and



approximately 6 acres of land on the east side of Latson Road and north of the railroad tracks to ICPUD, in accordance with the Township's Master Plan and Article 10 of the Zoning Ordinance and the Michigan Zoning Enabling Act, MCL 125.3101 *et. seq.* (the "PUD Project").

C. After receiving the unanimous recommendations in favor of the PUD Project from the Township's Planning Commission and from Livingston County, the Township Board at its regular meeting held on August 3, 2020, approved the PUD site rezoning, the PUD Plan and execution of a PUD Agreement.

D. The parties entered into a Planned Unit Development Agreement ("PUD Agreement") as of September 30, 2020, which was recorded on October 6, 2020, with the Livingston County Register of Deeds.

E. The Developer, through a related entity Latson Beck, LLC, has acquired approximately 7.94 acres of land located on the east side of Latson Road, south of the I-96 expressway and north of the railroad tracks (Parcel No. 11-09-300-46), as depicted on the Amended Innovation Interchange Parcel Map attached as **Exhibit C** (the "Latson Beck Property"). The Latson Beck Property is Zoned Country Estate (CE) and is master-planned by the Township for "Interchange Commercial" uses. Latson Beck has submitted and is pursuing a request for planned unit development and to rezone the CE Latson Beck Property to ICPUD consistent with the Master Plan's future land use map designation for that land (referred to herein as the "Commercial PUD").

F. As depicted on **Exhibit C**, the Latson Beck Property abuts the approximate 6 acres of land located on the east side of Latson Road and north of the railroad tracks (Parcel Nos. 4711-09-300-040 and 4711-09-300-044), which is currently included in the PUD Project and already zoned ICPUD. The PUD Agreement describes this property as the North Area.

G. The PUD Agreement provides that a portion of North Area (Parcel No. 4711-09-300-044, north of Beck Road) may be used for the erection of a stand-alone project sign (the “Sign Parcel”). The remainder of the North Area (Parcel No. 4711-09-300-040, south of Beck Road) may also be developed, either separately or in combination with adjacent properties which may be acquired by the Developer at a later date, for uses authorized in the ~~ICPUD provisions of the Zoning Ordinance~~ **Master Plan** (PUD Agreement, at ¶ 3.C.)

H. Various commercial uses, including a gas station and hotel, are currently permissible on a portion of the PUD Property consisting of approximately 10.46 acres of land located on the east side of Latson Road and south of the railroad tracks (Parcel No. 4711-09-300-031), as depicted on **Exhibit B**. This land has been designated as the “Accessory Commercial Area” or “East Area” in the PUD Agreement and is zoned CAPUD. The approved uses for this East Area are proposed to be changed as set forth below in this Amendment. The Latson Beck Property, the North Area and Accessory Commercial Area and their relationship to each other are depicted in **Exhibit C** hereto.

**submits**  
I. ~~The Township and Developer agree~~, among other things as provided herein and in the PUD Agreement, that it is logical and appropriate that the North Area within the PUD be developed in conjunction with the adjacent Latson Beck Property within the scope of the **Commercial PUD**. The inclusion of these adjacent ICPUD-zoned properties in one PUD would foster a more integrated and coherent development plan consistent with the ICPUD lands in the Township. Further, certain commercial uses, including gas stations and hotels, will be eliminated as approved land uses for the Accessory Commercial Area.

**NOW, THEREFORE**, in consideration of the foregoing premises, which the ~~Township and~~ Developer represent to be true and accurate, and which shall be incorporated into the parties’

obligations set forth herein, the parties intending to be legally bound by this Amendment, agree as follows:

1. **Removal of a Portion of the North Area from PUD.** The portion of the North Area located south of Beck Road (and excluding the Sign Parcel, which will remain in the Innovation Park PUD), as defined above and depicted in **Exhibit C**, which shall remain zoned ICPUD, is hereby removed from the PUD and ~~shall~~ may be included as land within the Commercial PUD and subject to the Commercial PUD Agreement entered or to be entered by the parties in connection with the Interchange Commercial Property rezoning. None of the provisions of the PUD Agreement, as amended hereby, shall apply to the portion of the North Area removed from the Innovation Park PUD.

2. **Amended PUD Plan.** The Amended PUD Plan attached hereto as **Exhibit D**, which removes the North Area from the PUD, is hereby approved by the Township and shall replace and supersede the PUD Plan attached to the PUD Agreement. All references to the North Area in the PUD Agreement and Exhibits thereto, with the exception of the Sign Parcel, shall be deemed removed and of no force or effect. The total acreage of the PUD land shall hereby be amended to reflect a total of 187 acres as the revised Project Area. The Accessory Commercial Area or East Area shall be designated only as the “East Area” on the Amended PUD Plan, and all references to the Accessory Commercial Area in the PUD Agreement shall be deemed to refer to East Area.

3. **Permitted Uses within the East Area.** The East Area may be developed for any of the uses or combination of uses set forth in **Exhibit E** hereto, which include most of the same uses permitted in the West Area for high-tech, light industrial uses. **Exhibit E** hereto shall entirely replace and supersede Exhibit 5 to the PUD Agreement.

4. **Development Standards for the East Area.** The PUD Design Guidelines attached as Exhibit 8 to the PUD Agreement are hereby amended and restated to reflect, among other things, the elimination of commercial development guidelines for the East Area. The Amended and Restated PUD Design Guidelines are attached as **Exhibit F** hereto and replace entirely Exhibit 8 to the PUD Agreement.

5. **Future Road Improvements.** An updated traffic analysis was undertaken by Flies & Vandenbrink as set forth in the Memo dated June 6, 2024 (attached as **Exhibit G**) to analyze the impact of this Amendment on the original traffic analysis. The updated analysis concluded that this Amendment would generate significantly less vehicle trips as compared to the original Innovation Interchange PUD. As a result, the provisions of the PUD Agreement regarding Future Road Improvements (par. 10) remain the same.

6. **Project Gateway Sign.** The Project gateway sign described in paragraph 7 in the PUD Agreement and depicted in Exhibit 7 thereto has been changed. The revised Project gateway sign as approved by the Planning Commission is reflected on page 13 of the Amended and Restated Design Guidelines attached as **Exhibit F**, which replaces and supersedes Exhibit 7 to the PUD Agreement.

~~7. **Amendment Consistent with Police Powers.** The action of the Township in entering into this Amendment is based upon the understanding that many of the land use, design and environmental objectives of the Township are reflected in the design of the development as proposed and the Township is thus achieving its police power objectives and has not, by this Amendment, bargained away or otherwise compromised any of its police power objectives.~~

8. **Timing of Development of the East Area.** Paragraph 18 of the PUD Agreement shall be amended to remove the sentence—“However, no building shall commence construction

The east area is separate and distinct from the overall campus area and although the uses are now similar, the timing of development should continue to require that the campus development be initiated in the west area before consideration in the east area. The east area should continue to be viewed as an "accessory" to the overall campus park.

The east area is separate and distinct from the overall campus area and although the uses are now similar, the timing of development should continue to require that the campus development be initiated in the west area before consideration in the east area. The east area should continue to be viewed as an "accessory" to the overall campus park.

in the Accessory Commercial Area until at least one building is under construction and proceeded substantially towards completion in the High-Tech Light Industrial Area.” This limitation is no longer applicable. Developer may pursue site planning and development of the East Area for any use or combination of uses authorized in **Exhibit E** hereto at any time, because the East Area is now limited to the interchange campus of the high-tech, light industrial uses also authorized in the West Area.

9. **Effect of Amendment.** Except as specifically revised in this Amendment, all other terms and conditions of the PUD Agreement and Exhibits thereto shall remain in effect and are reaffirmed by the parties hereto.

10. **Relationship of the Parties.** The relationship of the Township and the Developer shall be defined solely by the expressed terms of this Amendment, including the implementing documents described or contemplated herein, and neither the cooperation of the parties hereunder nor anything expressly or implicitly contained herein shall be deemed or construed to create a partnership, limited or general, or joint venture between the Township and the Developer, nor shall any party or their agent be deemed to be the agent or employee of any other party to this Amendment.

11. **Michigan Law to Control.** This Amendment and the rights and obligations of the parties hereunder shall be construed in accordance with Michigan law.

12. **Due Authorization.** The Township and the Developer each warrant and represent to the other that this Amendment and the terms and conditions thereof have been duly authorized and approved by, in the case of the Township, its Board of Trustees, and as to the Developer, by the appropriate officers or members of the companies constituting the Developer,

The removal of land will impact the number of REU's allocated in the original agreement. This must also be addressed in this agreement.

and that the persons who have executed this Amendment below have been duly authorized to do so.

13. **Amendment to Run with the Land; Recording.** This Amendment shall be binding upon and inure to the benefit of the parties to this Amendment and their respective heirs, successors, assigns and transferees, and shall run with the Property. This Amendment shall be recorded by Developer at its expense with the office of the Livingston County Register of Deeds and a copy provided to the Township.

14. **Counterparts.** It is understood and agreed that this Amendment may be executed in several counterparts, each of which, for all purposes, shall be deemed to constitute an original and all of which counterparts, when taken together, shall be deemed to constitute one and the same agreement, even though all of the parties hereto may not have executed the same counterpart. Delivery via facsimile or PDF transmission of a counterpart of this Amendment as executed by the parties making such delivery shall constitute good and valid execution and delivery of this Amendment for all purposes.

**IN WITNESS WHEREOF**, the parties hereto have executed this Amendment on the date first set forth above.

*[Signatures on following pages]*

“DEVELOPER”

Latson Partners, LLC  
a Michigan limited liability company

By: \_\_\_\_\_  
Todd Wyett  
Its: Manager

STATE OF MICHIGAN     )  
                                          ) ss.  
COUNTY OF OAKLAND    )

The foregoing instrument was acknowledged before me this \_\_\_\_\_ day of \_\_\_\_\_, 2024, by Todd Wyett, the Manager of Latson Partners, LLC, a Michigan limited liability company, on behalf of the company.

\_\_\_\_\_  
Notary Public  
\_\_\_\_\_ County, Michigan  
Acting in \_\_\_\_\_ County, Michigan  
My Commission Expires: \_\_\_\_\_

“DEVELOPER”

Latson Farms, LLC  
a Michigan limited liability company

By: \_\_\_\_\_  
Todd Wyett  
Its: Manager

STATE OF MICHIGAN     )  
                                          ) ss.  
COUNTY OF OAKLAND    )

The foregoing instrument was acknowledged before me this \_\_\_\_\_ day of \_\_\_\_\_, 2024, by Todd Wyett, the Manager of Latson Farms, LLC, a Michigan limited liability company, on behalf of the company.

\_\_\_\_\_  
Notary Public  
\_\_\_\_\_ County, Michigan  
Acting in \_\_\_\_\_ County, Michigan  
My Commission Expires: \_\_\_\_\_



“DEVELOPER”

Covenant of Faith, LLC  
a Michigan limited liability company

By: \_\_\_\_\_  
Todd Wyett  
Its: Manager

STATE OF MICHIGAN     )  
                                          ) ss.  
COUNTY OF OAKLAND    )

The foregoing instrument was acknowledged before me this \_\_\_\_\_ day of \_\_\_\_\_, 2024, by Todd Wyett, the Manager of Covenant of Faith, LLC, a Michigan limited liability company, on behalf of the company.

\_\_\_\_\_  
Notary Public  
\_\_\_\_\_ County, Michigan  
Acting in \_\_\_\_\_ County, Michigan  
My Commission Expires: \_\_\_\_\_

“TOWNSHIP”

**GENOA TOWNSHIP,**  
a Michigan municipal corporation

By: \_\_\_\_\_  
Its: Supervisor

STATE OF MICHIGAN        )  
                                          ) ss.  
COUNTY OF LIVINGSTON )

The foregoing instrument was acknowledged before me this \_\_\_\_\_ day of \_\_\_\_\_, 2024, by \_\_\_\_\_, Supervisor of Genoa Township, a Michigan municipal corporation, on behalf of the corporation.

\_\_\_\_\_  
Notary Public  
Livingston County, Michigan  
Acting in Livingston County, Michigan  
My Commission Expires: \_\_\_\_\_

and

By: \_\_\_\_\_  
Its: Clerk

STATE OF MICHIGAN        )  
                                          ) ss.  
COUNTY OF LIVINGSTON )

The foregoing instrument was acknowledged before me this \_\_\_\_\_ day of \_\_\_\_\_, 2024, by \_\_\_\_\_, Clerk of Genoa Township, a Michigan municipal corporation, on behalf of the corporation.

\_\_\_\_\_  
Notary Public  
Livingston County, Michigan  
Acting in Livingston County, Michigan  
My Commission Expires: \_\_\_\_\_

Drafted by and when recorded return to:

Alan M. Greene, Esq.  
Dykema Gossett PLLC  
39577 Woodward Avenue, Suite 300  
Bloomfield Hills, MI 48304

## **INDEX OF EXHIBITS**

- A. Legal Descriptions of Original PUD Properties
- B. Original Innovation Interchange PUD Parcel Map
- C. Amended Innovation Interchange PUD Parcel Map
- D. Amended PUD Plan
- E. Permitted Uses
- F. Amended and Restated Design Guidelines
- G. Traffic Memo dated June 6, 2024

**EXHIBIT A**  
(Legal Descriptions of Original PUD Properties)

THE WEST 1/2 OF THE SOUTHEAST 1/4, LYING SOUTH OF THE C&O RAILROAD RIGHT-OF-WAY, SECTION 8, TOWN 2 NORTH, RANGE 5 EAST, GENOA TOWNSHIP, LIVINGSTON COUNTY, MICHIGAN, AND THE NORTH 1/2 OF THE NORTHEAST 1/4 OF SECTION 17, TOWN 2 NORTH, RANGE 5 EAST, GENOA TOWNSHIP, LIVINGSTON COUNTY, MICHIGAN, MORE PARTICULARLY DESCRIBED AS FOLLOWS:

BEGINNING AT THE SOUTHEAST CORNER OF SAID SECTION 8, ALSO BEING THE NORTHEAST CORNER OF SAID SECTION 17; THENCE ALONG THE EAST LINE OF SAID SECTION 17, SOUTH 01 DEGREES 56 MINUTES 14 SECONDS EAST, 1,327.79 FEET; THENCE ALONG THE EAST-WEST 1/8 LINE OF SAID SECTION 17, SOUTH 87 DEGREES 03 MINUTES 13 SECONDS WEST, 2,593.52 FEET; THENCE ALONG THE NORTH-SOUTH 1/4 LINE OF SAID SECTION 17, NORTH 01 DEGREES 43 MINUTES 52 SECONDS WEST, 1,325.61 FEET TO THE NORTH 1/4 CORNER OF SAID SECTION 17, ALSO BEING THE SOUTH 1/4 CORNER OF SAID SECTION 8; THENCE ALONG THE NORTH-SOUTH 1/4 LINE OF SAID SECTION 8, NORTH 01 DEGREES 35 MINUTES 01 SECONDS WEST, 2,485.28 FEET; THENCE ALONG THE SOUTHERLY RIGHT-OF-WAY LINE OF SAID C&O RAILROAD, ALONG THREE (3) COURSES: 1) SOUTHEASTERLY ON AN ARC LEFT, HAVING A LENGTH OF 558.30 FEET, A RADIUS OF 3,135.50 FEET, A CENTRAL ANGLE OF 10 DEGREES 12 MINUTES 07 SECONDS AND A LONG CHORD WHICH BEARS SOUTH 59 DEGREES 26 MINUTES 23 SECONDS EAST, 557.56 FEET; 2) SOUTHEASTERLY ON AN ARC LEFT, HAVING A LENGTH OF 375.08 FEET, A RADIUS OF 19,734.50 FEET, A CENTRAL ANGLE OF 01 DEGREES 05 MINUTES 20 SECONDS AND A LONG CHORD WHICH BEARS SOUTH 65 DEGREES 05 MINUTES 07 SECONDS EAST, 375.07 FEET; 3) SOUTH 65 DEGREES 37 MINUTES 47 SECONDS EAST, 538.12 FEET; THENCE ALONG THE NORTH-SOUTH 1/8 LINE OF SAID SECTION 8, SOUTH 01 DEGREES 39 MINUTES 42 SECONDS EAST, 1,753.86 FEET; THENCE ALONG THE SOUTH LINE OF SAID SECTION 8, ALSO BEING THE NORTH LINE OF SAID SECTION 17, NORTH 87 DEGREES 00 MINUTES 12 SECONDS EAST, 1,294.40 FEET TO THE POINT OF BEGINNING, CONTAINING 141.059 ACRES AND SUBJECT TO ALL MATTERS AND EASEMENTS OF RECORD.

PARCEL # 11-17-200-008

LEGAL DESCRIPTION:

PART OF THE SOUTHEAST 1/4 OF SECTION 8, TOWN 2 NORTH, RANGE 5 EAST, GENOA TOWNSHIP, LIVINGSTON COUNTY, MICHIGAN. DESCRIBED AS COMMENCING AT THE SOUTHEAST CORNER OF SAID SECTION 8; THENCE ALONG THE EAST LINE OF SAID SECTION, N 01° 46' 00" E 505.00' TO THE POINT OF BEGINNING; THENCE N 89° 29' 06" W 1293.80'; THENCE N 01° 51' 05" E 168.41'; THENCE S 89° 29' 06" E 1293.55'; THENCE S 01° 46' 00" W 168.40' TO THE POINT OF BEGINNING CONTAINING 5.00 ACRES.

PARCEL # 11-08-400-004

LEGAL DESCRIPTION:

PART OF THE SOUTHEAST 1/4 OF SECTION 8, TOWN 2 NORTH, RANGE 5 EAST, GENOA TOWNSHIP, LIVINGSTON COUNTY, MICHIGAN. DESCRIBED AS COMMENCING AT THE SOUTHEAST CORNER OF SAID SECTION 8; THENCE ALONG THE EAST LINE OF SAID SECTION, N 01° 46' 00" E 673.40' TO THE POINT OF BEGINNING; THENCE N 89° 29' 06" W 1293.55'; THENCE N 01° 51' 05" E 168.44'; THENCE S 89° 29' 06" E 1293.30'; THENCE S 01° 46' 00" W 168.43' TO THE POINT OF BEGINNING CONTAINING 5.00 ACRES.

PARCEL # 11-08-400-006



LEGAL DESCRIPTION:

PART OF THE SOUTHEAST 1/4 OF SECTION 8, TOWN 2 NORTH, RANGE 5 EAST, GENOA TOWNSHIP, LIVINGSTON COUNTY, MICHIGAN. DESCRIBED AS COMMENCING AT THE SOUTHEAST CORNER OF SAID SECTION 8, THENCE ALONG THE EAST LINE OF SAID SECTION 8, N 01° 46' 00" E 252.46'; THENCE N 89° 29' 06" W 899.00' TO THE POINT OF BEGINNING; THENCE N 89° 29' 06" W 395.17'; THENCE N 01° 51' 05" E 252.55'; THENCE S 89° 29' 06" E 394.80'; THENCE S 01° 46' 00" W 252.54' TO THE POINT OF BEGINNING CONTAINING 2.289 ACRES. SUBJECT TO AND INCLUDING THE USE OF A PRIVATE ROAD EASEMENT, THE CENTERLINE OF WHICH IS DESCRIBED BELOW.

PRIVATE ROAD EASEMENT: A 66 FOOT WIDE STRIP OF LAND LOCATED IN THE SOUTHEAST 1/4 OF SECTION 8, TOWN 2 NORTH, RANGE 5 EAST, GENOA TOWNSHIP, LIVINGSTON COUNTY, MICHIGAN FOR PURPOSE OF INGRESS AND EGRESS AND THE LOCATION OF PUBLIC AND PRIVATE UTILITIES, THE CENTERLINE OF WHICH IS DESCRIBED AS FOLLOWS: BEGINNING AT A POINT ON THE EAST LINE OF SAID SECTION 8, DISTANT N 01° 46' 00" E 252.46' FROM THE SOUTHEAST 1/4 CORNER OF SAID SECTION 8; THENCE N 89° 29' 06" W 970.35' TO THE CENTER OF A 75 FOOT RADIUS TURNAROUND; THENCE CONTINUING N 89° 29' 06" W 323.82' TO THE POINT OF ENDING.

PARCEL # 11-08-400-012

LEGAL DESCRIPTION:

PART OF THE SOUTHEAST 1/4 OF SECTION 8, TOWN 2 NORTH, RANGE 5 EAST, GENOA TOWNSHIP, LIVINGSTON COUNTY, MICHIGAN. DESCRIBED AS COMMENCING AT THE SOUTHEAST CORNER OF SAID SECTION 8, THENCE ALONG THE SOUTH LINE OF SAID SECTION 8, N 89° 29' 06" W 323.45' TO THE POINT OF BEGINNING; THENCE CONTINUING ALONG THE SOUTH LINE OF SAID SECTION 8, N 89° 29' 06" W 971.01'; THENCE N 01° 51' 05" E 252.47'; THENCE S 89° 29' 06" E 970.72'; THENCE S 01° 47' 14" W 252.46' TO THE POINT OF BEGINNING CONTAINING 5.626 ACRES. SUBJECT TO AND INCLUDING THE USE OF A PRIVATE ROAD EASEMENT, THE CENTERLINE OF WHICH IS DESCRIBED BELOW.

PRIVATE ROAD EASEMENT: A 66 FOOT WIDE STRIP OF LAND LOCATED IN THE SOUTHEAST 1/4 OF SECTION 8, TOWN 2 NORTH, RANGE 5 EAST, GENOA TOWNSHIP, LIVINGSTON COUNTY, MICHIGAN FOR PURPOSE OF INGRESS AND EGRESS AND THE LOCATION OF PUBLIC AND PRIVATE UTILITIES, THE CENTERLINE OF WHICH IS DESCRIBED AS FOLLOWS: BEGINNING AT A POINT ON THE EAST LINE OF SAID SECTION 8, DISTANT N 01° 46' 00" E 252.46' FROM THE SOUTHEAST 1/4 CORNER OF SAID SECTION 8; THENCE N 89° 29' 06" W 970.35' TO THE CENTER OF A 75 FOOT RADIUS TURNAROUND; THENCE CONTINUING N 89° 29' 06" W 323.82' TO THE POINT OF ENDING.

PARCELS # 11-08-400-013 + 11-08-400-014 + 11-08-400-015

LEGAL DESCRIPTION:

PART OF THE SOUTHEAST 1/4 OF SECTION 8, TOWN 2 NORTH, RANGE 5 EAST, GENOA TOWNSHIP, LIVINGSTON COUNTY, MICHIGAN. DESCRIBED AS COMMENCING AT THE SOUTHEAST CORNER OF SAID SECTION 8; THENCE ALONG THE EAST LINE OF SAID SECTION 8 N 01° 46' 00" E 841.83' TO THE POINT OF BEGINNING; THENCE N 89° 29' 06" W 1293.30'; THENCE N 01° 51' 05" E 912.72' TO A POINT ON THE SOUTHERLY RAILROAD RIGHT-OF-WAY LINE (AS CONSTRUCTED); THENCE ALONG THE SAID SOUTHERLY RAILROAD RIGHT-OF-WAY LINE THE FOLLOWING THREE COURSES:

- (1) S 62° 03' 36" E 88.07'
- (2) CURVE TO THE LEFT 527.26', RADIUS = 4000.00', CENTRAL ANGLE = 07° 33' 09",  
CHORD BEARING AND LENGTH S 65° 50' 11" E 526.88'
- (3) S 69° 36' 45" E 765.54'

TO A POINT ON THE EAST LINE OF SAID SECTION 8; THENCE ALONG THE EAST LINE OF SAID SECTION 8 S 01° 46' 00" W 400.43' TO THE POINT OF BEGINNING CONTAINING 18.9707 ACRES (826,362 S.F.) AND SUBJECT TO THE RIGHTS OF THE PUBLIC IN NIXON ROAD TAKEN OR DEEDED FOR ROAD PURPOSES AND SUBJECT TO ALL MATTERS AND EASEMENTS OF RECORD.

PARCEL # 11-08-400-020

LEGAL DESCRIPTION:

LAND SITUATED IN THE TOWNSHIP OF GENOA, COUNTY OF LIVINGSTON, STATE OF MICHIGAN, DESCRIBED AS:

A PART OF THE WEST 1/2 OF THE SOUTHWEST 1/4 OF SECTION 9 , TOWN 2 NORTH, RANGE 5 EAST, GENOA TOWNSHIP, LIVINGSTON COUNTY, MICHIGAN, DESCRIBED AS FOLLOWS: BEGINNING AT A POINT ON THE CENTERLINE OF NIXON ROAD 542.26 FEET NORTH 1° 46' EAST FROM THE SOUTHWEST CORNER OF SAID SECTION 9; THENCE NORTH 1° 46' EAST 700 FEET ALONG THE CENTERLINE OF NIXON ROAD TO THE SOUTHERLY RIGHT OF WAY LINE OF THE C & O RAILROAD; THENCE SOUTH 69° 38' 45" EAST 870.11 FEET ALONG THE SOUTHERLY RIGHT OF WAY LINE OF THE C & O RAILROAD; THENCE SOUTH 1° 46' WEST 395.96 FEET; THENCE SOUTH 89° 56' 30" WEST 825 FEET TO THE POINT OF BEGINNING.  
AREA: 10.3732 ACRES.

PARCEL # 11-09-300-031

Land situated in the Township of Genoa, County of Livingston, State of Michigan, described as follows:

A part of the West 1/2 of the Southwest 1/4, Section 9, Town 2 North, Range 5 East, described as follows: commencing at a point on the centerline of Beck Road and the East and West 1/4 line 768.87 feet East of the West 1/4 of said Section 9; thence East 262.03 feet along the centerline of Beck Road and the East and West 1/4 line; thence South 01 degree 46 minutes West 1711.99 feet to the Northerly right-of-way line of the C & O Railroad; thence North 69 degrees 36 minutes 45 seconds West 276.36 feet along the Northerly right-of-way line of the C & O Railroad; thence North 01 degrees 46 minutes East 1615.68 feet to the point of beginning.

Tax Item No.: 11-09-300-001

**EXHIBIT B**

(Original Innovation Interchange PUD Parcel Map)

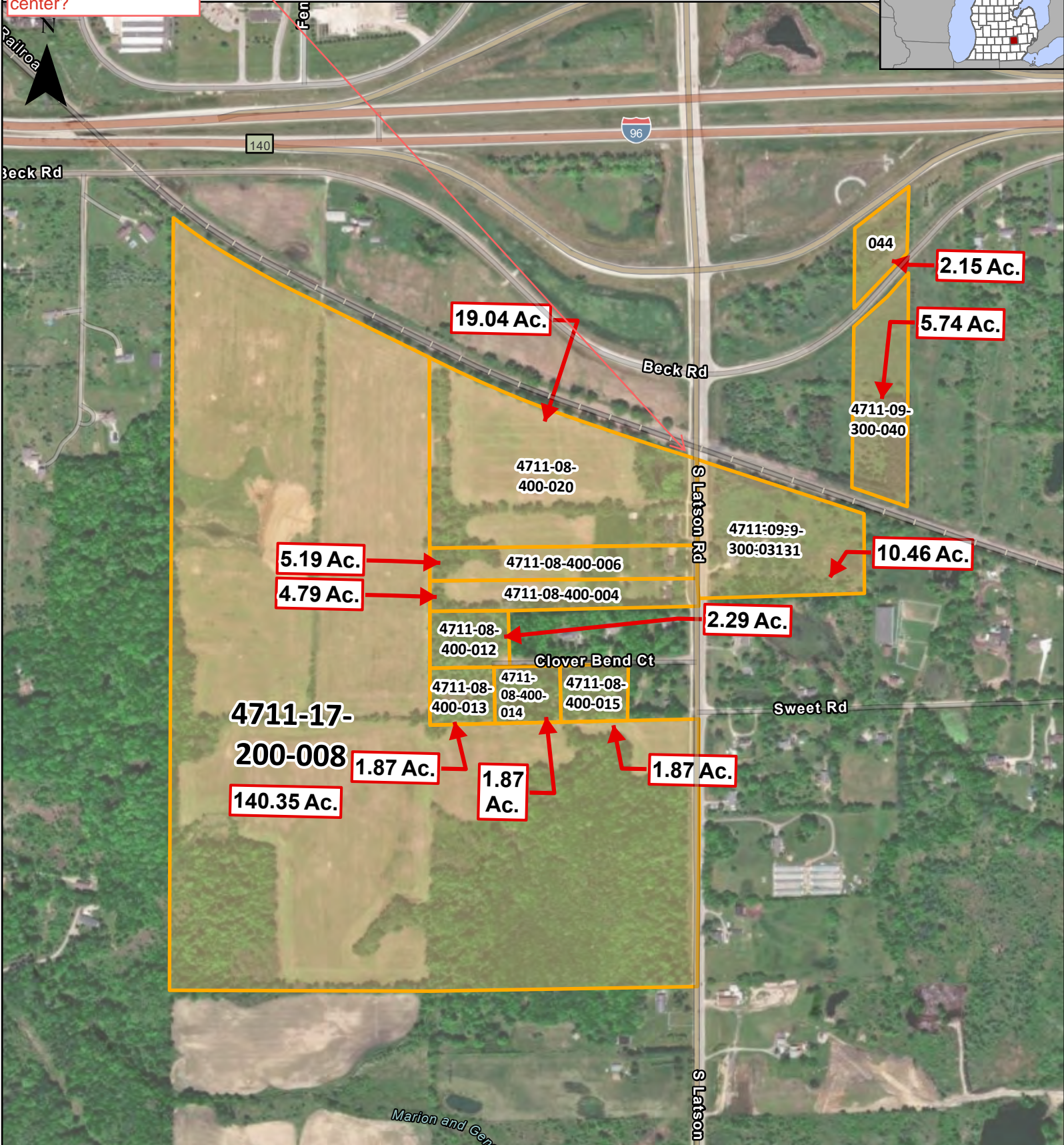
# Original Innovation Interchange PUD Parcels

## Parcel Exhibit Map

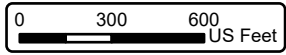
Genoa Township, Livingston County, Michigan



Are these property lines accurate? Do they extend to the road center?



Original Innovation Interchange PUD



**EXHIBIT C**

(Amended Innovation Interchange PUD Parcel Map)



# Amended Innovation Interchange PUD

## Parcel Exhibit Map

Genoa Township, Livingston County, Michigan



Are these property lines accurate? Do they extend to the road center?



19.04 Ac.

2.15 Ac.

044

5.19 Ac.

4.79 Ac.

10.46 Ac.

2.29 Ac.

1.87 Ac.

1.87 Ac.

1.87 Ac.

140.35 Ac.

4711-08-400-020

4711-08-400-006

4711-08-400-004

4711-09-300-031

4711-08-400-012

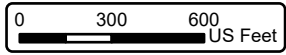
4711-08-400-013

4711-08-400-014

4711-08-400-015

4711-17-200-008

Amended  
Innovation  
Interchange PUD



**EXHIBIT D**  
(Amended PUD Plan)

**OVERALL PLAN**

THE APPROVED PUD CONCEPT INCLUDED A 15' MEDIAN GRAPHIC AND A 30' MEDIAN GRAPHIC. THIS CONCEPT PLAN ELIMINATES THE 30' MEDIAN OPTION AND REPLACES IT WITH A NO MEDIAN OPTION.



**Option 1:**  
15 ft median

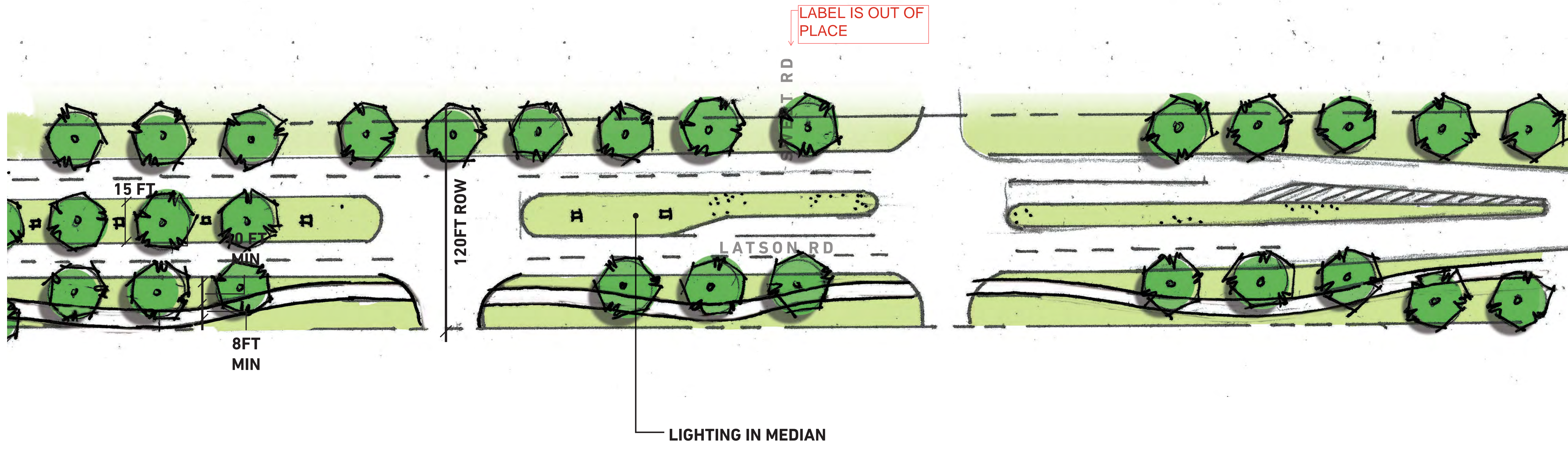
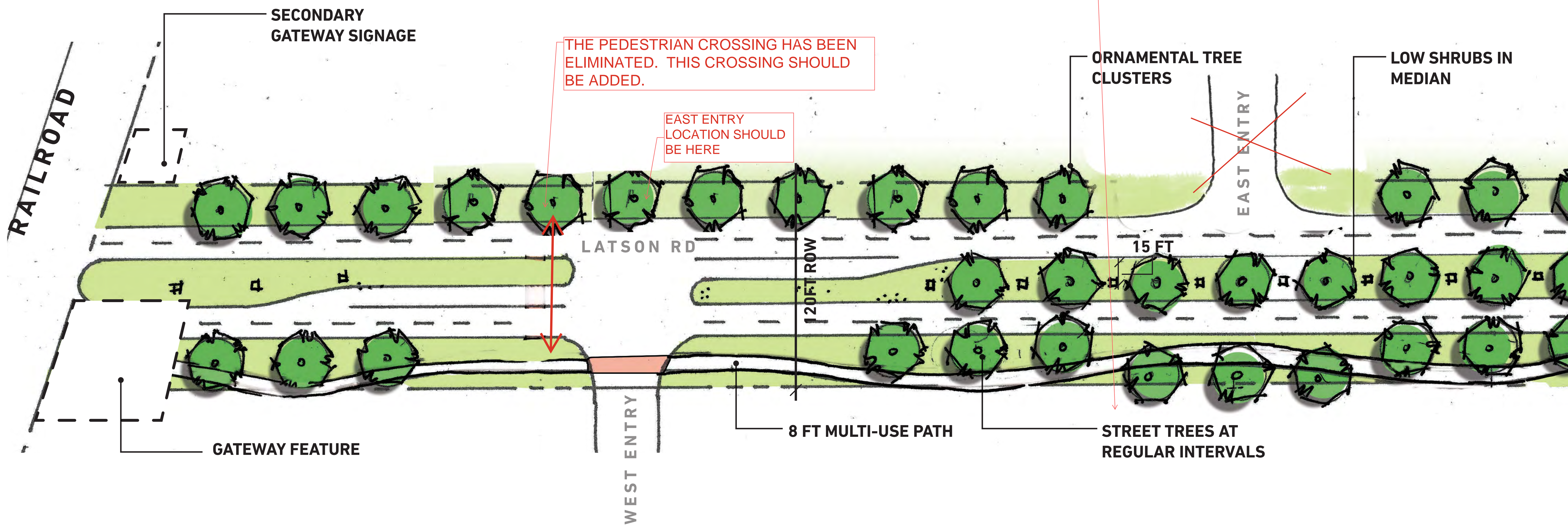
**Option 2:**  
No median



LATSON ROAD LANDSCAPE : Option 1

THIS WAS FORMERLY THE 30' MEDIAN PLAN

APPROVED PLAN INDICATED STREET TREE SPACING EVERY 50 FEET.

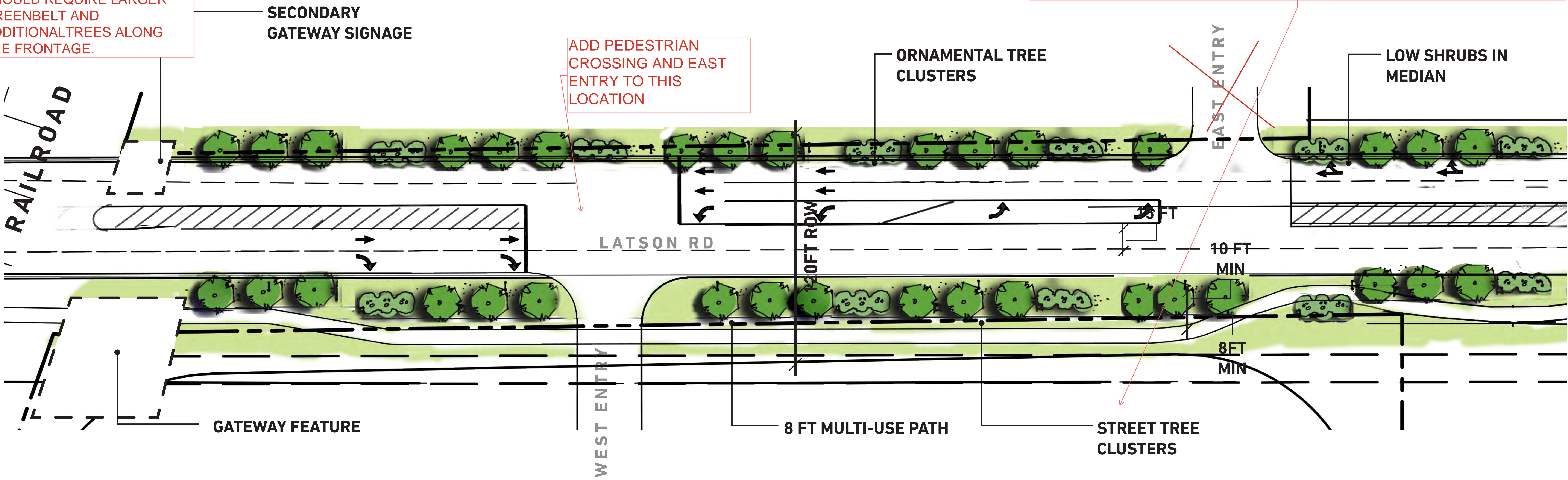


# LATSON ROAD LANDSCAPE : Option 2

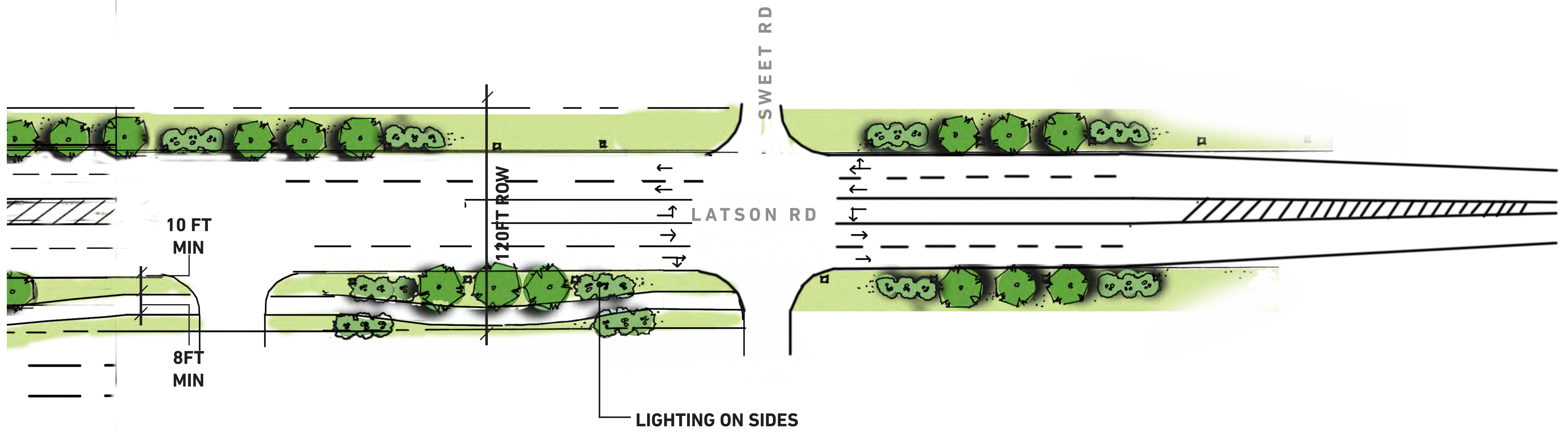
THIS WAS FORMERLY A PLAN FOR A 15' MEDIAN. THIS PROPOSES TO ELIMINATE THE 30' MEDIAN PLAN.

THIS PLAN ELIMINATES TREES SPACED AT REGULAR INTERVALS AND PROPOSES CLUSTERS OF TREES. THE REGULARLY SPACED TREE INTERVALS SHOULD BE REQUIRED.

ELIMINATION OF THE MEDIAN SHOULD REQUIRE LARGER GREENBELT AND ADDITIONAL TREES ALONG THE FRONTAGE.



10' 130' 140' 190' ⊕



10' 130' 140' 190' ⊕

**EXHIBIT E**  
(Permitted Uses)

**VERSA PUD: Permitted Land Uses in Innovation Interchange**

**Business Park**

P= Permitted; S= Special Land Use

Types of Uses (see also regulation by size as noted at the bottom of the table)	West of Latson	East of Latson
<b>OFFICE, RESEARCH &amp; DEVELOPMENT, LIGHT INDUSTRIAL</b>		
Offices, including: executive, medical, administrative, and professional, including architecture, planning, and engineering	P	P
Conference Centers	P	P
Multimedia production facilities	P	P
Corporate and technical education and training facilities	P	P
Data processing and computer centers, including computer programming and software development, training, and service of electronic data processing equipment	P	P
Research and Development, Pilot or Experimental Product Development	P	P
Distribution facilities, air freight forwarders, expediting and delivery services, and warehousing establishments, including wholesale trade (includes whole sale and industrial distributors, warehousing, freight forwarders, wholesale assemblers) if located at least 500 feet from Latson Road	P	-
Distribution and other facilities listed above when within 500 feet of Latson Road	S	-
Light industrial as defined in the Genoa Township Zoning Ordinance	P	P
<b>MEDICAL</b>		
Hospitals, medical urgent care facilities/centers/clinics, medical research facilities, diagnostic, optical, and pharmaceutical and other laboratories	P	P
<b>USES PERMITTED ONLY WHEN ACCESSORY TO A MEDICAL USE</b>		
Educational facilities for training of interns, nurses, and allied health care personnel	P	P
Multiple family housing for use by physicians, interns, nurses, allied health personnel and their families	P	S
Ambulance service and maintenance facilities	P	-
Helipads, heliports, and helistops	S	-
Accessory mobile medical technology unit	P	P
<b>OTHER</b>		
Hotels	P	P
Day care centers	P	P
Pet Day Care and overnight boarding	P	S
Indoor recreation facilities, health clubs, and studios	P	P
<b>OTHER USES, ACCESSORY USES</b>		
Public facilities and uses to serve the district including police, fire, EMS, public utilities, and communications	P	P
Accessory Outdoor storage of materials used in the operation of the Principal Use screened from view along public roads or the expressway	S	- S
Accessory parking of vehicles, trucks, trailers and equipment. Any parking of semi-trailers or trucks of more than 24 hours is prohibited in the front yard. Area of parking must be shown on the site plan and specify screening from view.	P	- S
Accessory buildings and accessory uses customarily incidental to any of the above principal uses permitted; however, accessory uses shall not exceed 50% of the gross building area (e.g., general office, child care, food service, health/workout rooms intended for use by employees, not the general public).	P	P
<b>SIZE RESTRICTIONS</b>		
Any permitted use over 200,000 square feet	P	S NA
Any permitted use over 40,000 square feet	P	NA S
Uses similar to, and compatible with, other permitted uses and not listed as Prohibited, as determined by the Planning Commission	P	or S P or S
1 No truck parking can be located in the front. Truck parking that abuts the south or east side must increase the amount of plant materials or their sizes by 30% to provide an effective year round screening.		



P ("S" OVER 45 FEET OR 3 STORIES)

Consider Special Land use for Hotel over 45 feet or 3 stories given proximity to residential and inclusion of hotel to the north

Consider prohibition on outdoor storage in east area.

**VERSA PUD: Prohibited Uses (applies throughout the project)**

Types of Uses
Manufacture of automobiles and bodies, trucks, engines, batteries, etc.
Asphalt, cement, concrete, batching or paving plants
Auto service/repair
Blast furnace, steel furnace, blooming or rolling mill; smelting of copper, iron, or zinc ore
Painting, sheet metal and welding shops, metal and plastic molding and extrusion shops
Production, refining, storage of petroleum and other flammable or combustible materials
Deep well injection of hazardous waste or non-hazardous waste
Incineration of garbage or refuse
Junk yards and salvage yards
Hazardous waste recycling, incineration, treatment, transfer, storage or disposal
Non-hazardous waste transfer stations, treatment, storage or disposal facilities
Sludge composting
Truck Terminals
Truck driving schools
Lumber and planning mills
Metal plating, buffing, and polishing
Sheet metal stamping operations
Commercial kennels
Storage facilities for building materials, sand, gravel, stone, lumber, open storage for construction contractor's equipment and supplies
Truck Stops
Mini or Self Storage Warehouse
Laundry, dry-cleaning establishments or pick-up stations

Approved PUD included as Prohibited use: "Accessory Uses - Accessory outdoor storage of raw materials, supplies, equipment, and products..." This should be added back in.

planing

or facility

**EXHIBIT F**  
(Amended and Restated Design Guidelines)



# INNOVATION INTERCHANGE

## PUD DESIGN GUIDELINES AMENDMENT

GENOA TOWNSHIP, MICHIGAN

**OVERVIEW**

Generally, the design of the innovation Interchange Planned Unit Development will follow the standards described in the Genoa Township Zoning Ordinance and the applicable specifications of other agencies involved in the approval process. These guidelines are considered as a supplement to those standards. Generally, the more restrictive standard between the Zoning Ordinance and these guidelines will apply.

Some of the standards herein are more restrictive, such as certain landscape and lighting specifications. In other cases, the dimensional standards in the guidelines are more generous than the ordinance would otherwise allow, as permitted by the "Flexibility in Design" provisions in Section 10.01.03 of the PUD Article.

A comparison of existing zoning ordinance standards to the PUD is shown on the table on the next page.

**GENOA TOWNSHIP, MI**



**DESIGN GUIDELINES  
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ZONE DESIGN GUIDELINES**.....8

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**PART 4: OPEN SPACE CONCEPTS AND  
REPRESENTATIVE AMENITIES**.....11

Todd Wyett  
Versa RE  
29201 Telegraph Road Suite 410  
Southfield, MI, 48034

**PROJECT TEAM:**

## HIGH TECH/LIGHT INDUSTRIAL DEVELOPMENT INTENT

These guidelines are intended to illustrate the design quality anticipated with the commercial and light industrial portions of the PUD. The "Owner" of the PUD or subsequent purchaser of land will be responsible for providing these guidelines to design professionals who will be involved in the preparation of site plans. Specific compliance will be described in more detail with a site plan that will be submitted to the Township for approval.

In general these guidelines include the following components:

1. A description of architecture supplemented with photographs from similar developments to illustrate the general outcomes expected consistent with the standards to support a deviation from the Township's standards that would otherwise apply.
2. Specific parking requirements associated with the intended uses along with a provision to permit a reduction for shared parking when uses have different peak parking occupancy hours.
3. Efforts to share access to reduce the number of driveways and provide good traffic operations along Latson Road.
4. Provision of additional height for modern-style light industrial and R+D buildings, and a hotel, up to 4 stories or 5 stories as a Special Land Use (in conjunction with setbacks from existing single family homes as illustrated on an exhibit).
5. Some flexibility in the building setbacks.
6. An overall open space concept plan with representative amenities.
7. A greenbelt along Latson Road that exceeds Genoa Township requirements and plant sizes that are larger than required at installation.
8. A reduction in street trees along the internal industrial streets, but provisions for a variety of street tree species.
9. Additional lighting standards to reduce lighting impacts on adjacent homes to the west.
10. Provision for three project entry signs, one at each entrance. These signs may include name plates for major buildings or businesses in the PUD.
11. Allowance for a project identification sign visible to traffic along I-96. The height and design shall be negotiated with Genoa Township.

COMMERCIAL USES WERE REMOVED. DOES THIS SECTION NEED TO BE HERE.

CONSIDER LIMIT ON HOTEL HEIGHT IN THE EAST AREA SINCE A HOTEL IS PROPOSED TO BE ALLOWED TO THE NORTH AND A LOWER HEIGHT SHOULD BE CONSIDERED TO TRANSITION TO THE ADJACENT RESIDENTIAL.

THERE ARE DUPLICATE OPTIONS HERE. CLARIFY THE STANDARD TO KEEP ONLY THE LARGER SIZES.

The following table provides a comparison summary between the zoning requirements of the Genoa Township Zoning Ordinance and the proposed Versa PUD standards. The standards listed here provide a snapshot of where there are differences between the Township's standards and the PUD standards, including for setbacks, height, and landscaping requirements.

ZONING COMPARISON TABLE		
Existing Zoning Requirements	PUD Standards:	
Commercial	<b>Setbacks</b> Regional Commercial Side Yard: 20 feet	Side Yard: 20 feet for each side plus an additional 0.5 feet per foot of height over 45 feet tall
	<b>Maximum Height</b> Regional Commercial: 45 feet or 3 stories	All other uses in commercial: 45 feet, 3 stories Hotel: 57 feet or 4 stories, whichever is less*
Industrial	<b>Existing Zoning Requirements</b> <b>Setbacks</b> Front Yard: 85 feet if parking is located in the front yard; 50 feet if no parking is located in the front yard	Front Yard: 85 feet (50 feet if no parking is located in the front yard and/or building height is 30 feet or less)
	Side Yard: 25 feet (or 50 feet if adjacent to residential)	Side Yard: 25 feet (or 50 feet if adjacent to residential) & 25 plus an additional 0.5 feet per foot of height over 30 feet (if not adjacent to residential)
	<b>Maximum Height</b> 30 feet or 2 stories	All other uses in industrial: 55 feet or 3 stories, whichever is less <b>HOTEL IN EAST AREA - 45 FEET OR 3 STORIES*</b> Hotel: 57 feet or 4 stories, whichever is less*
Landscaping	<b>Existing Zoning Requirements</b> <b>Frontage - Greenbelt along Latson Road</b> Minimum Width of Greenbelt: 20 feet with one canopy tree planted every 40 feet of frontage	Minimum width of Greenbelt: 30 feet with one canopy tree planted for every 40 feet of frontage
	<b>Frontage - Tree Sizes</b> Minimum Required Plant Sizes: Deciduous Canopy Tree: 2.5" caliper Deciduous Ornamental Tree: 2" caliper Evergreen Tree: 6' height Deciduous Shrub: 2' height Upright Evergreen Shrub: 2' height Spreading Evergreen Shrub: 18" - 24" spread	Minimum Required Plant Sizes (along Latson Road only): Deciduous Tree: 3-4 inch caliper (with minimum average size of 3.5 inches) Ornamental Tree: 2.5 - 3.5 inch caliper Evergreen Tree: 10 - 14 feet tall (with minimum average size of 12 feet tall) Shrubs and Hedges: 30-36 inches tall Canopy Tree: 2.5 inch caliper Deciduous Ornamental Tree: 2 inch caliper Evergreen Tree: 6 feet height Deciduous Shrub: 2 feet height Upright Evergreen Shrub: 2 feet height Spreading Evergreen Shrub: 18 inch - 24 inch spread
	<b>Other</b>	See Design Guidelines for additional standards related to: Parking Lighting Architecture Signs (currently no off-premise signs are permitted, this PUD proposes some with specific guidelines)

\*The Hotel may be increased to 65 feet or 5 stories, provided minimal distance from adjacent residential home is 500 feet and the Township determines the design is compatible with residential in the area in terms of views and lighting

CONSIDER IMPROVED FACADE STANDARDS IN THE EAST AREA

CONSIDER SPLIT FACE BLOCK ONLY AND REMOVAL OF PRE-CAST CONCRETE.

CONSIDER PROHIBITION ON OUTDOOR STORAGE IN EAST AREA

## HIGH TECH/LIGHT INDUSTRIAL DESIGN GUIDELINES

The primary purpose of the building design standards is to promote and enforce high-quality architectural design for building sides visible from Latson Road to enhance the Township's entryway from the I-96 interchange. The design and materials on building sides visible from the interior roads are not required to meet the more stringent standards but should still utilize some of these elements to promote an attractive appearance. Building along the "north edge" shall also meet the guidelines described on page 10.

### A. Facade Plane and Material Delineation

- **Horizontal delineation.** Long lengths of building facade wall planes shall be broken up using different materials and offset of planes, to serve as a visual breakup of long exterior walls. The following criteria shall be applied to the horizontal plane of walls with a minimum building length of 100 feet:
  - » Buildings with frontages 100 feet to 500 feet in length
    - Require a major material change at a rate of 1.5 times the height of the building.
    - Require a shift in wall façade a minimum of 2 feet in dimension every 40 feet.
  - » Buildings with frontages over 500 feet in length
    - Require a major material change at a rate of 1.75 times the height of the building.
    - Require a shift in wall façade a minimum of 2 feet in dimension every 40 feet and a shift in wall façade a minimum of 4 feet in dimension every 80 feet.
    - If side and/or rear building walls face primary roadways, the same regulations as the guidelines apply to the secondary facades. If the building's side and/or rear walls face internal lots, rates for planar variation can double guidelines.
- **Vertical delineation.** To create visual interest and encourage an active street frontage, interruption in the vertical plane should be prevalent on tall buildings. Primary entrances and exits should be highlighted through planar variation and/or difference in height.
  - » Buildings up to 30 feet in height
    - Require a change in material color or texture in a minimum of 3 locations. Height of change is required to be a minimum of 5 feet.
    - Require a shift in wall façade or provide a visual break in wall façade at a minimum of two locations.
  - » Buildings over 30 feet in height
    - Require a change in material color or texture in a minimum of 5 locations. Height of change is required to be a minimum of 10 feet.

- Require a shift in wall façade or provide a visual break in wall façade (through canopies or accent bands/recesses) at a minimum of four locations.
- **Corner Articulation.** To ensure that building corners that face or can be viewed from public or private roads shall be distinctive in the use of architectural elements, materials, and design.
  - » The continuation of architectural elements that are required for horizontal and vertical material delineation shall also wrap the corners of the building extending at least 50 feet around the corner of the building.
  - » Corner articulation may be provided in the form of glass or other types transparent materials.

### EXTERIOR BUILDING MATERIALS

SPLIT FACE

- Exterior façade materials shall consist of high quality, durable products on any side visible from a public or private roads. Materials are not limited to the brick requirements that typically applies in the Township. Appropriate building materials includes combinations of: brick, flush metal/aluminum panels, ~~concrete block, and pre-cast concrete.~~
- Varying patterns and textures shall be introduced to give the building smaller scale relationships of materials vs. monotonous and large surfaces without visual variations.
- Glass shall be used on primary facades to provide transparency.

### SIGHTLINE REQUIREMENTS AND DOCK DOORS

- All mechanical installations and/or features shall be adequately screened from street view or view from nearby public space. The choice of screening shall complement or enhance the building's dominant color and overall character.
- Dock doors must be located in the side or rear yard and have appropriate buffers to minimize impacts from abutting residential and commercial uses. In order to limit uses with higher truck volumes, up to one truck dock door per 4,000 square feet is permitted for building footprints that are up to 100,000 square feet. One truck dock door per 8,000 square feet of building footprint is permitted over 100,000 square foot. These standards may be relaxed for sites within the interior for walls not visible from a public street or I-96. Dock doors shall be set back at least 50 feet from the lot line (or 75 feet from the lot line if adjacent to residential). Buffer Zone Type A is required for any dock doors located adjacent to residential, and Buffer Zone Type B is required for any dock doors located adjacent to commercial.
- Accessory uses that include outdoor storage (including for trucks and trailers and loading areas) shall indicate the location of such areas

on the site plan. These areas shall not be located in the front yard and shall be no larger than 40% of the total square footage of the building on site. Sites shall also not have outdoor storage visible from I-96. Outdoor storage must have appropriate buffering between adjacent residential and commercial areas; Buffer Zone Type A is required for any outdoor storage area located adjacent to residential, and Buffer Zone Type B is required for any outdoor storage area located adjacent to commercial.

OR LATSON ROAD

## HIGH TECH / LIGHT INDUSTRIAL DESIGN GUIDELINES

Examples of building that meet the Industrial Building Design Standards are shown on pages 5 and 6.

HIGH-TECH / LIGHT INDUSTRIAL DESIGN DIMENSIONAL STANDARDS	
<b>Minimum setbacks:</b>	
Front Yard	85 feet (or 50 feet if no parking is located in the front yard and/or building height is 30 feet or less) <sup>1</sup>
Side Yard	25 feet (or 50 feet if adjacent to residential) <del>25 feet plus an additional 0.5 feet per foot of height over 30 feet (if not adjacent to residential)<sup>2</sup></del>
Rear Yard	40 feet (or 80 feet if adjacent to residential)
Parking Lot	20 feet front, 10 feet side and rear
Maximum Height	55 feet or 3 stories, whichever is less <sup>3</sup>
Maximum Height of Hotel	57 feet or 4 stories <sup>4</sup>

OR WITHIN EAST AREA

WEST AREA

EAST AREA MAX. HEIGHT 45/3 STORIES

1 Proposed addition to front yard setback with lesser building height.  
 2 Proposed standard to provide for a greater side yard set back for taller buildings.  
 3 Existing maximum height in the Zoning Ordinance is 30 feet or 2 stories  
 4 As a Special Land Use, the Hotel may be increased to 65 feet or 5 stories, provided minimal distance from adjacent residential home is 500 feet and the Township determines the design is compatible with residential in the area in terms of views and lighting.

MINIMUM PARKING REQUIREMENTS	
Light Industrial	1.5 spaces per 1,500 square feet of gross floor area or 1.2 spaces per employee at peak shift, whichever is greater; plus 1 for each corporate vehicle, with the ability to reduce the amount of parking required to "bank" an area for future parking, as permitted in the Township's Zoning Ordinance.

Height over 45 requires SLU in East Area. In west area, height over 57f/4s is SLU

This graphic includes uplights which conflict with the downward directed/shielded requirement.

## INDUSTRIAL BUSINESS PARK OUTDOOR LIGHTING STANDARDS

The purpose and intent of the Outdoor Lighting standards is to:

- Minimize light trespass onto adjacent properties
- Help eliminate artificial lighting that contributes to “sky glow “and disrupts the natural quality of the nighttime sky
- Provide a safe nighttime environment

Any future site plan within the PUD shall be required to submit an outdoor lighting plan to abide by the standards set forth in this section. The site plan shall contain a photometric layout for the exterior lighting which may subsequently be waived if there is no parking area present on the site. These standards generally apply throughout the PUD, but flexibility may be allowed when the development is not adjacent to residential areas, ~~and for the mixed use area.~~

The following outdoor lighting types shall be exempt from the provisions of this section:

- Emergency lighting
- Temporary lighting for performance areas, construction sites and community festivals.
- Seasonal and holiday lighting provided that the lighting does not create direct glare onto other properties or upon the public rights-of-way.

The following outdoor lighting types shall be prohibited:

- Floodlights or swivel luminaires designed to light a scene or object to a level greater than its surroundings unless aimed downward. No fixtures may be positioned at an angle to permit light to be emitted horizontally or above the horizontal plane.
- Unshielded lights that are more intense than 2,250 lumens or a 150 watt incandescent bulb.
- Search lights and any other device designed solely to light the night sky except those used by law enforcement authorities and civil authorities.
- Laser source light or any similar high intensity light when projected above the horizontal plane.
- Mercury vapor lights.
- Metal halide lights, unless used for outdoor sport facilities.
- Quartz lights.

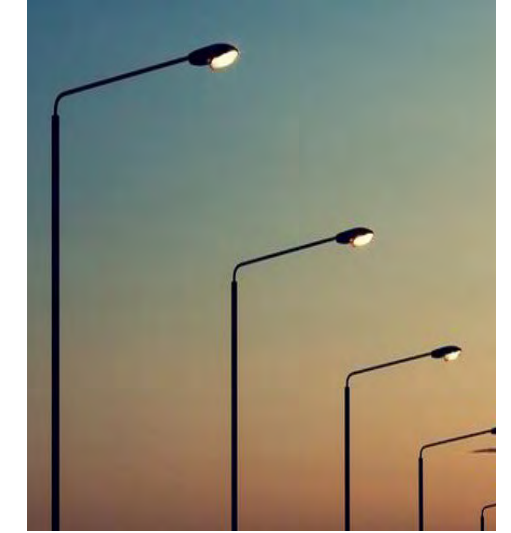
Outdoor Lighting Design Standards – Internal to the Site:

- Direct or reflected outdoor lighting shall be designed and located to be confined to the site for which it is accessory. The maximum lighting levels at the property lines of any other property shall not exceed 0.2 footcandles.
- Lighting of building facades shall be from the top and directed downward with full cut-off shielding.
- The average lighting values for areas intended to be lit on ~~commercial and industrial~~ parcels shall not exceed 1.0 footcandles on average. The uniformity ratio (maximum to minimum) for all parking lots shall not exceed the current IESNA RP-20 uniformity ratio guideline. (Note: Current guideline is 15:1) **20' for East Area**
- Lighting fixtures for industrial properties shall meet the township maximum height of **30 feet** and 10 footcandles with the following exceptions:
  1. The Township may permit maximum light levels of 12 footcandles on average (common with new LED lighting systems), designed to have no spillover onto adjacent properties and a maximum pole height of 35 feet to reduce the number of poles upon a finding that the result will provide more efficient lighting and aesthetics throughout the day.
  2. Provided that when lighting is adjacent to, and visible from, abutting residential properties, the maximum height of lighting poles shall be 20 feet unless the Township approves taller poles with a demonstration that it is an overall better lighting design in terms of aesthetics.
  3. Site lighting for non-residential uses shall not exceed 1.0 footcandles on average when a use is not open for business.

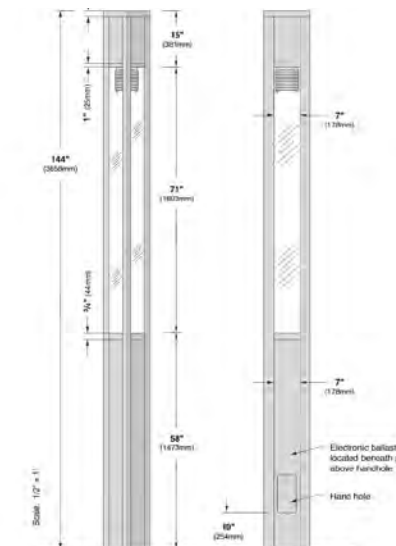
Outdoor Lighting Design Standards – Public Street Lighting:

- Streetlights in the public rights-of-way shall be the minimum necessary to provide adequate illumination for public safety and be designed to direct lighting downward onto the public rights-of-way.
- Luminaires installed up to the edge of any bordering property are permitted.
- Ornamental lighting will be installed as part of the northern entry features will be included (see bottom right for representative types of light fixtures). The fixtures will be selected during the design of the entry feature. The lighting could potentially also be installed along the Latson Road frontage along the right-of-way in the future as part of a corridor wide urban design project (see language in the PUD Agreement).
- Public street illumination shall use the most current American National Standard Practice for Roadway Lighting ANSI/IESNA RP-08 for all public street lighting.

## HIGH TECH/LIGHT INDUSTRIAL OUTDOOR LIGHTING STANDARDS



12 ft “Triangular Column” by Selux



Recommended ornamental pedestrian-scale lighting for northern entry on Latson Rd.



Mando, Novi



BLM Group, Novi

**HIGH TECH/LIGHT INDUSTRIAL**  
 REPRESENTATIVE PHOTOGRAPHS THAT MEET INDUSTRIAL BUILDING DESIGN STANDARDS



Rapid Packaging, Grand Rapids



EPIC Equipment and Engineering, Shelby Parkway Corporate Park



AEV, Lyon Township



Kawasaki Robotics, Lyon Township



Martinrea International, Auburn Hills  
\*Image from Faudie Architecture



Harman International, Novi

**HIGH TECH/LIGHT INDUSTRIAL**  
REPRESENTATIVE PHOTOGRAPHS THAT MEET INDUSTRIAL BUILDING DESIGN STANDARDS



SW Technology People



Vioneering, Auburn Hills



TI Automotive Headquarters, Auburn Hills



Magna

## NORTH EDGE VISUAL ENHANCEMENT ZONE DESIGN GUIDELINES

The following guidelines apply to the North edge. The intent is to provide "front door" type views for building facades and areas that can be seen from traffic along I-96 or Beck Road. The area where this additional design requirement may apply is illustrated on the sight line study (right). As site plans are submitted, the Township will consider the size of the building, its height, setbacks, presence of loading docks, parking, and other activities. Those factors will be used to determine the extent that the following may be necessary to meet the intent:

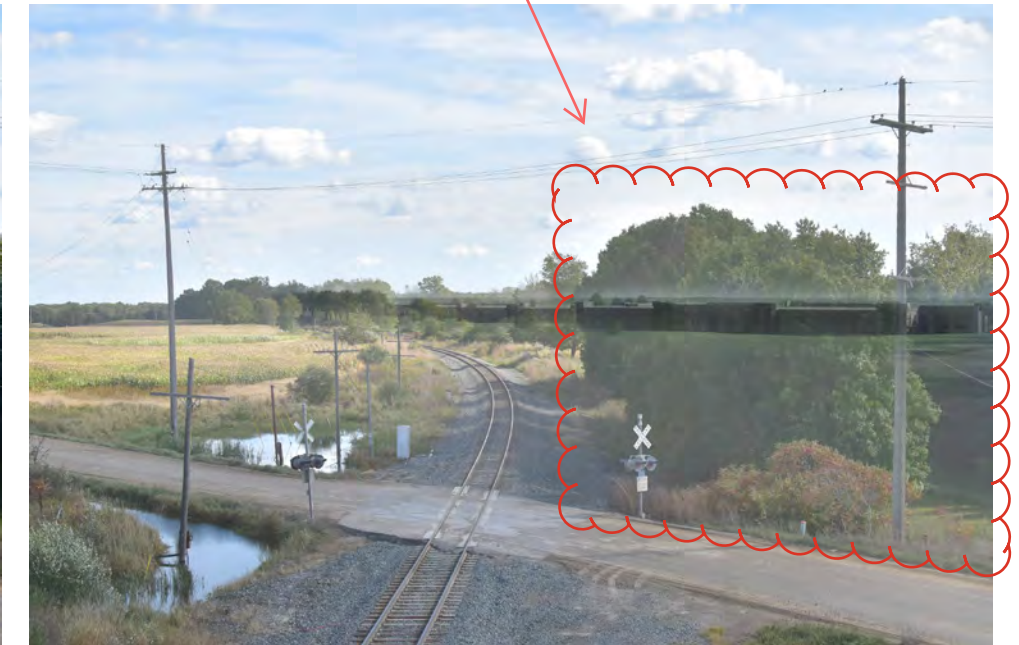
### EXTERIOR BUILDING MATERIALS AND LAYOUT

- Exterior building walls visible from I-96 or Beck Road shall be similar to building materials used on the front facade, and/or additional landscape will be provided to screen views, or fill in gaps in views.
- Dock doors shall be located on the building walls that are not directly visible or shall be screened with landscaping along the site boundary.
- Buffers and landscaping may be reduced or modified in consideration of the distance from the interchange or if woodlands are preserved to achieve the intent of these guidelines.

## SIGHT LINE STUDY

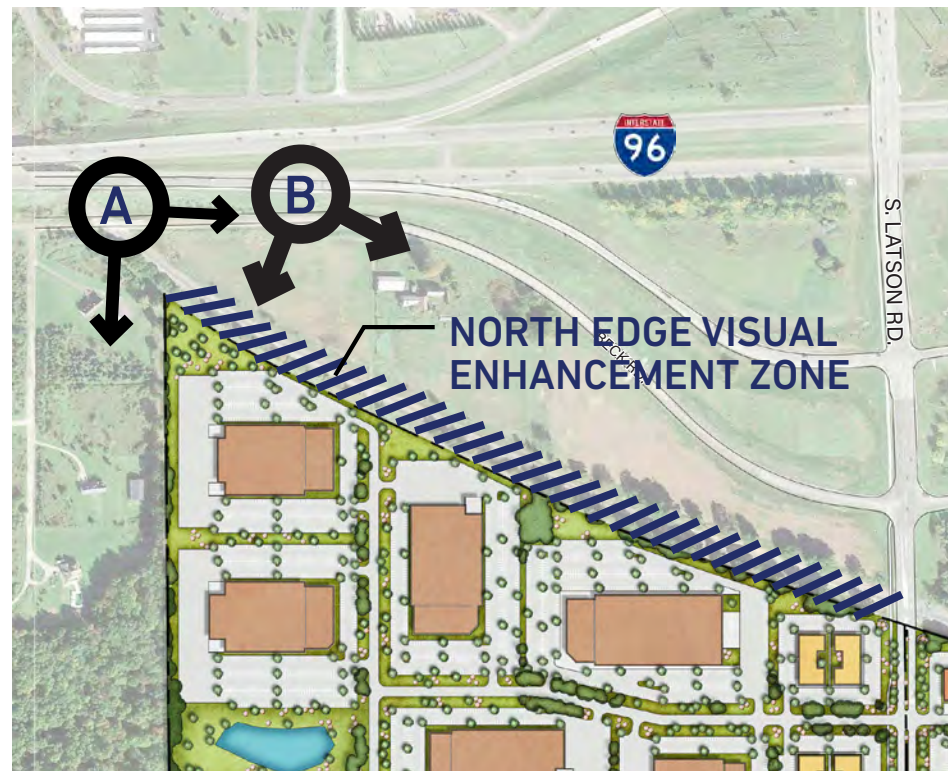


EXISTING CONDITIONS



SIMULATED VIEW

Verify property limits.  
This image appears to include off site properties.



EXISTING CONDITIONS



SIMULATED VIEW



## ACCESS

Two access points are proposed along Latson Road. The northern access will align with the accessory commercial on the east side. It is anticipated that this access will be signalized.

The southern access is shown as offset with the current Sweet Road on the east side of Latson Road (see sketch). This alignment may be modified to more closely align with Sweet Road, if approved by the Township and Livingston County Road commission (see overall concept). There is also a possible road connection shown to the vacant property to the south.

## TRAFFIC SIGNALS

- Two traffic signals are proposed at both the north and south entrances with appropriate improvements.
- It is anticipated that mast arm signals would complement the Latson Road entrance features.

## LATSON ROAD FRONTAGE STREETScape GUIDELINES

- Generally a 30-foot landscaped greenbelt (see illustrations labeled "Option 1" and "Option 2") shall be installed along the east and west sides of Latson Road.
- Larger trees than the minimum sizes typically required:
  - » Deciduous Tree: 3-4 inch caliper (with minimum average size of 3.5 inches)
  - » Ornamental Tree: 2.5 - 3.5 inch caliper
  - » Evergreen Tree: 10 - 14 feet tall (with minimum average size of 12 feet tall)
  - » Shrubs and Hedges: 30-36 inches tall
  - » Canopy Tree: 3 inch caliper
  - » Deciduous Ornamental Tree: 2 inch caliper
  - » Evergreen Tree: 6 foot height
  - » Deciduous Shrub: 2 foot height
  - » Upright Evergreen Shrub: 2 foot height

## REQUIRED GREENBELT ALONG STREET FRONTAGE

For all other public roads outside of Latson Road, a twenty (20) foot wide greenbelt shall be planted along each public street right-of-way including the equivalent of one (1) canopy tree, rounded upward, for every fifty (50) linear feet of frontage. The Planning Commission may approve clustering of trees or substitution of evergreen trees for up to fifty percent (50%) of the required trees. All greenbelt trees shall be arranged to simulate a natural setting such as staggered rows or massings.

Sweet Road offset alignment concept



**CONFLICTING STANDARDS FOR LATSON ROAD STREETScape AND LANDSCAPE GUIDELINES. LARGER SPECIES SHALL BE REQUIRED.**

## LATSON ROAD STREETScape AND LANDSCAPE GUIDELINES

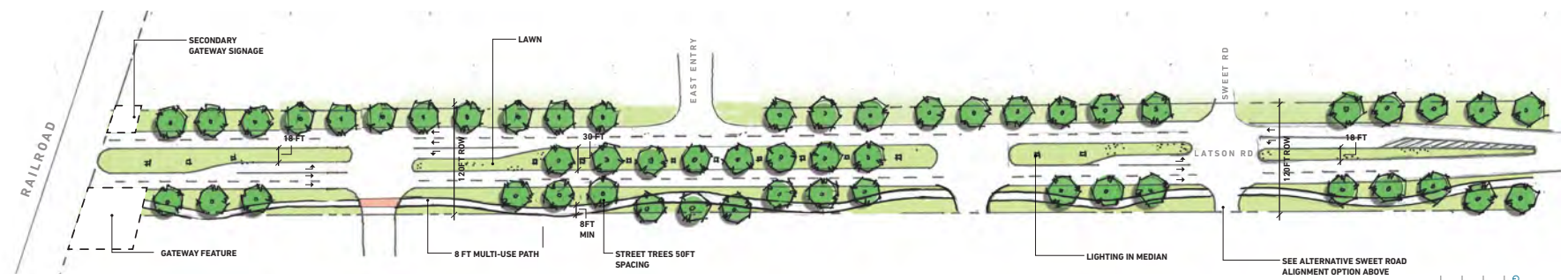
### OVERALL MINIMUM STREETScape SIZES

- **Outside of the Latson Road Greenbelt, the minimum required plant sizes shall be as follows:**
  - Deciduous Canopy Tree: 2.5" caliper
  - Deciduous Ornamental Tree: 2" caliper
  - Evergreen Tree: 6' height
  - Deciduous Shrub: 2' height
  - Upright Evergreen Shrub: 2' height
  - Spreading Evergreen Shrub: 18" - 24" spread

### LATSON ROAD LANDSCAPING AND IMPROVEMENTS

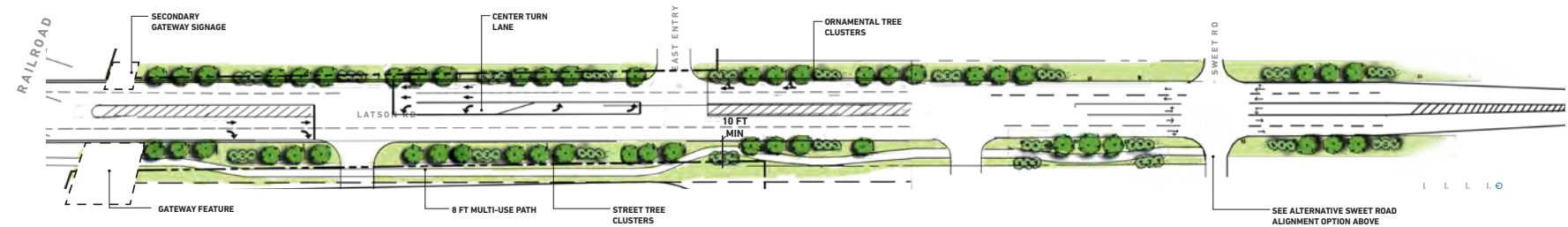
Two options for landscape design along Latson Road are shown below. The level of road improvements anticipated is described in the separate Traffic Impact Study. Versa only controls part of the Latson Road frontage shown, therefore, coordination will be needed between the County, Township, and other property owners, See details in the PUD Agreement.

### OPTION 1



This option shows a potential median along Latson road, which would need to be endorsed by the Livingston County Road Commission. This PUD reserves sufficient right-of-way to accommodate this alternative along the frontage owned by Versa.

### OPTION 2



Option 2 shifts much of the median landscaping, illustrated in Option 1, to the greenbelt along each side of Latson Road.

**DESIGN GUIDELINES**  
**BUFFER ZONES**

**BUFFER ZONE LANDSCAPING**

- Buffer Yard Standards shall be in accordance with Tables 12.02.03 A and B “Buffer Zone Requirements” and “Description of Required Buffer Zones” as required by the Genoa Township Zoning Ordinance.
- Buffers and landscaping may be reduced or waived if woodlands are preserved to achieve the intent.

BUFFERS FOR THE EAST SIDE SHOULD BE EXPANDED TO ALLOW TRANSITION TO ADJACENT RESIDENTIAL. CONSIDER LARGER BUFFERS AND LANDSCAPING.

THERE IS NO MIXED USE PROPOSED.

**Mixed Use Buffer Yard Requirements:**

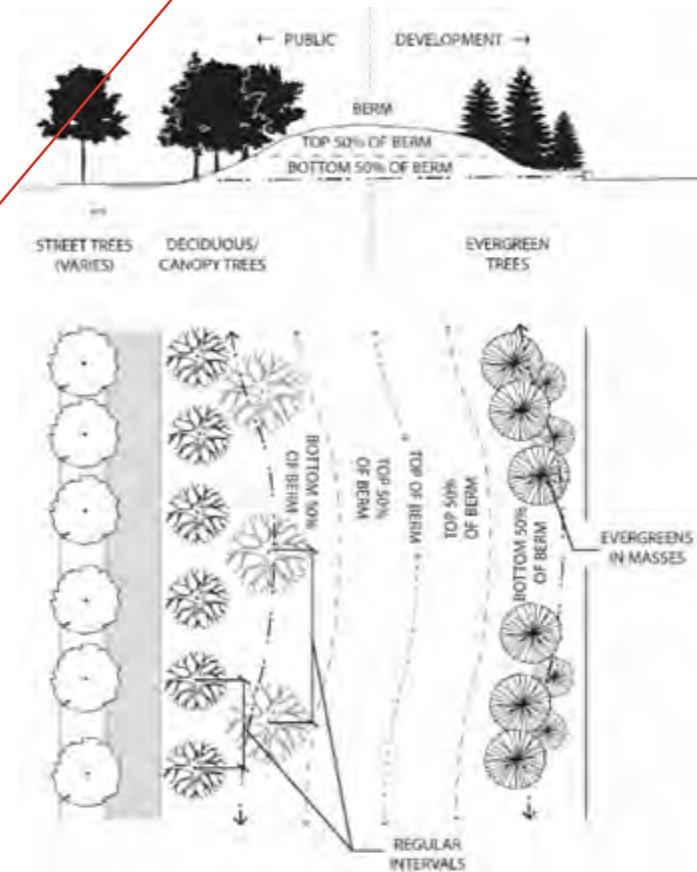
- For mixed uses adjacent to residential uses:
  - Minimum width: 20 feet
  - 6 foot high continuous wall or 3 foot high berm
  - 1 canopy tree, 1 evergreen tree and 4 shrubs per each thirty (30) linear feet along the property line, rounded upward
- For mixed uses adjacent to commercial uses:
  - Minimum width: 10 feet
  - 1 canopy or evergreen tree or 4 shrubs per each twenty (20) linear feet along the property line, rounded upward

**Buffering Between Industrial and Residential or Mixed Uses.**

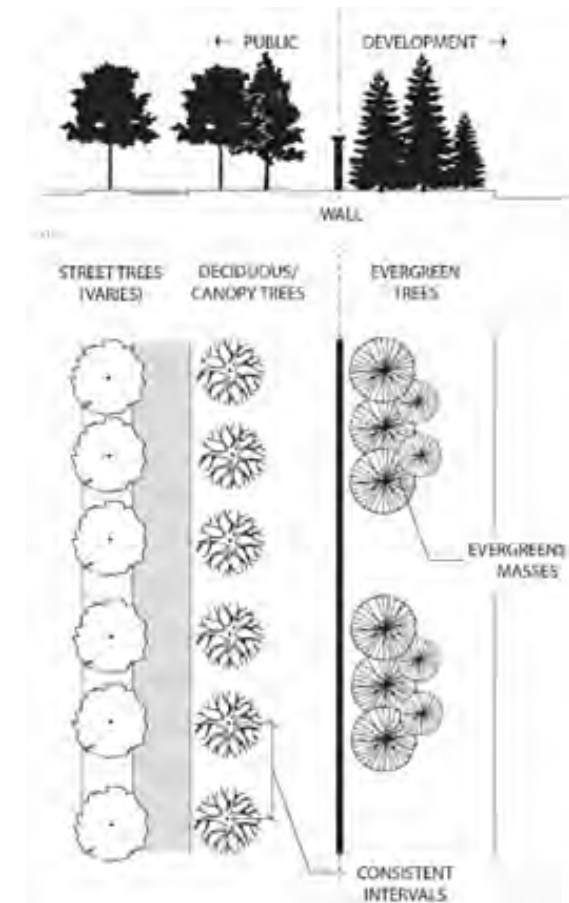
- For industrial uses adjacent to residential uses:
  - Minimum width: 50-75 feet
  - 6 foot high continuous wall or 4 foot high berm
  - 1 canopy tree, 2 evergreen trees and 4 shrubs per each twenty (20) linear feet along the property line, rounded upward

**Notes:**

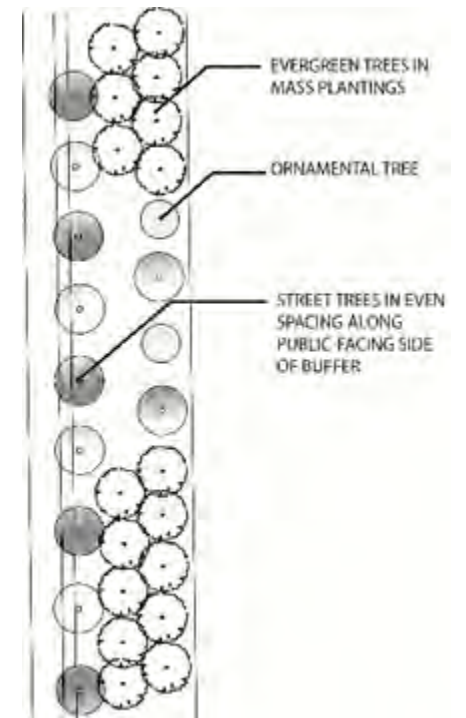
- Existing quality trees (hickory, oak, maple) with a caliper of at least eight (8) inches shall count as two (2) trees toward the buffer requirements.
- Canopy trees shall have a minimum caliper of 2.5 inches at the time of planting.
- Evergreens shall have a minimum height of six (6) feet at the time of planting.
- At least 50% of the shrubs shall be 24 inches tall at planting, with the remainder over 18 inches.



**TYPE A: BUFFER ZONE WITH BERM**



**TYPE B: BUFFER ZONE WITH WALL**



**TYPE C: BUFFER ZONE BETWEEN STREET AND BUILDING**

BUFFER ZONE REQUIREMENTS			
Adjacent District for Use			
Proposed Use	SF	MF or MHP	INDUSTRIAL <del>Commercial</del>
Industrial	A/B	A/B	B/C

**WETLANDS**

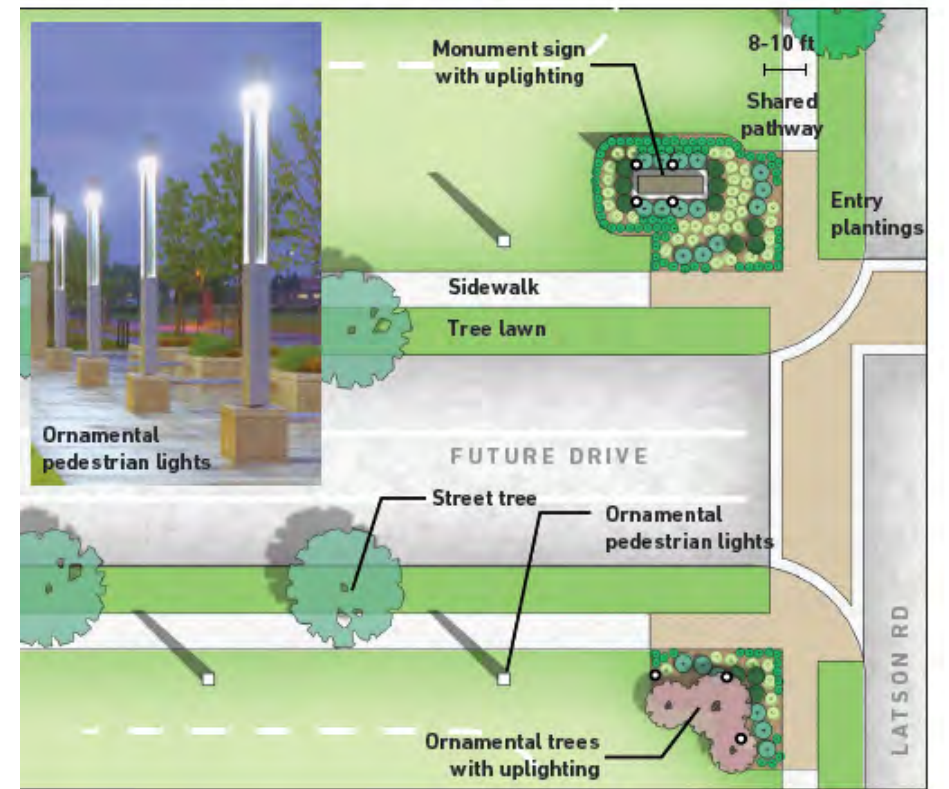
- An undisturbed natural setback shall be maintained twenty-five (25) feet from a MDEQ determined/regulated wetland. Trails and recreational areas may be allowed in the wetland setback.

**OPEN SPACE CONCEPT AND REPRESENTATIVE AMENITIES**

This concept illustrates a potential layout that would be consistent with the PUD Agreement and Design Guidelines for the roads, development areas, wetlands, detention, open space, pedestrian system, entrance features and other amenities. The actual layout will vary based on more detailed site engineering evaluation, building/lot sizes, specific nature and needs of the business end users' proposed space and other building requirements, and other factors. More specific plans for the overall development, consistent with the intent will be submitted with future site plans.



**TOWNSHIP GATEWAY SIGN AREA  
ENTRY SIGNAGE LANDSCAPE PLAN**



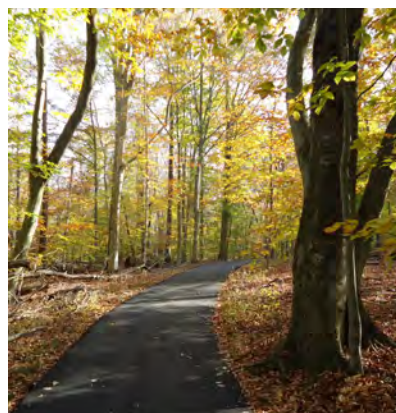
**50 FT PLANTING BUFFER**



**DETENTION PONDS WITH OPEN SPACE AMENITIES**



**WETLAND OPEN SPACE**



**POTENTIAL TRAIL CONNECTION TO MARION GENOA DRAIN TO SOUTH**



**OVERALL INDUSTRIAL CAMPUS AMENITIES**



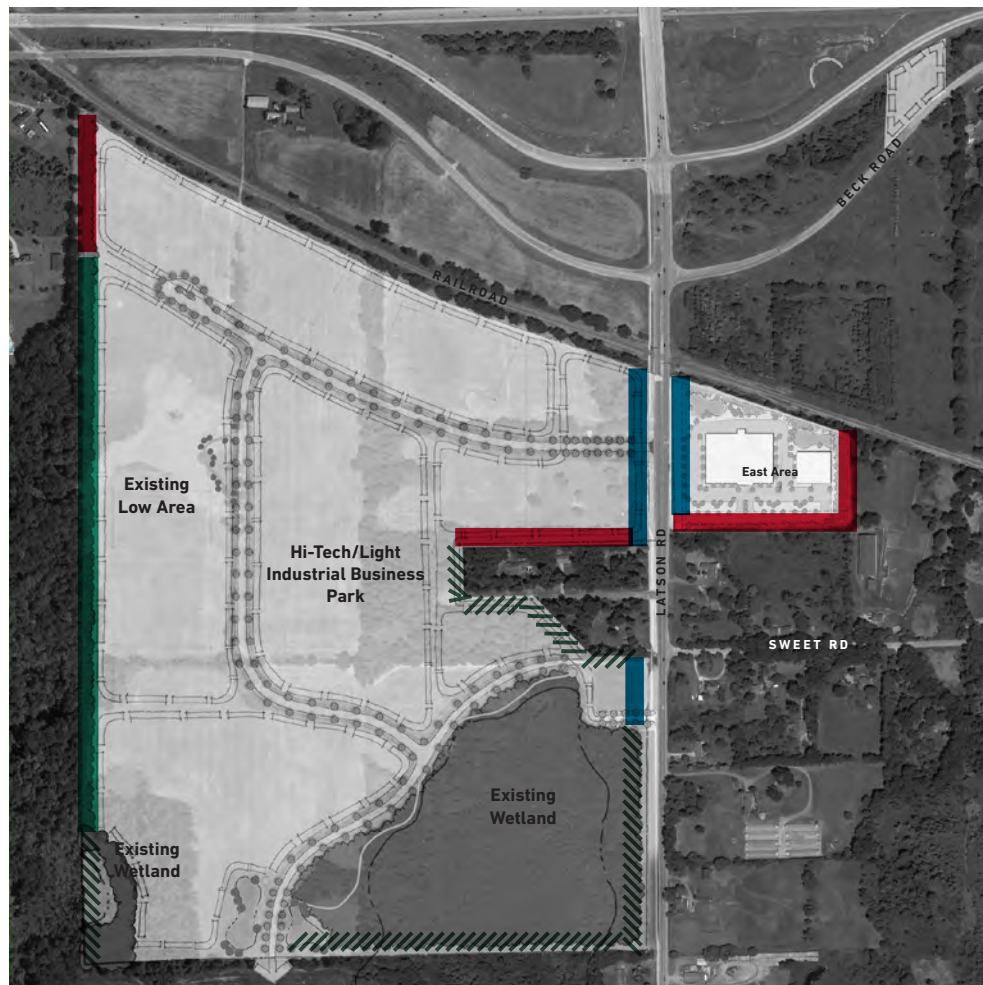
## OPEN SPACE CONCEPT AND REPRESENTATIVE AMENITIES - BUFFERS

Buffer Yard Standards shall be in accordance with Tables 12.02.03 A and B "Buffer Zone Requirements" and "Description of Required Buffer Zones" as required by the Genoa Township Zoning Ordinance.

Buffers and landscaping may be reduced or waived if woodlands are preserved to achieve the intent.

See key plan for minimum buffer widths and corresponding diagrams for example buffer landscape plans fulfilling buffer yard standards of the Township Ordinance.

### LANDSCAPE BUFFER KEY PLAN

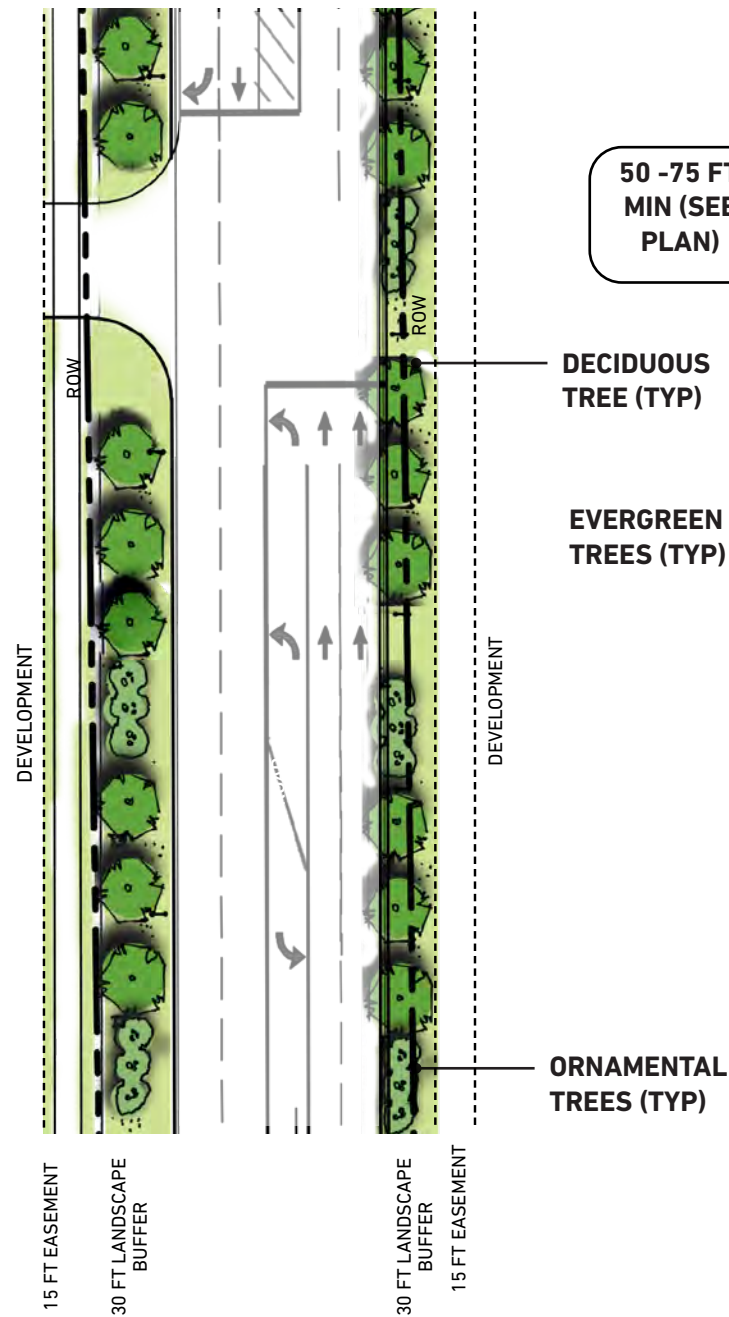


### LEGEND

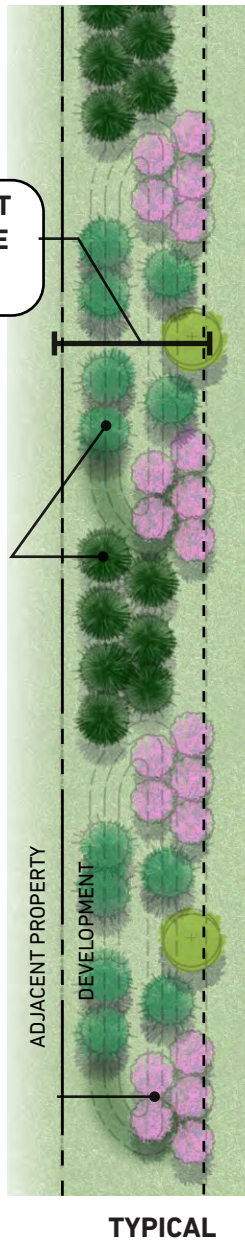
- 50 FT BUFFER TO ADJACENT USE
- 75 FT LANDSCAPE BUFFER TO ADJACENT RESIDENTIAL\*
- NATURAL LANDSCAPE TO REMAIN
- LATSON RD ROW 30' LANDSCAPE BUFFER

\*BUFFER MAY BE CHANGED TO 20 FT IF NON-RESIDENTIAL ADJACENT

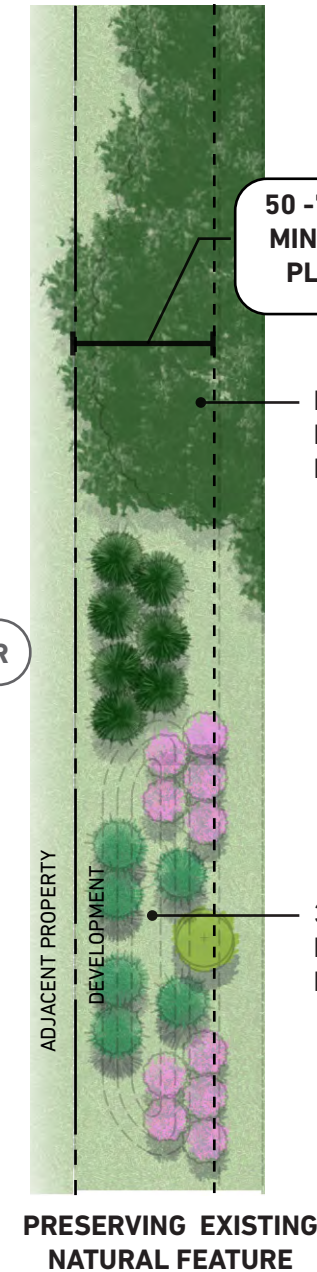
### LATSON ROAD ROW LANDSCAPE BUFFER



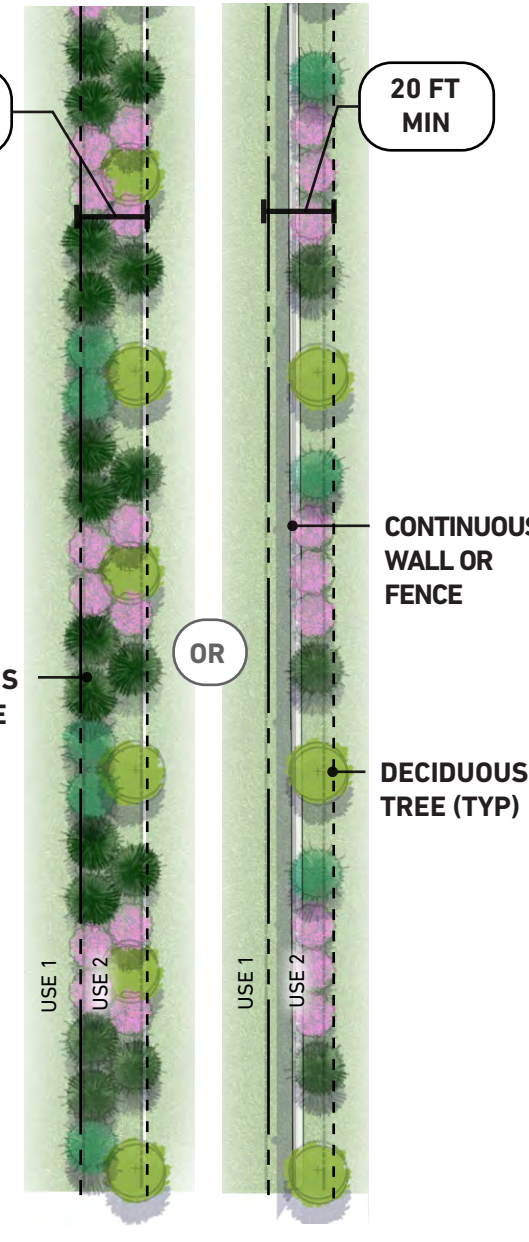
### 50-75 FT LANDSCAPE BUFFER TO ADJACENT USES



### OR



### 20 FT LANDSCAPE BUFFER FOR BETWEEN INTERNAL USES (OR ADJACENT NON-RESIDENTIAL USES)



## HIGHWAY DEVELOPMENT SIGNAGE

The highway development signage not only directs travelers to Innovation Exchange, but is also an opportunity to highlight Genoa Township itself. The materiality reflects both the modern construction of the PUD and local materiality.

View from on-ramp



Conceptual illustration of highway development signage



## LANDSCAPE PLAN

While the highway development signage is visible from far away on its own, the landscape can complement it at eye-level for an on-ramp passerby. A 6 ft berm lifts the sign itself while blending into the existing tree line. Landscape boulders, matching the signage stone base, emerge from the gradual slope. Low-maintenance plantings surround the foundation and provide year-round interest and physical deterrence to the wayfinding structure.

### A. Planting Materials

- Planting materials are to be of a high quality and substantial size to provide a degree of maturity to the appearance of the landscaping immediately upon installation.

## DESIGN GUIDELINES

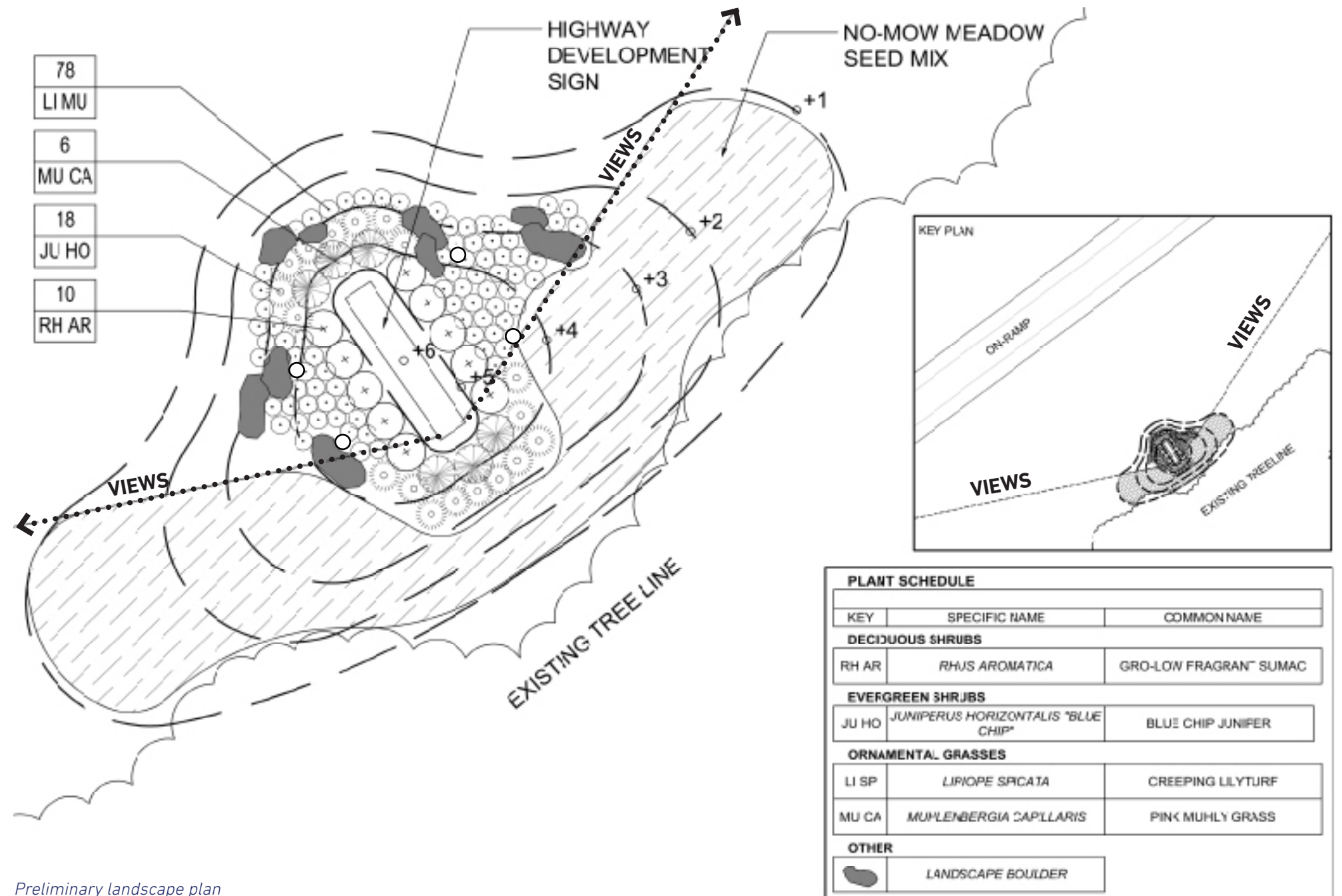
### DEVELOPMENT SIGNAGE AND LANDSCAPE

#### B. Lawn Areas.

- All areas of a Unit not landscaped with plant materials or hard surfaces or kept as natural wooded areas shall be established as lawn areas by sodding or seeding. Preservation of wooded rear yard areas in their natural condition is strongly encouraged.

#### C. Edging and Mulching Materials.

- The use of natural cut sod edging to define planting beds is strongly encouraged. Edging materials made of steel, aluminum or plastic may be used to define planting beds.



Preliminary landscape plan

**EXHIBIT G**  
(Traffic Memo dated June 6, 2024)

102984.000185 4892-2860-5339.6

# MEMO

VIA EMAIL: [todd@versacos.com](mailto:todd@versacos.com)

**To:** Todd Wyett  
Latson South, LLC

**From:** Julie Kroll, PE, PTOE  
Mason Gamble, EIT  
Fleis & VandenBrink Engineering

**Date:** June 6, 2024

**Re:** Proposed Latson Road PUD Amendment  
Genoa Township, Michigan  
Trip Generation Analysis

## INTRODUCTION

This memorandum presents the results of the Trip Generation Analysis (TGA) for an amendment to the previously approved Latson Road Planned Unit Development (PUD) project in Genoa Township, Michigan. The proposed PUD amendment to the overall development includes the following:

- (1) The removal of an approximately 6-acre parcel, which is located on the east side of Latson Road, north of the railroad tracks. This parcel is zoned ICPUD (Tax Parcel No. 11-09-300-001) and will be added to a new commercial PUD project.
- (2) Changes in the approved land uses for the approximately 10-acre parcel, which is located adjacent to the east side of Latson Road, south of the railroad tracks.

Most of the previously approved intense commercial uses for the 10-acre parcel, such as a gas station, hotel, coffee-shop, etc. will be removed from this 10-acres parcel. In their place, the amendment will permit the development of light industrial buildings on the property. For traffic analysis purposes, the proposed PUD amendment includes the construction of 80,000 square feet light industrial development on property that is currently vacant; site access is proposed via one (1) full access driveway on Latson Road.

Fleis and VandenBrink (F&V) previously completed a Traffic Impact Study (TIS) for the Latson Road PUD, dated September 13, 2019. The purpose of this study is to summarize the trip generation projections associated with the proposed PUD amendment and provide a comparison with the site-generated traffic from the land uses within the previously approved PUD.

## TRIP GENERATION

The number of weekday peak hour (AM and PM) and daily vehicle trips that would be generated by the original Latson Road PUD was determined based on the completed TIS; the trip generation associated with the proposed PUD amendment development was calculated using the equations published by the Institute of Transportation Engineers (ITE) in *Trip Generation, 11<sup>th</sup> Edition*.

The results of the trip generation comparison indicate that the proposed PUD amendment development will generate significantly less trips, as compared to the previously approved Latson Road PUD.

**TABLE 1: TRIP GENERATION COMPARISON**

	Land Use	ITE Code	Amount	Units	Average Daily Traffic	AM Peak Hour (vph)			PM Peak Hour (vph)		
						In	Out	Total	In	Out	Total
Approved Latson Road PUD	Hotel	310	100	Rooms	702	27	18	45	25	24	49
	Gas Station w/ Convenience Store	944	8	VFP	1,376	41	41	82	56	56	112
	<i>Pass-By</i>	58% AM, 42% PM			688	24	24	48	24	24	48
	<i>New Trips</i>	42% AM, 58% PM			688	17	17	34	32	32	64
	Coffee Shop w/ Drive-Thru	937	1,500	SF	1,231	68	65	133	33	32	65
	<i>Pass-By</i>	49% AM, 50% PM			616	33	32	65	17	16	33
	<i>New Trips</i>	51% AM, 50% PM			615	35	33	68	16	16	32
	Shopping Center	820	10,000	SF	1,256	6	3	9	48	51	99
	<i>Pass-By</i>	34%			628	2	1	3	16	17	33
	<i>New Trips</i>	66%			628	4	2	6	32	34	66
	High turnover (Sit-Down) restaurant	932	5,000	SF	561	28	22	50	30	19	49
	<i>Pass-By</i>	43%			241	12	9	21	13	8	21
	<i>New Trips</i>	57%			320	16	13	29	17	11	28
	<b>Total Trips</b>					<b>5,126</b>	<b>170</b>	<b>149</b>	<b>319</b>	<b>192</b>	<b>182</b>
<i>Total Pass-By</i>					2,173	71	66	137	70	65	135
<i>Total New Trips</i>					2,953	99	83	182	122	117	239
Proposed PUD Amendment	General Light Industrial	110	80,000	SF	351	51	7	58	5	29	34
<b>Difference</b>					<b>-4,775</b>	<b>-119</b>	<b>-142</b>	<b>-261</b>	<b>-187</b>	<b>-153</b>	<b>-340</b>

**CONCLUSIONS**

- The results of the trip generation comparison indicate the proposed PUD amendment development will generate significantly less trips, as compared to the Latson Road PUD. Therefore, the proposed light industrial development will have far less of an impact on the adjacent roadway network than what was previously approved.

Any questions related to this memorandum should be addressed to Fleis & VandenBrink Engineering.



I hereby certify that this engineering document was prepared by me or under my direct personal supervision and that I am a duly licensed Professional Engineer under the laws of the State of Michigan.

*Julie M. Kroll*  
**Julie M. Kroll**  
**2024.06.06**  
**16:55:49 -04'00'**

**Attached:** Site Concept Plan





PRIMARY  
ENTRANCE  
MUST ALIGN  
WITH WEST  
ENTRANCE

THIS COULD BE THE SECONDARY ACCESS BUT SHOULD NOT BE THE  
PRIMARY. RELOCATE THIS OUTSIDE OF THE 75' BUFFER TO SHOW  
DEVELOPMENT ASSUMING THE EXISTING RESIDENTIAL TO THE SOUTH  
STAYS.

ADD SIDEWALKS AND PEDESTRIAN CROSSING

LATSON ROAD

60,000 sf  
HI-TECH  
/LIGHT  
INDUSTRIAL

20,000 sf  
HI-TECH  
/LIGHT  
INDUSTRIAL

75 FT BUFFER

75 FT BUFFER (IF NON-RESIDENTIAL USE ADJACENT)

20 FT BUFFER (IF NON-RESIDENTIAL USE ADJACENT)



STAFF AND  
COUNSEL  
COMMENTS  
SHOWN IN RED

## GENOA CHARTER TOWNSHIP APPLICATION Planned Unit Development (PUD)

APPLICANT NAME: Latson Beck, LLC and Covenant of Faith, LLC

APPLICANT EMAIL: todd@versacos.com

APPLICANT ADDRESS & PHONE: 29201 Telegraph Rd., Ste. 410, Southfield, MI 48034, ( 248) 770-8484

OWNER'S NAME: Latson Beck, LLC and Covenant of Faith, LLC

OWNER ADDRESS & PHONE: 29201 Telegraph Rd., Ste. 410, Southfield, MI 48034 ( 248) 770-8484

TAX CODE(S): 11-09-300-046 and 11-09-300-040

### QUALIFYING CONDITIONS (To be filled out by applicant)

1. A PUD zoning classification may be initiated only by a petition.
2. It is desired and requested that the foregoing property be rezoned to the following type of PUD designation:
  - Residential Planned Unit Development (RPUD)
  - Planned Industrial District (PID)
  - Mixed Use Planned Unit Development (MUPUD)
  - Redevelopment Planned Unit Development (RDPUD)
  - Non-residential Planned Unit Development (NRPUD)
  - Town Center Planned Unit Development (TCPUD)
  - ICPUD - Covenant of Faith property is already zoned ICPUD
3. The planned unit development site shall be under the control of one owner or group of owners and shall be capable of being planned and developed as one integral unit.

EXPLAIN The property is owned by several owners under single control.

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4. The site shall have a minimum area of twenty (20) acres of contiguous land, provided such minimum may be reduced by the Township Board as follows:
  - A. The minimum area requirement may be reduced to five (5) acres for sites served by both public water and public sewer.
  - B. The minimum lot area may be waived for sites zoned for commercial use (NSD, GCD or RCD) where the site is occupied by a nonconforming commercial, office or industrial building, all buildings on such site are proposed to be removed and a new use permitted within the underlying zoning district is to be established. The Township Board shall only permit the PUD on the smaller site where it finds that the flexibility in dimensional standards is necessary to allow for innovative design in redeveloping the site and an existing blighted situation will be eliminated. A parallel plan shall be provided showing how the site could be redeveloped without the use of the PUD to allow the Planning Commission to evaluate whether the modifications to dimensional standards are the

minimum necessary to allow redevelopment of the site, while still meeting the spirit and intent of the ordinance.

- C. The PUD site plan shall provide one or more of the following benefits not possible under the standards of another zoning district, as determined by the Planning Commission:
  - preservation of significant natural or historic features
  - a complementary mixture of uses or a variety of housing types
  - common open space for passive or active recreational use
  - mitigation to offset impacts
  - redevelopment of a nonconforming site where creative design can address unique site constraints.
  
- D. The site shall be served by public sewer and water. The Township may approve a residential PUD that is not served by public sewer or water, provided all lots shall be at least one (1) acre in area and the requirements of the County Health Department shall be met.

Size of property is approximately 15 acres.

DESCRIBE BELOW HOW THE REQUESTED PUD DESIGNATION COMPLIES WITH AFOREMENTIONED MINIMUM LOT SIZE REQUIREMENTS.

The property is served by both public water and sewer.

This response is not related to the information requested.

**STANDARDS FOR REZONING TO PLANNED UNIT DEVELOPMENT (RESPOND HERE OR WITHIN THE IMPACT STATEMENT)**

- 1. How would the PUD be consistent with the goals, policies and future land use map of the Genoa Township Master Plan, including any subarea or corridor studies. If conditions have changed since the Master Plan was adopted, the consistency with recent development trends in the area;

The Latson Beck property (Parcel 11-09-300-046) is designated as ICPUD in the Township's Master Plan and is adjacent to the Covenant of Faith property which is already zoned ICPUD (Parcel 11-09-300-040), which adjacent land is also controlled by Applicant.

This response is not related to the question.

- 2. The compatibility of all the potential uses in the PUD with surrounding uses and zoning in terms of land suitability, impacts on the environment, density, nature of use, traffic impacts, aesthetics, infrastructure and potential influence on property values;

The Latson Road interchange was built in 2013, which provided an opportunity to create a well planned mixed use area in accordance with the vision of the Master Plan. Lands to the south and east have already been re-zoned consistent with the Latson Road development vision set forth in the Master Plan. The proposed PUD carries out that vision, as described in further detail in the proposed PUD design guidelines and impact assessment.

Mixed use is not in the Master Plan. This response must address compatibility

- 3. The capacity of infrastructure and services sufficient to accommodate the uses permitted in the requested district without compromising the "health, safety and welfare" of the Township;

Covenant of Faith and its affiliated entities worked closely with the Township, MHOG and County to fund the design and construction of water and sewer utility extensions to serve the area, including the property at issue. The capacity of the public utility system to serve development in this area has been studied and planned for. Recent improvements to the waste water treatment facility have also been performed to accommodate development of the area.

There is no sewer available to this property. What about other infrastructure and services?

4. The apparent demand for the types of uses permitted in the PUD;  
Given the newly constructed interchange on I-96, which is a highly traveled express way transportation corridor, and proximity to Ann Arbor, Lansing and metro Detroit market, there is significant demand for the uses proposed.

There should be evidence of demand provided.

**AFFIDAVIT**

The undersigned says that they are the owners (owner, lessee, or other specified interest) involved in this petition and that the foregoing answers and statements herein contained and the information herewith submitted are in all respects true and correct to the best of his/her knowledge and belief.

BY: \_\_\_\_\_

ADDRESS: 29201 Telegraph Rd., Ste. 410, Southfield, MI 48034

*Contact Information - Review Letters and Correspondence shall be forwarded to the following:*

Todd Wyett of Versa Real Estate at todd@versacos.com  
*Name Business Affiliation E-mail*

And Eric Lord of Atwell at elord@atwell-group.com

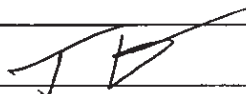


**FEE EXCEEDANCE AGREEMENT**

As stated on the site plan review fee schedule, all site plans are allocated two (2) consultant reviews and one (1) Planning Commission meeting. If additional reviews or meetings are necessary, the applicant will be required to pay the actual incurred costs for the additional reviews. If applicable, additional review fee payment will be required concurrent with submittal to the Township Board. By signing below, applicant indicates agreement and full understanding of this policy.

PROJECT NAME: Latson Road / I-96 Interchange Commercial

PROJECT LOCATON & DESCRIPTION: South of Beck and east of Latson Road

SIGNATURE:  DATE: \_\_\_\_\_

PRINT NAME: Todd Wyett PHONE: 248-770-8484

COMPANY NAME & ADDRESS: Versa Real Estate, 29201 Telegraph Rd., Ste. 410, Southfield, MI 48034



**GENOA CHARTER TOWNSHIP**  
**Application for Site Plan Review**

**TO THE GENOA TOWNSHIP PLANNING COMMISSION AND TOWNSHIP BOARD:**

Todd Wyett 29201 Telegraph Rd., Ste. 410, Southfield,  
APPLICANT NAME & ADDRESS: MI 48034

*If applicant is not the owner, a letter of Authorization from Property Owner is needed.*  
Todd Wyett 29201 Telegraph Rd., Ste. 410, Southfield,  
OWNER'S NAME & ADDRESS: MI 48034

11-09-300-046  
SITE ADDRESS: \_\_\_\_\_ PARCEL #(s): 11-09-300-040

APPLICANT PHONE: ( 248 ) 770-8484 OWNER PHONE: ( 248 ) 770-8484

OWNER EMAIL: todd@versacos.com

LOCATION AND BRIEF DESCRIPTION OF SITE: \_\_\_\_\_

The site is located south of the Latson Road Interchange with I-96, east of Latson between Beck Rd and the Railroad.

BRIEF STATEMENT OF PROPOSED USE: \_\_\_\_\_

The area is intended for supportive commercial use as indicated in the Township Master Plan.

THE FOLLOWING BUILDINGS ARE PROPOSED: To be determined.

**I HEREBY CERTIFY THAT ALL INFORMATION AND DATA ATTACHED TO AND MADE PART OF THIS APPLICATION IS TRUE AND ACCURATE TO THE BEST OF MY KNOWLEDGE AND BELIEF.**

BY: Todd Wyett


ADDRESS: 29201 Telegraph Rd., Ste. 410, Southfield, MI 48034

**Contact Information** - Review Letters and Correspondence shall be forwarded to the following:

1.) <u>Todd Wyett</u>	of <u>Versa Real Estate</u>	at <u>todd@versacos.com</u>
<small>Name</small>	<small>Business Affiliation</small>	<small>E-mail Address</small>
<u>Eric Lord</u>	<u>Atwell Group</u>	<u>elord@atwell-group.com</u>

**FEE EXCEEDANCE AGREEMENT**

As stated on the site plan review fee schedule, all site plans are allocated two (2) consultant reviews and one (1) Planning Commission meeting. If additional reviews or meetings are necessary, the applicant will be required to pay the actual incurred costs for the additional reviews. If applicable, additional review fee payment will be required concurrent with submittal to the Township Board. By signing below, applicant indicates agreement and full understanding of this policy.

SIGNATURE:  DATE: 2/22/2024

PRINT NAME: Todd Wyett PHONE: 248-770-8484

ADDRESS: 29201 Telegraph Rd., Ste. 410, Southfield, MI 48034



**GENOA CHARTER TOWNSHIP**  
**Application for Re-Zoning**

APPLICANT NAME: Latson Beck, LLC ADDRESS: 29201 Telegraph Rd, ste 410, Southfield, MI 48034  
OWNER NAME: Latson Beck, LLC ADDRESS: Same as above  
PARCEL #(s): 11-09-300-046 PRIMARY PHONE: ( 248 ) 770-8484  
EMAIL 1: todd@versacos.com EMAIL 2: elord@atwell-group.com

We, the undersigned, do hereby respectfully make application to and petition the Township Board to amend the Township Zoning Ordinance and change the zoning map of the township of Genoa as hereinafter requested, and in support of this application, the following facts are shown:

**A. REQUIRED SUBMITTAL INFORMATION**

1. A legal description and street address of the subject property, together with a map identifying the subject property in relation to surrounding properties;
2. The name, signature and address of the owner of the subject property, a statement of the applicant's interest in the subject property if not the owner in fee simple title, and proof of consent from the property owner;
3. It is desired and requested that the foregoing property be rezoned from:

CE to ICPUD.

4. A site plan illustrating existing conditions on the site and adjacent properties; such as woodlands, wetlands, soil conditions, steep slope, drainage patterns, views, existing buildings, sight distance limitations, relationship to other developed sites. and access points in the vicinity;
5. A conceptual plan demonstrating that the site could be developed with representative uses permitted in the requested zoning district meeting requirements for setbacks, wetland buffers access spacing, any requested service drives and other site design factors;
6. A written environmental impact assessment, a map of existing site features as described in Article 18 describing site features and anticipated impacts created by the host of uses permitted in the requested zoning district;
7. A written description of how the requested rezoning meets Sec. 22.04 "Criteria for Amendment of the Official Zoning Map."
8. The property in question shall be staked prior to the Planning Commission Public Hearing.

**B. DESCRIBE HOW YOUR REQUESTED RE-ZONING MEETS THE ZONING ORDINANCE CRITERIA FOR AMENDING THE OFFICIAL ZONING MAP:**

1. How is the rezoning consistent with the goals, policies and future land use map of the Genoa Township Master Plan, including any subareas or corridor studies. If not consistent, describe how conditions have changed since the Master Plan was adopted?

The Latson Beck property (Parcel 11-09-300-046) is designated as ICPUD in the Township's Master Plan and is adjacent to  
the Covenant of Faith property which is already zoned ICPUD (Parcel 11-09-300-040), which adjacent land is also controlled  
by Applicant.

This response should include an analysis of consistency with the Master plan.

2. Are the site's physical, geological, hydrological and other environmental features suitable for the host of uses permitted in the proposed zoning district?

The Latson Road interchange was built in 2013, which provided an opportunity to create a well planned mixed use area in accordance with the vision of the Master Plan. Lands to the south and east have already been re-zoned consistent with the Latson Road development vision set forth in the Master Plan

This answer doesn't match the question.

3. Do you have any evidence that a reasonable return on investment cannot be received by developing the property with one (1) of the uses permitted under the current zoning?

Developing under the existing CE zoning would be inconsistent with the Twp master plan and does not present a reasonable return on investment given the proximity to the I-96 interchange.

This question asks for evidence. Please provide evidence.

4. How would all the potential uses allowed in the proposed zoning district be compatible with surrounding uses and zoning in terms of views, noise, air quality, the environment, density, traffic impacts, drainage and potential influence on property values?

Lands to the south and east have already been re-zoned consistent

with the Latson Road development vision set forth in the Master Plan. The proposed PUD carries out that vision, as described in further detail in the proposed PUD design guidelines and impact assessment

5. Are infrastructure capacity (streets, sanitary sewer, water, and drainage) and services (police and fire protection, etc.) sufficient to accommodate the uses permitted in the requested district?

Water and sewer utility extensions to serve the area have already been constructed.

This property is not served by sanitary sewer and this response should address all infrastructure and services.

6. Is there a demonstrated demand in Genoa Township or the surrounding area for the types of uses permitted in the requested zoning district? If yes, explain how this site is better suited for the zoning than others which may be planned or zoned to accommodate the demand.

Given the newly constructed interchange on I-96, which is a highly traveled express way transportation

corridor, and proximity to Ann Arbor, Lansing and metro Detroit market, there is significant demand

for the uses proposed.

The purported demand should be demonstrated and all parts of the question should be addressed.

7. If you have a particular use in mind, is another zoning district more appropriate? Why should the Township re-zone the land rather than amend the list of uses allowed in another zoning district to accommodate your intended use?

The list of uses in the CE district is not compatible with the Master Plan for the ICPUD development of the Latson Rd corridor.

This response does not answer the question.



8. Describe any deed restrictions which could potentially affect the use of the property.

None

**C. AFFIDAVIT**

The undersigned says that they are the Owner (owner, lessee, or other specified interest) involved in this petition and that the foregoing answers and statements herein contained and the information herewith submitted are in all respects true and correct to the best of his/her knowledge and belief.

BY: Todd Wyett

ADDRESS: 29201 Telegraph Rd., Ste. 410, Southfield, MI 48034

SIGNATURE

The following contact should also receive review letters and correspondence:

Name: Eric Lord Email: elord@atwell-group.com


Business Affiliation: Engineer

**FEE EXCEEDANCE AGREEMENT**

As stated on the site plan review fee schedule, all site plans are allocated two (2) consultant reviews and one (1) Planning Commission meeting. If additional reviews or meetings are necessary, the applicant will be required to pay the actual incurred costs for the additional reviews. If applicable, additional review fee payment will be required concurrent with submittal to the Township Board. By signing below, applicant indicates agreement and full understanding of this policy.

PROJECT NAME: Interchange Commercial PUD

PROJECT LOCATON & DESCRIPTION: Latson Road south of I-96

SIGNATURE:  DATE: 2/22/2024

PRINT NAME: Todd Wyett PHONE: (248) 770-8484

COMPANY NAME & ADDRESS: Versa Real Estate

September 10, 2024

Planning Commission  
 Genoa Township  
 2911 Dorr Road  
 Brighton, Michigan 48116

<b>Attention:</b>	Amy Ruthig, Planning Director
<b>Subject:</b>	Versa Development – Interchange Commercial PUD (Review #2)
<b>Location:</b>	East side of Latson Road, between Beck Road and the rail line
<b>Zoning:</b>	CE Country Estate and ICPUD Interchange Commercial Planned Unit Development

Dear Commissioners:

At the Township’s request, we have reviewed the revised submittal from Versa Development requesting PUD rezoning and conceptual PUD plan review for 13.18 acres of undeveloped land generally located east of Latson Road between Beck Road and the rail line.

It is important to note that 5.74 acres of the subject area is already zoned ICPUD as part of the original Innovation Interchange PUD, but is being transferred into this newly proposed PUD via an amendment.

**A. Summary**

**1. PUD Qualifying Conditions (Section 10.02):**

- a. The Township may reduce the minimum site area provided “the design elements of a proposed development are integrated into and consistent with the broader Master Plan Latson Road Subarea Plans with compatible land uses.”
- b. The application form states that public utilities are available; however, the Impact Assessment identifies the sewer extension as proposed (water is currently available).
- c. We suggest the applicant provide a Utility Construction Agreement as part of this project.
- d. The applicant must address any technical comments provided by the Township Engineer and/or Utilities Director.

**2. Rezoning Criteria (Section 22.04):**

- a. The proposed zoning designation of ICPUD is consistent with the I-96/Latson Road Subarea Plan and goals of the Township Master Plan.
- b. If the wetland is regulated, it should be blended into the overall site design.
- c. The applicant must address any technical comments provided by the Township’s engineering consultant, Utilities Director and/or Brighton Area Fire Authority.
- d. Rezoning is necessary to implement the vision and goals of the I-96/Latson Road Subarea Plan.

**3. Conceptual PUD Plan, including PUD Agreement and Design Guidelines (Section 10.03.06):**

- a. There is an inconsistency between the PUD Agreement and Design Guidelines with respect to the height of a hotel.
- b. The applicant must address any comments provided by the Township’s engineering consultant and/or the Livingston County Road Commission with respect to the Traffic Impact Study.
- c. The applicant must address any comments provided by the Utilities Director.
- d. The applicant must address staff and/or Township Attorney comments.



*Aerial view of site and surroundings (looking north)*

## **B. Proposal/Process**

The request is to create an Interchange Commercial Planned Unit Development (ICPUD) for 13.18 acres of land generally located east of Latson Road between Beck Road and the rail line.

The proposal includes a 5.74-acre parcel that is already zoned ICPUD as part of the original Innovation Interchange PUD. This change is included in a proposed amendment to the original PUD.

At this time, the applicant seeks Planning Commission consideration of ICPUD rezoning for 7.44 acres of land, the conceptual PUD plan, Environmental Impact Assessment and draft PUD Agreement.

Following a public hearing, the Commission may put forth recommendations to the Township Board, who has final approval authority.

## **C. Qualifying Conditions**

We have reviewed the request for compliance with the PUD Qualifying Conditions (Section 10.02), as follows:

- 1. Single Ownership.** Per the PUD application form, “the property is owned by several owners under single control.”
- 2. Initiated by Petition.** The request has been properly initiated by submittal of the required application forms and materials.
- 3. Minimum Site Area.** Section 10.02.03 requires a minimum of 20 acres for the establishment of a PUD; however, there are instances where the Township Board may reduce this requirement.

For Interchange PUDs in particular, the Ordinance states that “the Township Board may waive the minimum lot area where the design elements of a proposed development are integrated into and consistent with the broader Master Plan Latson Road Subarea Plans with compatible land uses.”

Provided the Commission (and ultimately the Board) find this to be the case, the minimum site area may be reduced accordingly.

- 4. Benefits.** The PUD will provide for a complementary mix of commercial uses, enhanced streetscaping, building design and site elements, pedestrian and vehicular connectivity, and public infrastructure improvements, including dedication of land for an expanded right-of-way.

- 5. Sewer and Water.** The PUD application form states that “the property is served by both public water and sewer.” However, the Impact Assessment indicates sanitary sewer extensions are proposed/needed (though water is currently available).

The original Innovation Interchange PUD included a Utility Construction Agreement. While the PUD Agreement notes the need for public utilities, we suggest the applicant provide a Utility Construction Agreement as part of this project.

The applicant must address any technical comments provided by the Township’s engineering consultant and/or Utilities Director under this criterion.

#### **D. Rezoning Criteria**

We have reviewed the request for compliance with the Criteria for Amendment of the Official Zoning Map (Section 22.04), as follows:

- 1. *Consistency with the goals, policies and future land use map of the Genoa Township Master Plan, including any subarea or corridor studies. If conditions have changed since the Master Plan was adopted, the consistency with recent development trends in the area.***

The Township Master Plan and Future Land Use map identify the subject site as Interchange Commercial, which is consistent with the proposed ICPUD zoning designation.

The Latson/I-96 Subarea Plan provides the following statements applicable to the proposal:

- The areas immediately south of the interchange along S. Latson Road are planned for Interchange Commercial. This area is intended to accommodate the needs of interstate traffic and should complement, not duplicate, the commercial areas north along Latson and Grand River.
- A diversified mixture of uses that may include commercial and office/research and development.
- A mixture of uses that will diversify traffic generated from the site by spreading out the peak hour over times that minimize impact to the interchange’s peak hour traffic.
- Distinct and prominent architectural features of enhanced character, which reflect the importance of the site’s location and create a positive visual landmark for this gateway to the community.
- Extensive landscaping along Latson Road and Grand River Avenue to enhance the appearance of these corridors and the gateway to the community.
- Uniformity in design through coordination of architectural styles, landscaping, ornamental lighting, pedestrian circulation and vehicular access.

Based on the submittal materials, the proposal is generally consistent with the Township Master Plan, including the Latson/I-96 Subarea Plan.

- 2. *Compatibility of the site's physical, geological, hydrological and other environmental features with the host of uses permitted in the proposed zoning district.***

The subject site contains a small wetland area, though there is no indication whether it is regulated by the State.

If the wetland is regulated, it should be blended into the site design with a minimum 25-foot setback for any buildings or structures. The Design Guidelines have been revised to require natural feature setback demarcation signs, as requested.

Based on the materials submitted, including the updated Environmental Impact Assessment, we do not foresee any issues under this criterion; however, the applicant must address any concerns raised by the Township's engineering consultant.

**3. *The ability of the site to be reasonably developed with one (1) of the uses permitted under the current zoning.***

In 2013, the Township Master Plan was updated to include an I-96/Latson Road Subarea Plan in anticipation of the new interchange.

The Subarea Plan was developed with an understanding that the new interchange would create development opportunities not allowed under CE zoning.

Accordingly, the Township's vision for the Interchange area cannot be accomplished under CE zoning, which is primarily intended for single-family residential on 5-acre lots.

**4. *The compatibility of all the potential uses allowed in the proposed zoning district with surrounding uses and zoning in terms of land suitability, impacts on the environment, density, nature of use, traffic impacts, aesthetics, infrastructure and potential influence on property values.***

Section 10.03.06(c) of the Zoning Ordinance reads as follows:

*ICPUD: permitted land uses include restaurants (fast food, sit-down, and take out), auto/gasoline service stations, retail/service, hotels, entertainment (movie theaters, indoor commercial recreation, etc.), conference centers, financial institutions, and offices. The Township may permit additional compatible uses as part of the approval process.*

The revised use table incorporates comments from our initial review letter, and is consistent with the uses allowed in the Zoning Ordinance (as noted above).

**5. *The capacity of Township infrastructure and services sufficient to accommodate the uses permitted in the requested district without compromising the "health, safety and welfare" of the Township.***

The applicant must address any comments provided by the Township engineering consultant, Utilities Director and/or Brighton Area Fire Authority related to this criterion.

**6. *The apparent demand for the types of uses permitted in the requested zoning district in the Township in relation to the amount of land in the Township currently zoned to accommodate the demand.***

Similar to comments under criterion #3 above, the Township has planned for this area to be developed as an Interchange Commercial PUD in accordance with the I-96/Latson Road Subarea Plan (originally adopted in 2013).

**7. *Where a rezoning is reasonable given the above criteria, a determination the requested zoning district is more appropriate than another district or amending the list of permitted or Special Land Uses within a district.***

Rezoning to ICPUD to implement the Master Plan and I-96/Latson Road Subarea Plan, is more appropriate than another zoning district or amending host of allowable uses in CE.

**8. *The request has not previously been submitted within the past one (1) year, unless conditions have changed or new information has been provided.***

No rezoning requests for the subject property have been submitted in the past year.

## **E. Conceptual PUD Plan**

We have reviewed the request for compliance with the standards of Section 10.03.06, as follows:

- 1. Land Use.** The revised use table is consistent with the Zoning Ordinance.
- 2. Dimensional Standards.** Per Section 10.06.03(d), ICPUDs are to meet the dimensional standards for the RCD zoning district.

The Design Guidelines match RCD dimensional standards, save for the proposed height increase allowed specifically for hotels (57 feet/4 stories or 65 feet/5 stories if more than 500 feet from a residence and with special land use approval).

This aspect requires approval by the Township as a dimensional deviation; however, we suggest that it be capped at 57 feet/4 stories and that the language regarding 65 feet/5 stories be removed for consistency with the draft PUD Agreement.

- 3. Site Design.** The Design Guidelines include site design requirements for the development in terms of landscaping, lighting, and connectivity.

As requested, the revised submittal depicts the fuel pump canopy to the east of the building, which will help mitigate its views from Latson Road.

The PUD Agreement also references site amenities, such as pathway connections, seating areas, and bike racks, as required by the Ordinance.

- 4. Architecture.** The Design Guidelines provide detailed descriptions of the building design and material requirements for the development that generally meet or exceed conventional Ordinance standards.
- 5. Access Management and Connectivity.** The conceptual PUD site plan depicts 2 drives on the south side of Beck Road, with no direct access to/from Latson Road, as required.

Ultimately, the proposed spacing between drives on Beck Road must meet the access management standards of the Zoning Ordinance.

The plan includes vehicular and pedestrian connections throughout the site, though the sidewalk should be extended along the entire Beck Road frontage.

The applicant must also implement the recommendations of the Traffic Impact Study, and address comments provided by the Township's engineering consultant.

- 6. Utilities.** We defer technical review to the Township's engineering consultant, Brighton Area Fire Authority and Utilities Director.
- 7. PUD Agreement.** The revised submittal addresses the comments raised in our initial review letter; however, the applicant must address any additional comments provided by Township staff and/or the Township Attorney.
- 8. Impact Assessment.** The submittal includes an updated Environmental Impact Assessment (dated August 27, 2024), as well as an updated Traffic Impact Study (dated July 26, 2024).


The revised Impact Assessment addresses the comments raised in our initial review letter; however, the applicant must address any comments provided by the Township's engineering consultant and/or the Livingston County Road Commission with respect to the Traffic Impact Study.

- 9. Design Guidelines.** The revised submittal addresses the comments raised in our initial review letter.

Genoa Township  
**Versa Development**  
Interchange Commercial PUD (Review #2)  
Page 6

Should you have any questions concerning this matter, please do not hesitate to contact our office.

Respectfully,  
**SAFEBUILT**



Brian V. Borden, AICP  
Michigan Planning Manager



September 10, 2024

Ms. Amy Ruthig  
Genoa Township  
2911 Dorr Road  
Brighton, MI 48116

**Re: Latson Road - Versa PUD Rezoning  
Conceptual Site Plan Review No. 4**

Dear Ms. Ruthig:

Tetra Tech conducted a fourth site plan review of the South Latson Commercial PUD submittals last dated August 27, 2024. The plans and impact assessment were prepared by MKSK, Atwell LLC, and Fleis & Vandenbrink on behalf of Todd Wyett and Latson Partners, LLC. The traffic impact study was prepared by Fleis & Vanderbrink. The project site includes approximately 14 acres and is located south of the Latson Road interchange and east of Latson Road between Beck Road and the railroad. The petitioner is requesting to rezone the property from CE to ICPUD. We offer the following comments:

#### **GENERAL**

1. The site plan provided is conceptual and our comments on the engineering design are general in nature.

#### **SANITARY AND WATER SERVICES**

1. The impact assessment shows that this development will connect to the proposed gravity sewer along Latson Road to a proposed pump station. The sewer, pump station, and force main will need to be constructed as part of this development. When the sewer system is designed it will need to be coordinated with the Innovation Interchange PUD on the west side of Latson Road to ensure both PUDs can be served by the proposed pump station.

#### **DRAINAGE AND GRADING**

1. The impact assessment states that a stormwater management system will be designed for the development in accordance with LCDC requirements. The site is tributary to the Marion Genoa Drain that is a county maintained and operated drain. The LCDC office will need to be included in the stormwater master plan development process.

#### **TRAFFIC AND ROAD CONCEPTS**

1. The revised traffic impact study has addressed all our previous comments.



**Ms. Amy Ruthig**  
**Re: Latson Road - Versa PUD Rezoning**  
**Conceptual Site Plan Review No. 4**  
**September 10, 2024**  
**Page 2**

Given the conceptual nature and limited detail of the plans, it is difficult to perform an engineering review. Our general findings are presented above. These should be discussed with the applicant and planning commission and any comments incorporated in future submittals.

Sincerely,

A handwritten signature in blue ink that reads "Shelby Byrne". The signature is written in a cursive, flowing style.

Shelby Byrne, P.E.  
Project Engineer

Marianne McCreary  
Genoa Township Planning Commission  
2911 Dorr Road  
Genoa Township, Michigan 48116

My name is Bonnie Spicker  
and I live at 5606 Mountain Rd.

I am expressing my opposition to the proposed Latson PUD development. Genoa Township already suffers from over-development and a population explosion, and our Township cannot bear the cost, traffic, additional population increase or noise and light pollution of a large commercial or industrial development in this area.

The Proposed development is not compatible with the surrounding area, and would begin a surge of industrial and commercial development that will spread far beyond the current plan and throughout what has long been a quiet residential area of country estate homes, North Shore, and Oak Pointe, and will substantially decrease both the value and the character of these homes.

The uses permitted in this development are not conducive to the intent of the zoning ordinance, and many of the permitted uses, like a large industrial distribution warehouse, would bring significant amounts of light, noise and traffic pollution that were never the intent of the original CAPUD zoning. Moreover, Three Rivers Elementary School with over 600 elementary students would be compromised by the increased safety risk of an industrial district and a high-density housing population across the street.

I moved to Genoa Township because I was seeking a tranquil bedroom community away from urban sprawl, as did many others, and this development invites urban sprawl. Genoa Township has seen too much development in recent years and it is time we started protecting and preserving the character of our community before it disappears.

For these and many other reasons, I ask the township to deny any further rezoning of the Latson - Crooked Lake area and to reconsider significantly restricting or removing the current CAPUD - ICPUD South of Beck Road.

Sincerely,



Rec'd 5/3/24  
HARD COPY ALSO DELIVERED  
TO EACH BOARD AND  
PC MEMBER.

Kelly VanMarter, Township Manager  
Genoa Township  
2911 Dorr Road  
Genoa Township, MI 48116

May 2, 2024

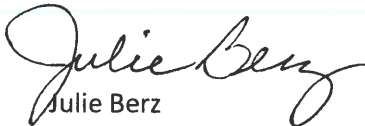
My name is Julie Berz and I live at 3093 Pineview Trail, Genoa Township.

I am expressing my opposition to the proposed Latson PUD development. Livingston County, and particularly Genoa Township, already is negatively affected by over development and an explosion of population. We cannot bear the cost, traffic, additional population increase or noise/light pollution of a large commercial or industrial development in this area.

My husband and I moved to Genoa Township from Oakland County 29 years ago. You can only imagine the urbanization of our former County, where forests and wetlands fell to the almighty god of development making living there sad, crowded and polluted. What Genoa Township lacks is more conserved natural park space, not more ugly development.

Livingston County is becoming overrun by development. I am urging you to put a stop to it. I ask the Planning Commission and Genoa Township Board to deny any further rezoning of the Latson- Crooked Lake area and to reconsider significantly restricting or removing the current CAPUD and ICPUD South of Beck Road.

Sincerely,

  
Julie Berz

Marianne McCreary  
Genoa Township Planning Commission  
2911 Dorr Road  
Genoa Township, Michigan 48116

My name is KEITH SPICHER

and I live at 5606 MOUNTAIN RD BRIGHTON.

I am expressing my opposition to the proposed Latson PUD development. Genoa Township already suffers from over-development and a population explosion, and our Township cannot bear the cost, traffic, additional population increase or noise and light pollution of a large commercial or industrial development in this area.

The Proposed development is not compatible with the surrounding area, and would begin a surge of industrial and commercial development that will spread far beyond the current plan and throughout what has long been a quiet residential area of country estate homes, North Shore, and Oak Pointe, and will substantially decrease both the value and the character of these homes.

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For these and many other reasons, I ask the township to deny any further rezoning of the Latson - Crooked Lake area and to reconsider significantly restricting or removing the current CAPUD - ICPUD South of Beck Road.

Sincerely,



Genoa Township Planning Commission  
2911 Dorr Road  
Genoa Township, Michigan 48116

My name is RON KING.

and I live at 4758 TREASURE LK.

I am expressing my opposition to the proposed Latson PUD development. Livingston County, and particularly Genoa Township, already suffers from over-development and a population explosion, and we cannot bear the cost, traffic, additional population increase or noise and light pollution of a large commercial or industrial development in this area.

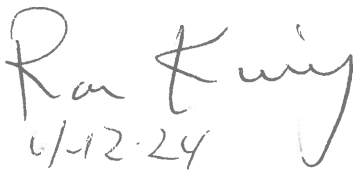
I moved to Livingston County because I was seeking a tranquil bedroom community away from urban sprawl, as did many others, and this development invites urban sprawl. Genoa Township and Livingston County has seen too much development in recent years and it is time we started protecting and preserving the character of our community before it disappears.

Grand River alone has dozens of vacancies for both commercial and industrial use, and no fewer than four additional housing or apartment developments are going up in Genoa Township currently.

The reasons that may have existed five or ten years ago to justify any additional development in this area do not exist today, and Livingston County is becoming overrun by development. I am urging you to put a stop to it.

For these and many other reasons, I ask the township to deny any further rezoning of the Latson - Crooked Lake area and to reconsider significantly restricting or removing the current CAPUD and ICPUD South of Beck Road.

Sincerely,

  
6-12-24

Genoa Township Planning Commission  
2911 Dorr Road  
Genoa Township, Michigan 48116

My name is Pamela King.

and I live at 4758 TREASURE LK DRIVE HOWELL.

I am expressing my opposition to the proposed Latson PUD development. Livingston County, and particularly Genoa Township, already suffers from over-development and a population explosion, and we cannot bear the cost, traffic, additional population increase or noise and light pollution of a large commercial or industrial development in this area.

I moved to Livingston County because I was seeking a tranquil bedroom community away from urban sprawl, as did many others, and this development invites urban sprawl. Genoa Township and Livingston County has seen too much development in recent years and it is time we started protecting and preserving the character of our community before it disappears.

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For these and many other reasons, I ask the township to deny any further rezoning of the Latson - Crooked Lake area and to reconsider significantly restricting or removing the current CAPUD and ICPUD South of Beck Road.

Sincerely, Pamela King  
4-12-24

April 29, 2024

GENOA TOWNSHIP

Genoa Township Board of Trustees  
2911 Dorr Road  
Genoa Township, Michigan 48116

MAY 2, 2024

RECEIVED

My name is Kristine Cook Lindsey  
and I live at 3920 Highcrest Dr., Brighton, MI 48116

I am expressing my opposition to the proposed Latson PUD development. Genoa Township already suffers from over-development and a population explosion, and our Township cannot bear the cost, traffic, additional population increase or noise and light pollution of a large commercial or industrial development in this area.

The Proposed development is not compatible with the surrounding area, and would begin a surge of industrial and commercial development that will spread far beyond the current plan and throughout what has long been a quiet residential area of country estate homes, North Shore, and Oak Pointe, and will substantially decrease both the value and the character of these homes.

The uses permitted in this development are not conducive to the intent of the zoning ordinance, and many of the permitted uses, like a large industrial distribution warehouse, would bring significant amounts of light, noise and traffic pollution that were never the intent of the original CAPUD zoning. Moreover, Three Fires Elementary School with over 600 elementary students would be compromised by the increased safety risk of an industrial district and a high-density housing population across the street.

I live in Genoa Township because I am seeking a tranquil bedroom community away from urban sprawl, as do many others, and this development invites urban sprawl. Genoa Township has seen too much development in recent years and it is time we started protecting and preserving the character of our community before it disappears.

For these and many other reasons, I ask the township to deny any further rezoning of the Latson - Crooked Lake area and to reconsider significantly restricting or removing the current CAPUD South of Beck Road.

Sincerely,



# GENOA TOWNSHIP

APR - 0 2024

RECEIVED

March 28, 2024

To whom it may concern,

My name is Lira Lloyd and I live at 2319 E. Coon Lake Rd. I am expressing my opposition to the proposed Latson PUD development. Genoa Township already suffers from over-development and we are rapidly losing the rural and natural character of the community that we all love and cherish.

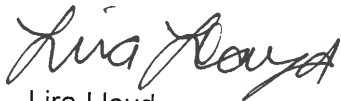
I live in Genoa Township because I do not want to live in the middle of urban sprawl, and this development invites urban sprawl. Genoa Township and Livingston County has seen too much development in recent years and it is time we started protecting and preserving the character of our community before it disappears.

I have serious concerns about the destruction and pollution of the wetlands in and around this proposed development, the Shiawasee and Huron Riversheds, the country drain that runs through this development, and the runoff that will be draining into all of these water sources. I also have significant concerns about any threatened or endangered species that may inhabit this area. Over 80 new species were added to the Endangered Species Act last Spring, and no study has been done to determine whether a development on this nearly 200 acres will destroy one or more essential habitats.

The reasons that may have existed five or ten years ago to justify any additional development in this area do not exist today, and Genoa Township is overrun by development. I am urging you to put a stop to it.

For these and many other reasons, I ask the township to deny any further rezoning of the Latson - Crooked Lake area and to reconsider significantly restricting or removing the current CAPUD South of Beck Road.

Sincerely,



Lira Lloyd



Genoa Township Planning Commission  
2911 Dorr Road  
Genoa Township, Michigan 48116

My name is Claire McCarthy.

and I live at 3222 Hillandale Dr. Howell.

I am expressing my opposition to the proposed Latson PUD development. Livingston County, and particularly Genoa Township, already suffers from over-development and a population explosion, and we cannot bear the cost, traffic, additional population increase or noise and light pollution of a large commercial or industrial development in this area.

I moved to Livingston County because I was seeking a tranquil bedroom community away from urban sprawl, as did many others, and this development invites urban sprawl. Genoa Township and Livingston County has seen too much development in recent years and it is time we started protecting and preserving the character of our community before it disappears.

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For these and many other reasons, I ask the township to deny any further rezoning of the Latson - Crooked Lake area and to reconsider significantly restricting or removing the current CAPUD and ICPUD South of Beck Road.

Sincerely,

*Claire McCarthy*

GENOA TOWNSHIP

MAY 08 2024

RECEIVED

Genoa Township Planning Commission  
2911 Dorr Road  
Genoa Township, Michigan 48116

My name is Claire McCarthy.

and I live at 3222 Hillandale Dr. Howell.

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Sincerely,

*Claire McCarthy*

GENOA TOWNSHIP

MAY 08 2024

RECEIVED

Genoa Township Planning Commission  
2911 Dorr Road  
Genoa Township, Michigan 48116

My name is Pamela King.

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Sincerely,  
*Pamela King*  
4-12-24

Genoa Township Planning Commission  
2911 Dorr Road  
Genoa Township, Michigan 48116

My name is Clarence (Bill) Reiber

and I live at 3154 Stillriver Rd Howell, MI 48843

I am expressing my opposition to the proposed Latson PUD development. Livingston County, and particularly Genoa Township, already suffers from over-development and a population explosion, and we cannot bear the cost, traffic, additional population increase or noise and light pollution of a large commercial or industrial development in this area.

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Sincerely,



**GENOA TOWNSHIP**

**APR 18 2024**

**RECEIVED**

Genoa Township Planning Commission  
2911 Dorr Road  
Genoa Township, Michigan 48116

My name is Todd Walker,

and I live at 4758 Roundtree Drive, Brighton, MI 48116.

I am expressing my opposition to the proposed Latson PUD development. Livingston County, and particularly Genoa Township, already suffers from over-development and a population explosion, and we cannot bear the cost, traffic, additional population increase or noise and light pollution of a large commercial or industrial development in this area.

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Sincerely,



GENOA TOWNSHIP

APR 19 2024

RECEIVED

# COMMUNITY IMPACT ASSESSMENT

## INTERCHANGE COMMERCIAL PUD

August 27, 2024



Prepared By:



ATWELL



In accordance with Section 18.07 of the Genoa Township Zoning Ordinance, this impact assessment describes the Versa property, the intended land uses, the potential impacts, and design features to minimize the negative impacts. Given the size of the property and the range of potential land uses, some portions of this report are general in nature. More specific assessments will be provided when more detailed site plans are submitted for a specific project or phase.

The Interchange Commercial PUD is designated for commercial uses. The scale of the commercial development is intended to meet the needs of employees and visitors to the adjacent Innovation Interchange PUD (a planned development for office, research, light industrial, and warehouse uses) and quick on-and-off trips by motorists along I-96.

**18.07.01 Preparer.**

This statement was prepared by Bradley Strader, AICP, Principal Planner, C2G and Eric Lord, P.E., Vice President, Atwell. A traffic impact study will be submitted separately, prepared by Julie Kroll of Fleis & Vandenbrink.

**Cincar Consulting Group  
(C2G)**  
17199 N. Laurel Park Drive  
Suite #204  
Livonia, MI 48152  
(313) 652-1101  
Bradley Strader, Principal  
[Brad.Strader@itsc2g.com](mailto:Brad.Strader@itsc2g.com)

**ATWELL, LLC**  
Two Towne Square, Suite 700  
Southfield, MI 48076  
(248) 447-2000  
Eric Lord, Vice President  
[elord@atwell-group.com](mailto:elord@atwell-group.com)

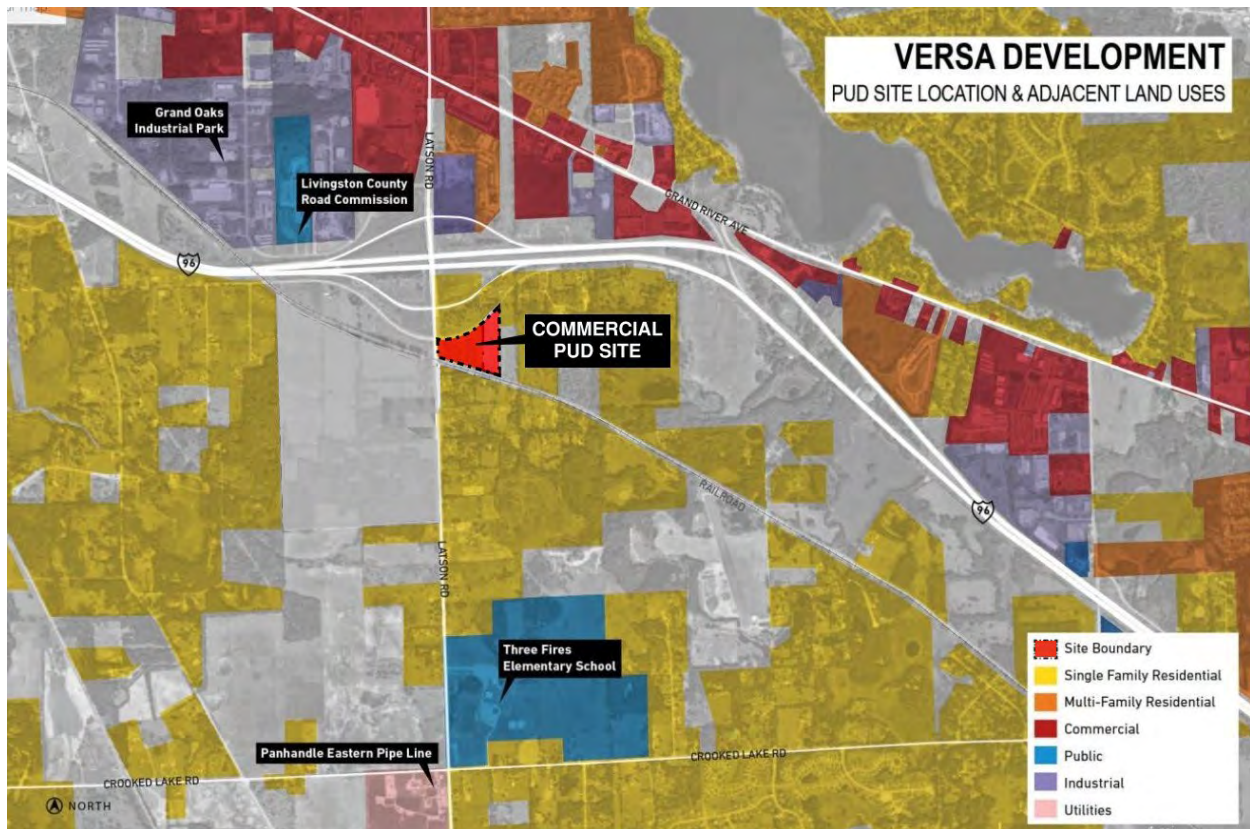
**FLEIS & VANDENBRINK**  
27725 Stansbury St #195  
Farmington Hills, MI 48334  
(248) 536-0080  
Julie Kroll, Traffic Services  
Group Manager  
[jkroll@fveng.com](mailto:jkroll@fveng.com)

**18.07.02 Location.**

The project site includes ±13 acres and is located south of the I-96 Interchange and north of the railroad tracks, along the eastern side of Latson Road. Properties adjacent to the PUD site are the Innovation Interchange PUD to the south, I-96 to the north, and large lot single-family homes or vacant land to the east.

The following parcels are included in the PUD:

- 11-09-300-040
- 11-09-300-046



**18.07.03 Impact on Natural Features.**

The subject property is comprised of approximately 13 acres of land located on the east side of Latson Road, north of the rail road. The property is primarily open, with some evidence of prior farming activity and a few small stands of trees. There appears to be a small, isolated wetland centrally located on the property from localized drainage. Topography generally slopes from north to southeast across the property. We anticipate this property to be developed for commercial use, and as such will likely see impacts to the trees and wetland located in the interior of the site, though opportunities will be explored to preserve trees around perimeter property lines where possible.

There are no Wawasee soils on the soil survey

**18.07.04 Impact on Stormwater Management.**

The topography east of Latson Road generally drains from north to south and continues south to and through a series of low-lying areas and potential wetlands on adjacent property. This area is part of the drainage district for the Marion Genoa Drain.

According to the USDA Natural Resources Conservation Service Soils information, the subject area east of Latson Road is primarily comprised of Wawasee and Miami Loam soil, which is classified as a soils group C. Soils of this type experience low to moderate infiltration with stormwater typically saturating the soil before running off toward lower areas. High groundwater is not anticipated. These soil types do not generally limit development of land.

There is a fair amount of grade change to the property, falling approximately 16 feet from northwest to southeast. Development of the property will be designed to maintain similar drainage patterns to what occurs now. A stormwater management system will be designed for the development in accordance with the requirements of the Livingston County Drain Commissioner’s office, which will include:

- Water quality measures
- Stormwater detention sized for the 100-year storm event
- Soil erosion control

The stormwater for this property (including post development ) was included in MDOT's design for the interchange basins. This will impact the storm water management requirements and should be evaluated and addressed as part of this assessment.



This paragraph is a duplicate from the Innovation Park PUD and isn't directly related to this rezoning. It should be revised to improve applicability to the subject property.

Large Lot Residential use is not anticipated in the transition area.

We anticipate the detention basin will be strategically located at or near the existing low points of the property where stormwater is currently leaving the site. The basin will retain the water for a period with a restricted release to maintain the current drainage patterns from the property. As mentioned earlier, the subject area is tributary to the Marion Genoa Drainage District which is the ultimate receiving water course.

A soil erosion control permit will be obtained prior to construction from Livingston County which will require the site to be managed to control erosion created by construction activity. Examples of erosion control measures that are typically deployed during site development include:

- Silt fencing and vegetative buffer strips to keep soil contained within the construction area.
- Mud Mats at construction entrances to avoid tracking onto public roads.
- Inlet protection – silt sacks in catch basins to avoid sediment buildup in storm pipes and ponds.
- Stone Rip Rap – at culvert outlets to reduce scour and erosion.
- Seed and mulch – of graded areas to promote vegetation growth, which is key to controlling erosion. established.

there should be evidence of this demand. There is available land and significant vacancies associated with commercial development in Genoa Township.

#### 18.07.05 Impact on Surrounding Land Use.

The Genoa Township Master Plan (2023) designates the Latson Road corridor south of the new I-96 Interchange as an area to concentrate new development, with a goal of an “Interchange Campus.” Uses contemplated in the Master Plan include research and development facilities, corporate offices, a conference center and hotel, and restaurants and other services that are complementary to the overall development. The site is within the Growth Boundary and designated as a “Primary Growth Area” in the Master Plan. South of the “Interchange Campus” area is what is described in the Master Plan as a “Transitional Area” which anticipates residential use and/or extension of the Interchange Campus area.

The proposed Commercial PUD accommodates those types of complimentary uses to service employees and visitors to the Interchange Campus area. The developer notes that there is significant demand in Livingston County for such uses, and that this location in Genoa Township is very appealing given the proximity to the well-designed I-96 interchange (as compared to many complex freeway interchanges in the county).

As shown on the concept plan, described in the Design Guidelines, and as prescribed in the PUD Agreement, a number of provisions are included to help ensure the development is compatible with the surrounding area. These include:

- Preserved or landscaped buffers adjacent to residential areas.
- Most of the anticipated traffic to and from future development will use the I-96 interchange and higher density development will occur closer to the interchange, helping to minimize traffic impacts to the surrounding area.
- An extensive streetscape and potentially a median along Latson Road to provide an attractive gateway to the PUD and Southern Genoa Township proposed as part of the adjacent interchange campus PUD
- Standards for high quality architectural design for facades visible to the public, including from I-96.
- Lighting standards to help preserve the existing “dark sky” environment.

There is no median proposed along the Latson Road frontage in this area.

All of the development is intended to comply with the operational requirements and performance measures in the Genoa Township Zoning Ordinance. More details regarding types of proposed uses, hours of operation, noise for particular uses, activity during construction periods, etc. will be provided once individual site plans are submitted for development.

This paragraph appears to be associated with the Innovation Park PUD and not this rezoning. This should be revised for accuracy.

**18.07.06 Impact on Public Facilities and Services.**

This section covers the anticipated broad impacts of the Development. Individual uses and site plans submitted in the future may need to provide more information on their particular impacts, depending upon the use. For example, water and sewer needs may vary for a particular use.

Generally, the main impacts will be traffic and public water and sewer, as noted in the sections below. In terms of employees, this will vary depending upon the types of sizes of the individual site plans. It is expected that the impacts on police, fire, emergency response and other Township or County services will be minimal. The tax benefits of the development will provide a high benefits-to-impact ratio, which will benefit the Township.

**18.07.07 Impact on Public Utilities.**

To provide ~~public water and~~ sanitary sewer service to the subject area south of I-96, ~~public extension of those utilities~~ is required. The initial stage to bring utilities to the south side of I-96 has already been completed in accordance with the permitted design plans prepared by Tetra Tech., which is shown on the attached utility exhibits. From there, utilities will be extended south along Latson Road as well as through the development area to service the district as reflected in conceptual utility exhibits. Water service will be provided by the Marion, Howell, Oceola & Genoa Sewer and Water Authority (MHOG). Sanitary sewer service will be provided by the Genoa Oceola Sewer and Water Authority (GO).

Water service is already available to the commercial site via a 12-inch water main, serviced by MHOG, which has been extended from Kohl's across I-96 to Beck Road then west to Latson and south to the northeast corner of the Latson Farms parcel south of the railroad tracks. Once the developments in the South Latson Road area are constructed, the internal watermain will complete the loop to the west.

Sanitary sewer within the proposed South Latson Road development area will consist of gravity sewers that flow to a proposed pump station located internal to the development on the west side of Latson Road adjacent to the Marion-Genoa Drain, the natural low point in the area. A force main will extend north from the pump station through the subject property and cross under I-96 before tapping into the existing sanitary system at Grand Oaks Drive. The area is ultimately serviced by the GO WWTP, which has recently received system capacity upgrades and is able to service the anticipated load from the South Latson Road development area.

Each development proposed within the South Latson Road area will be serviced by public water and sewer, designed to local, County and State requirements. Approximately 1,497 Residential Equivalent Units (REU) is anticipated for the South Latson Road development area with approximately 20 REUs assigned to the Commercial PUD. MHOG standards equate one REU to 250 gallons per day for average daily demand.

Franchise utilities serving the South Latson Road area will include gas, electric, telephone and data. Coordination with those utility providers to bring service to the area will continue as development plans progress.

Please see the Water Distribution Infrastructure and Sanitary Sewer Collection Infrastructure Maps in Appendix.

**18.07.08 Storage and Handling of any Hazardous Materials.**

The proposed gas station east of Latson Road will contain underground fuel storage tanks which will comply with all local, County, State and Federal requirements. Each development proposed within the subject area will be responsible for meeting all storage and handling requirements, as applicable.

This is confusing. If there are no historical and cultural resources, simply state that.

#### **18.07.09 Traffic Impact Study.**

A separate traffic impact study has been prepared by Fleis and Vandenbrink. The study area and contents of this study has been coordinated with the Livingston County Road Commission with a focus on the potential cross section for Latson Road (such as a median), its design, and the preferred location for access points to the PUD along with impacted intersections in the surrounding area. Please refer to this report for a detailed analysis of traffic impacts and recommended improvements.

#### **18.07.10 Historic and Cultural Resources.**

Three of the homes in the proposed development area were built in 1958 and thus are more than 50 years old. However, those homes are not included on the State or National Historic Registers. None of these homes are located on the Commercial PUD properties.

#### **18.07.11 Special Provisions.**

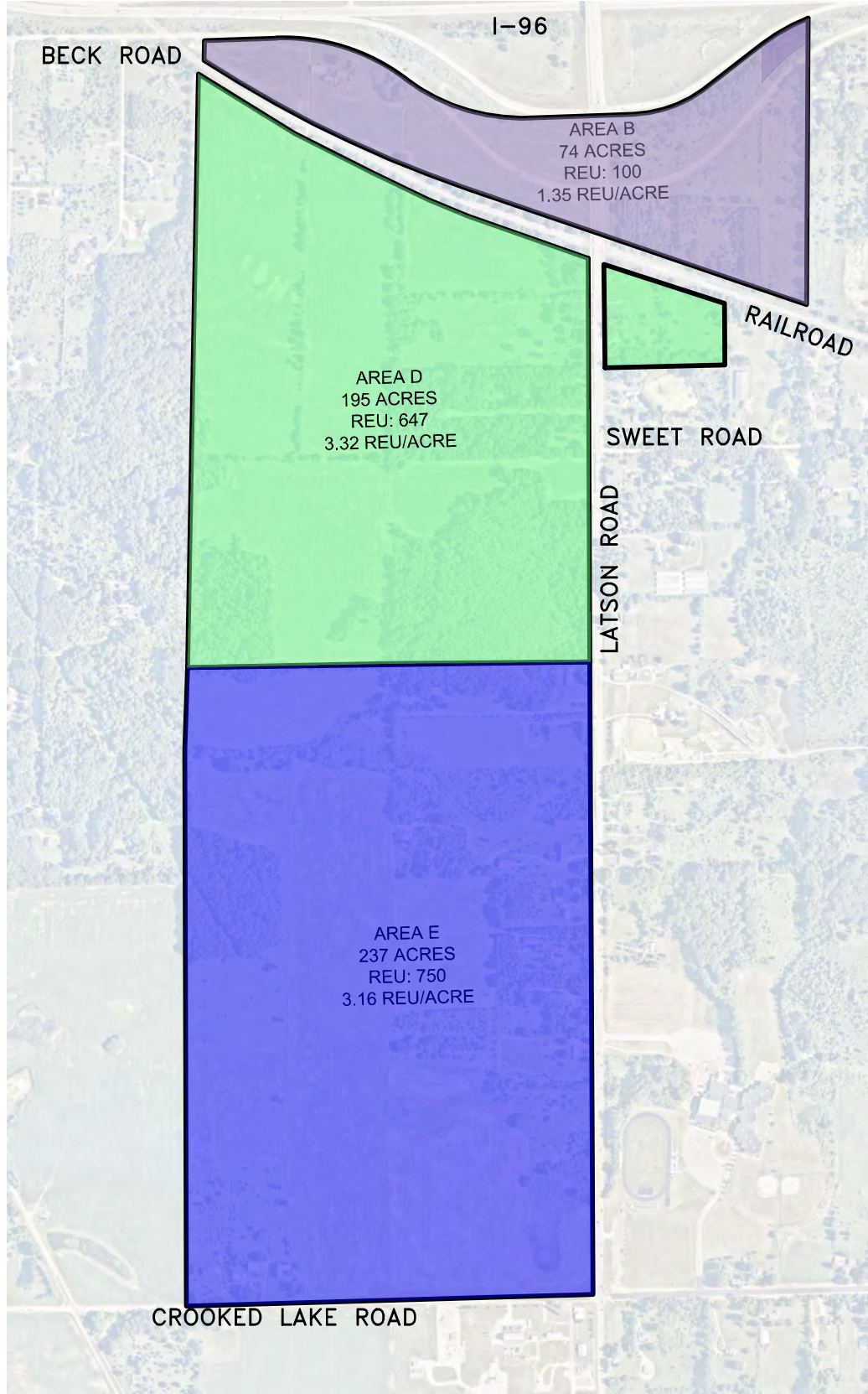
The PUD Agreement contains several provisions regarding the uses, operations, design and other standards that will apply to the Development and future site plans and owners.

#### **Sources:**

- Genoa Township Master Plan
- I-96 Interchange Environmental Impact Statement
- Conversations with the Township and Livingston County Road Commission staff

#### **Appendix:**

- South Latson Road Service Area Map
- PUD REU Allocation Map
- Figure 1: Water Distribution Infrastructure Map
- Water Main Concept Map
- Figure 2: Sanitary Sewer Collection Infrastructure Map
- Sanitary Sewer Concept Map
- Soils and Wetlands Site Map
- Topography and Natural Features Site Map



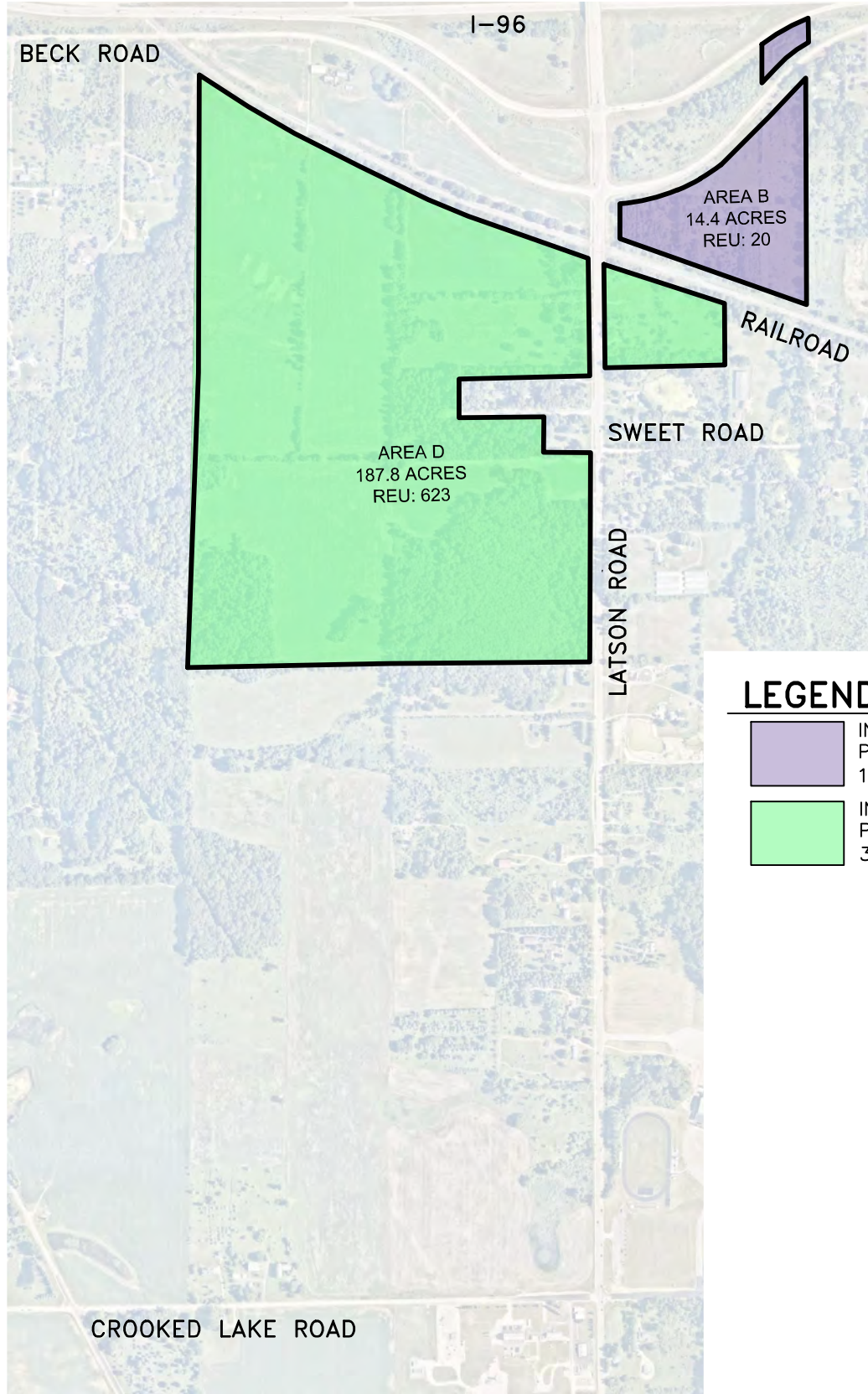
**SOUTH LATSON ROAD SERVICE AREA**

PROJECT NO.: 16001784  
DATE: 2023-08-15



**ATWELL**

866.850.4200 [www.atwell-group.com](http://www.atwell-group.com)



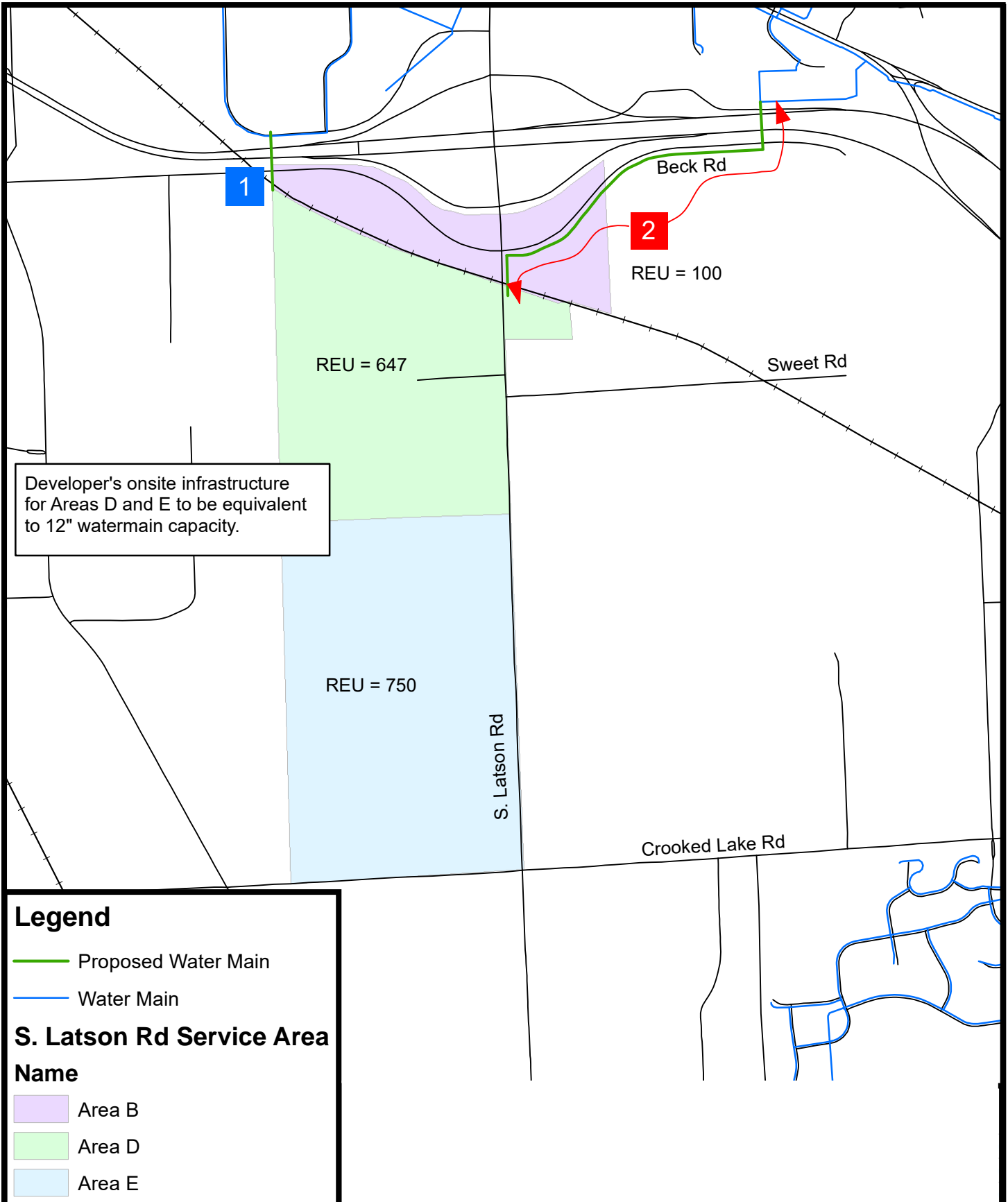
**LEGEND**

- INTERCHANGE COMMERCIAL  
PUD – AREA B  
1.35 REU/ACRE
- INNOVATION INTERCHANGE  
PUD – AREA D  
3.32 REU/ACRE

**INTERCHANGE COMMERCIAL PUD  
REU ALLOCATION**

PROJECT NO.: 16001784  
DATE: 2024-06-07

**ATWELL**  
866.850.4200 www.atwell-group.com



**Legend**

- Proposed Water Main
- Water Main

**S. Latson Rd Service Area Name**

- Area B
- Area D
- Area E

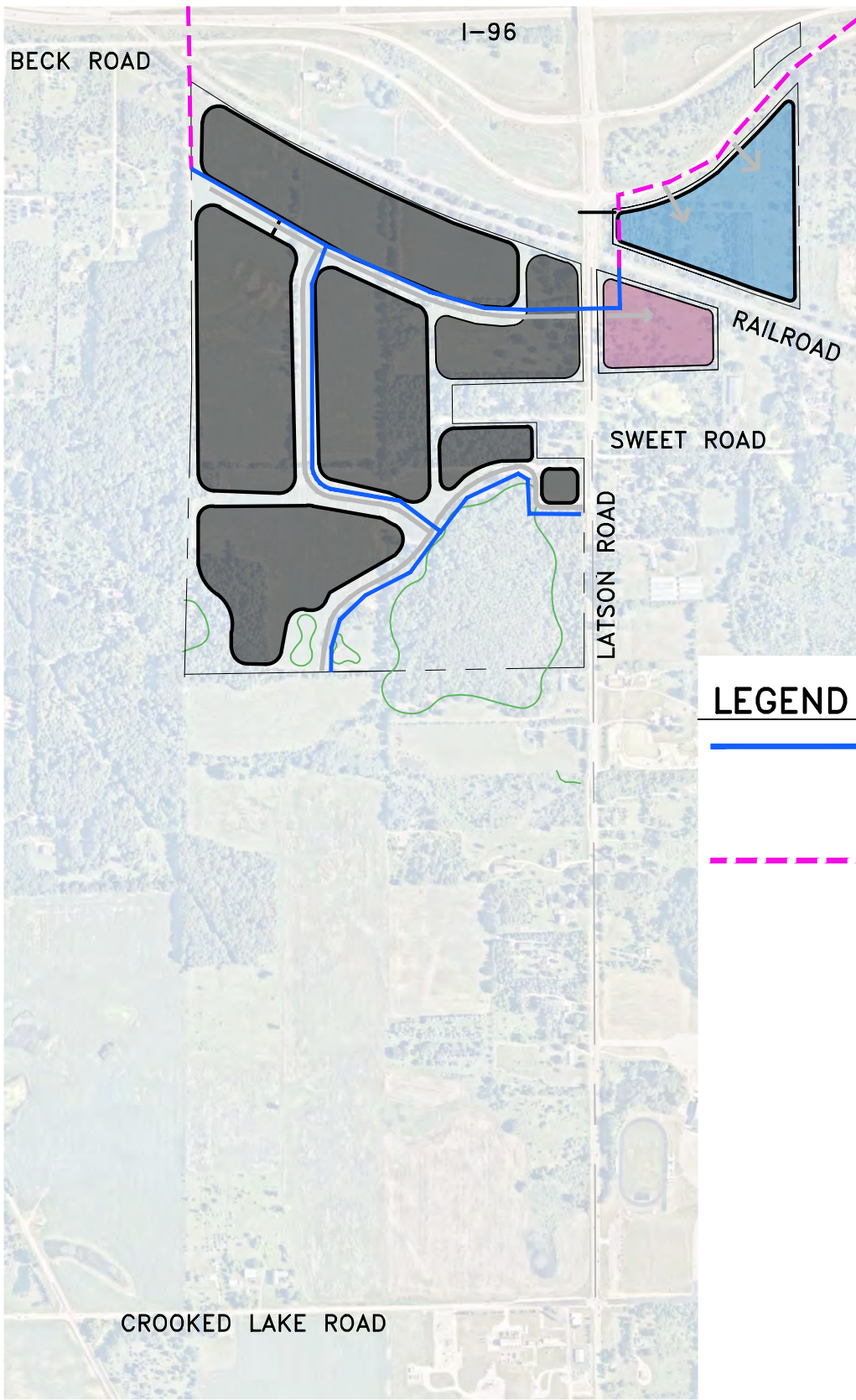
1 inch = 1,500 feet

**Figure 1**  
**Water Distribution Infrastructure**

Date: 3/2/2018

Note: This is a graphical representation of the required improvements. Final routing and location will be required during the design phase.





**LEGEND**

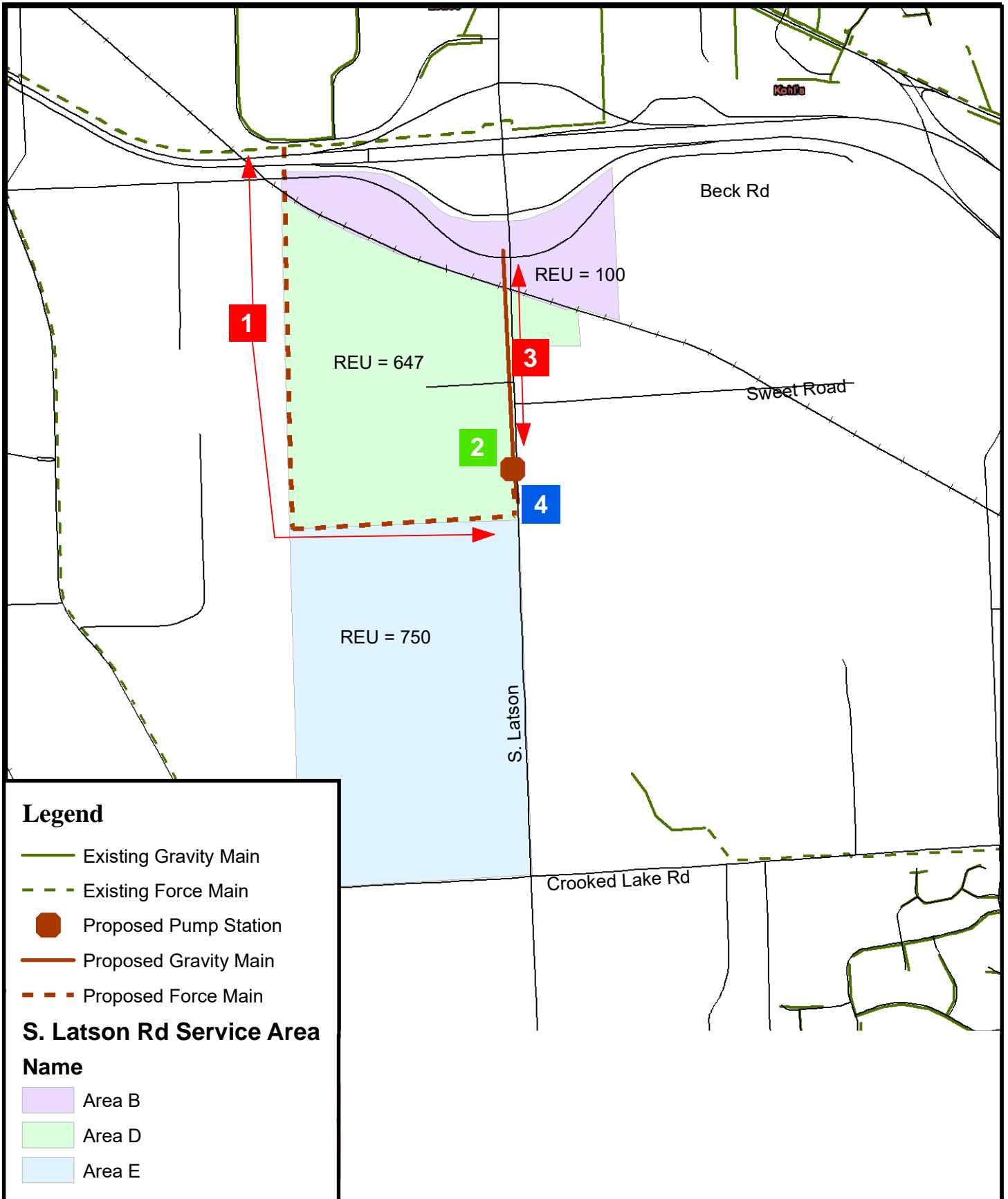
- PR WATERMAIN  
(TO BE INSTALLED AS PART OF THE INNOVATION INTERCHANGE PUD)
- - - - - EX WATERMAIN

**WATERMAIN CONCEPT**

NOTE:  
WATER MAIN TO BE A COMBINATION OF 8" AND 12" IN DIAMETER

PROJECT NO.: 16001784  
DATE: 2024-06-07





1 inch = 1,500 feet

**Figure 2**

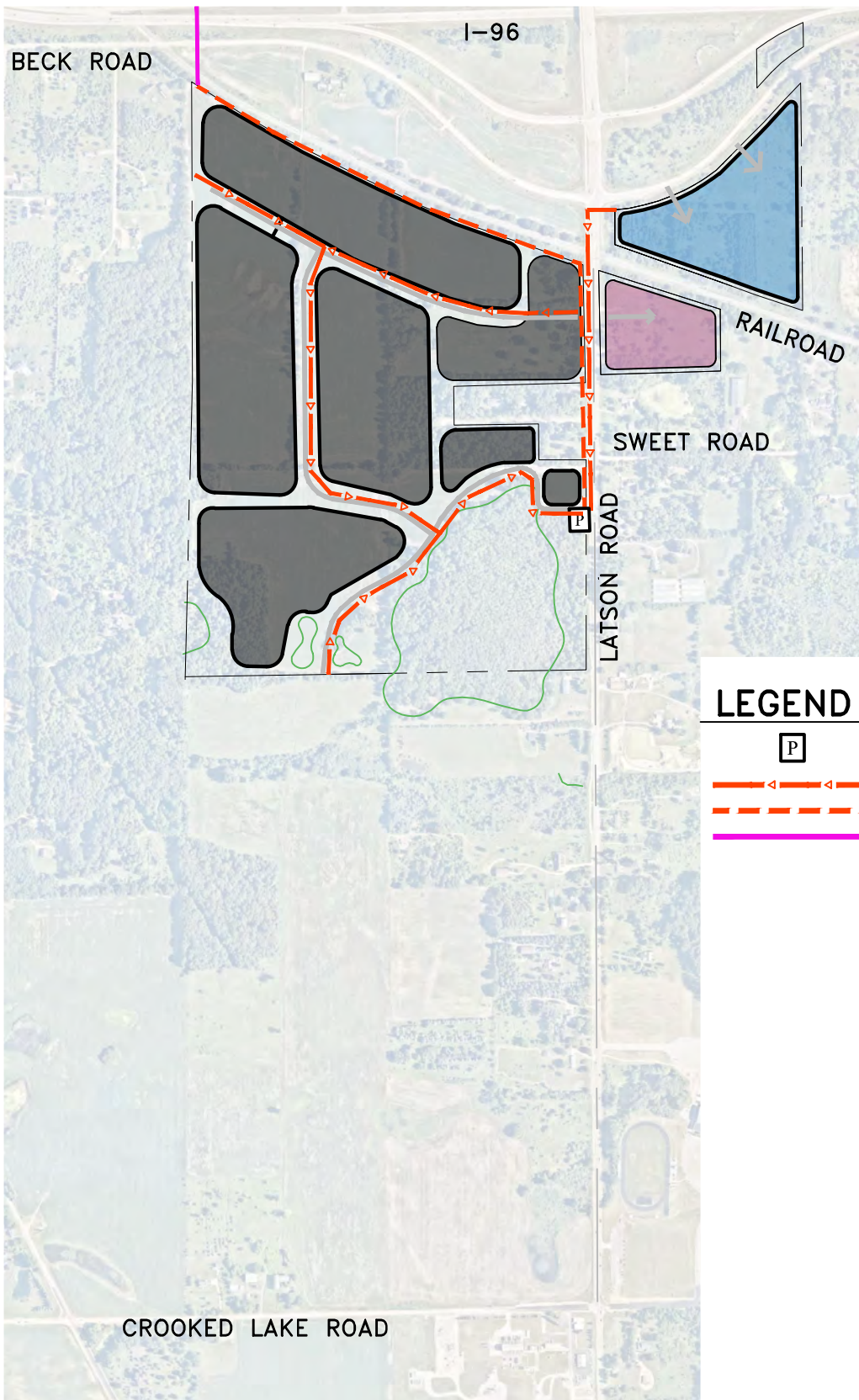
Date: 3/2/2018

**Sanitary Sewer Collection Infrastructure**





Note: This is a graphical representation of the required improvements. Final routing and location will be required during the design phase







**LEGEND**

-  PUMP STATION
-  PR GRAVITY SEWER
-  PR FORCE MAIN
-  EX 8" FORCE MAIN

**SANITARY SEWER CONCEPT**

PROJECT NO.: 16001784  
 DATE: 2024-06-07

NOTE:  
 SANITARY SEWER TO BE A COMBINATION OF 8"  
 AND 10" GRAVITY SEWER AND 8" FORCE MAIN

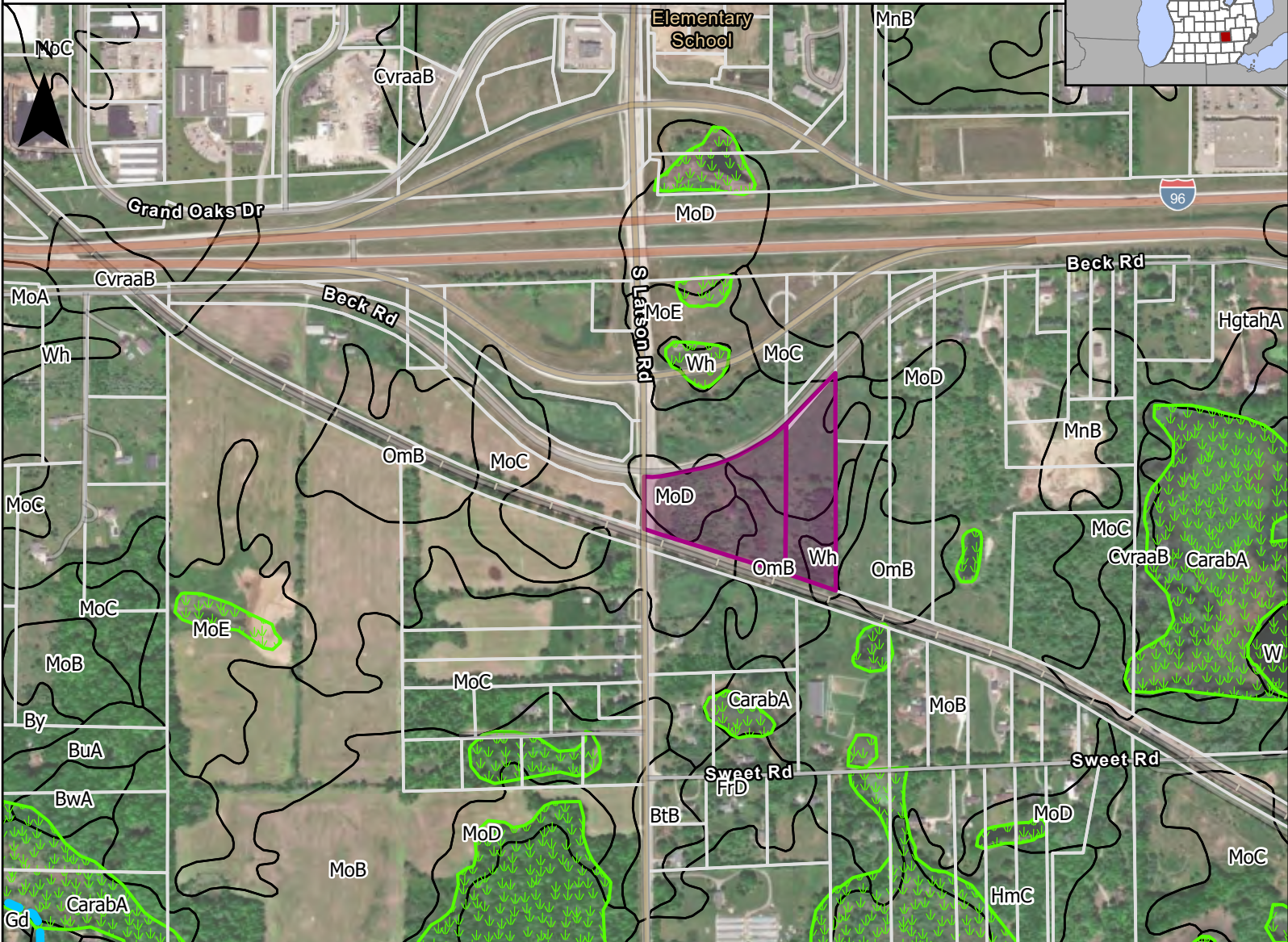


**ATWELL**  
 866.850.4200 www.atwell-group.com

# Interchange Commercial PUD

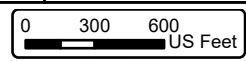
Site Map - Soils and Wetlands

Genoa Township, Livingston County, Michigan



Symbol	Name
BtB	Boyer-Oshtemo loamy sands, 2 to 6 percent slopes
BuA	Brady loamy sand, 0 to 2 percent slopes
FoB	Fox sandy loam, 2 to 6 percent slopes
Gd	Gilford sandy loam, 0 to 2 percent slopes, gravelly subsoil
MnB	Metae loamy sand, 2 to 6 percent slopes
MoB	Wawasee loam, 2 to 6 percent slopes
MoC	Wawasee loam, 6 to 12 percent slopes
MoD	Miami loam, 12 to 18 percent slopes
MoE	Miami loam, 18 to 25 percent slopes
OmB	Owosso-Miami sandy loams, 2 to 6 percent slopes
Wh	Washtenaw silt loam
CvraaB	Conover loam, 0 to 4 percent slopes
CarabA	Carlisle muck, 0 to 2 percent slopes

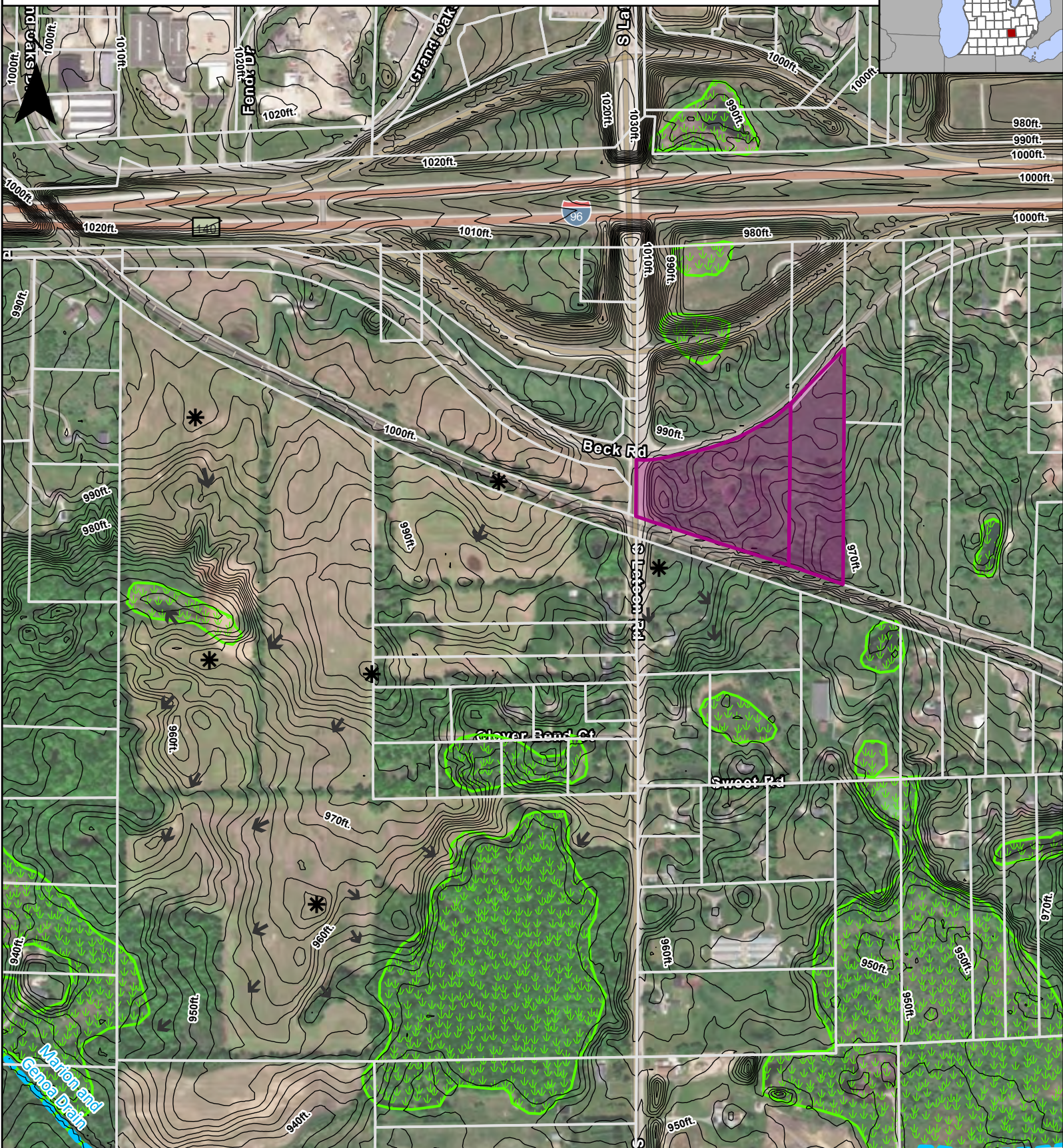
Interchange Commercial PUD  
 Parcel Boundary  
 NWI Wetlands  
 Soil Types



# Interchange Commercial PUD


## Topography & Natural Features

Genoa Township, Livingston County, Michigan



 Interchange Commercial PUD

 Parcel Boundary

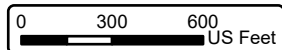
 Local High Point

 NWI Wetlands

 2 Ft Contour

 Estimated Flow Direction

 Watercourse NHD



**PLANNED UNIT DEVELOPMENT AGREEMENT  
(LATSON ROAD/I-96 INTERCHANGE COMMERCIAL)**

This **Planned Unit Development Agreement** (the “Agreement”) is made as of \_\_\_\_\_, 2024 (the “Effective Date”), by and between Latson Beck, LLC, a Michigan limited liability company (“Latson Beck”), and Covenant of Faith, LLC, a Michigan limited liability company (“Covenant of Faith”), whose address is 29201 Telegraph Road, Suite 410, Southfield, Michigan 48034, on the one hand, and the Charter Township of Genoa, a Michigan municipal corporation (the “Township”), whose address is 2911 Dorr Road, Brighton, Michigan 48116, on the other hand. Latson Beck and Covenant of Faith are collectively referred to herein as the “Developer.”

**RECITATIONS**

A. Latson Beck is the owner of approximately 7.44 acres of land located on the east side of Latson Road, south of the I-96 expressway (Parcel No. 11-09-300-046), as depicted on the Parcel Map attached hereto as **Exhibit 1** (the “Latson Beck Property”). Covenant of Faith is the owner of approximately 5.74 acres of land located on the east side of Latson Road, south of the I-96 expressway (Parcel No. 11-09-300-040), which is adjacent to the east side of the Latson Beck Property (the “Covenant of Faith Property”). The Latson Beck Property and Covenant of Faith Property are collectively referred to as the “Property” or “Project Area”.

B. The Latson Road/I-96 interchange was completed in approximately 2013. This new interchange provided the Township with the opportunity to create a new development district for coordinated, well-planned, mixed-use business, light industrial, high tech, office, commercial uses and related development, as described in, among other things, the Township's 2013 Master Plan Update and incorporated by reference in the 2023 Master Plan. The Master Plan designates the Property for use and development as an Interchange Commercial Planned Unit Development (or "ICPUD") which has been incorporated into Article 10 of the Zoning Ordinance.

C. In 2020, Covenant of Faith and other affiliated entities of Covenant of Faith and Latson Beck submitted a request to rezone approximately 177 acres of land located on the west side of Latson Road and another 10 acres on the east side of Latson Road to Campus Planned Unit Development ("CAPUD"); and the Covenant of Faith Property to ICPUD (collectively referred to as the "Innovation Park PUD").

D. At a meeting held on August 3, 2020, the Township Board approved the Innovation Park PUD rezoning, the PUD Plan and execution of a PUD Agreement for the Innovation Park PUD.

E. Covenant of Faith and its affiliated entities and the Township entered into a Planned Unit Development Agreement (the "Innovation Park PUD Agreement") as of September 30, 2020, which was recorded on October 6, 2020, with the Livingston County Register of Deeds.

F. The Latson Beck Property is currently zoned CE, which is not consistent with the Township's Future Land Use Plan for which the area is designated as Interchange Commercial.

G. Latson Beck has submitted an application for Planned Unit Development and to rezone the Latson Beck Property to ICPUD. In that the adjacent Covenant of Faith Property is already zoned ICPUD under the Innovation Park PUD Agreement, the ~~Township and Developer~~ believes ~~agree~~ that it is logical and appropriate that the Covenant of Faith Property should be developed in conjunction with the Latson Beck Property within the scope of this Commercial PUD. The inclusion of these adjacent ICPUD-zoned properties in one PUD would foster a more integrated and coherent development plan consistent with the Master Planning for the ICPUD lands in the Township. The Developer of the Innovation Park PUD is simultaneously pursuing an amendment to the Innovation Park PUD Agreement to remove the Covenant of Faith Property from that PUD. Covenant of Faith is joined as a party to this Agreement to bind the Covenant of Faith Property to the terms and conditions herein.

H. The Township Planning Commission reviewed the rezoning request, the Conceptual PUD Site Plan and Community Impact Statement and conducted a public hearing as required under the Zoning Ordinance. At its meeting held on \_\_\_\_\_, 2024, the Planning Commission recommended approval of the Commercial PUD to the Township Board and Livingston County Planning Commission as satisfying the requirements of the review standards set forth in the Zoning Ordinance.

I. At its meeting held on \_\_\_\_\_, 2024, the Livingston County Planning Commission recommended approval of the Commercial PUD to the Township Board.

J. At its regular meeting held on \_\_\_\_\_, 2024, the Township Board conducted another public hearing on the Project and after finding that the rezoning and Conceptual PUD Site Plan satisfied the standards and objectives of the Zoning Ordinance and Master Plan, approved the Commercial PUD rezoning, the Conceptual PUD Site Plan and execution of this PUD

NOTE - this will require that the amendment is approved prior to the rezoning.

Agreement for the Property, as reflected in the minutes of said meeting attached hereto as **Exhibit 2**, subject to the conditions of this Agreement and other conditions reflected in the meeting minutes. The Board also approved the amendment to the Innovation Park PUD Agreement to, among other things, remove the Covenant of Faith Property from the Innovation Park PUD Agreement so that it would be developed under the terms and conditions of this Agreement.

**NOW, THEREFORE**, in consideration of the foregoing premises, which the Township and Developer represent to be true and accurate, and which shall be incorporated into the parties' obligations set forth herein, the parties intending to be legally bound by this Agreement, agree as follows:

1. **Conceptual Commercial PUD Plan**. The Plan attached hereto as **Exhibit 3** is hereby approved by the Township as the PUD plan for the Project (the "Commercial PUD Plan"). The Commercial PUD Plan is conceptual and illustrative in nature and depicts the general nature and interrelationship of potential uses on the Property. The specific size and nature of any particular building or use and the relationship of such uses and buildings to each other within the Property will be subject to revisions based on the specific uses and businesses that may be attracted to the Property over time.

2. **Permitted Uses**. Notwithstanding anything contained in the Zoning Ordinance to the contrary, but subject to all of the terms and conditions of this Agreement and Exhibits hereto, the Property may be developed for any of the uses or combination of uses set forth in **Exhibit 4** hereto; provided, however, that: (a) a gas station shall not be in the nature of a truck stop; and (b) while a hotel is a permitted use, it is limited in height to 4 stories. ~~The Property is intended to provide, among other things, commercial services to interstate travelers and the interchange~~

This sentence seems to duplicate the intent of the first sentence in this section.

The exhibit uses a single letter S and this should be consistent.

~~campus, as well as residential areas south of I-96.~~ The uses listed as “Prohibited Uses” on **Exhibit 4** shall not be permitted under any circumstances.

S

3. **Special Land Uses.** Any of the uses: designated as “**SLU**” (or Special Land Use) contained in **Exhibits 4**, or any uses similar to or compatible with other special uses not specifically listed in the ICPUD district, as applicable to the Property, or commercial uses permitted by right or special approval in the RCD Zoning District but not listed in **Exhibit 4**, may be permitted upon determination of the Township Board following a recommendation by the Planning Commission as required by Township ordinance 10.03.06(c) in effect as of 2024, and shall be subject to all of the terms and conditions of this Agreement. ~~The parties recognize that all potential future uses may not be listed in the Township Zoning Ordinance or on Exhibits 4 as permitted or special uses, and therefore a non listed use is subject to consideration pursuant to and in compliance with § 10.03.06 (c)(1) of the Zoning Ordinance in effect at the time of executing this Agreement.~~ (Relevant excerpts from the Zoning Ordinance are attached hereto as **Exhibit 5**.)

4. **Development Standards.** The Project is intended to be a focal point of inter-change oriented commercial and other business activity in the community and to attract various commercial businesses that would take advantage of synergy of location and the expressway access and desire to be a part of a high quality, integrated business development plan. The location, design and uses allowed for the Project are intended to supplement and not compete with the Township’s major commercial districts along Grand River Avenue. Individual buildings and site amenities and landscaping are intended to be of high quality and design and include diverse building materials. All development within the Property shall adhere to the Commercial PUD Design Guidelines set forth in **Exhibit 6** hereto.



Prohibit limited access driveways

and Beck Road

There is no highway visibility zone in the PUD Plan

5. ~~**Latson Road Frontage and Highway Visibility Zone.**~~ The facades of the sides of all buildings fronting along Latson Road shall incorporate materials of enhanced durability, including combinations of brick, stone, glass, with permissible metal panel accents and such other equally durable and attractive materials as illustrated by the example facades in the PUD Design Guidelines. ~~Additional screening and landscaping requirements and upgraded building materials as described on page \_\_ of the PUD Design Guidelines shall apply to each portion of a building that is both within the Highway Visibility Zone depicted on the PUD Plan.~~

6. **Future Road Improvements.** All road access to the Property shall be off of Beck Road and not Latson Road. A traffic study was undertaken by Flies & Vandenbrink, dated July 26, 2024 (updated as of August 26, 2024), which recommends that a fully actuated and coordinated traffic signal with permissive/protected southbound left turn phasing be installed at the Latson and Beck Road intersection. While the final decision as to whether and when a traffic signal can be installed at the Beck/Latson intersection is within the jurisdiction of the Livingston County Road Commission, Developer agrees that at such time as the traffic signal is approved and authorized, Developer will install the signal and related improvements at its expense.

7. **Greenbelts.** Landscaped greenbelts shall be installed along the perimeter boundaries of the Property as depicted on the Commercial PUD Plan and as described in the Design Guidelines.

Road Commission

8. **Dedication of Land for Road Right of Way.** In connection with the submission of the first application for site plan approval of any building or development within the Property, Developer shall dedicate to Livingston County without compensation from the Township or the County a strip of land sixty (60') feet in width from the center line of Latson Road along the frontage of all of the Developer's Property on Latson Road. The dedication shall be subject to

Road Commission

What is the existing ROW for Latson? This should be shown on the concept plan. How much additional ROW is needed for 120'.

The Road Commission should be consulted to determine if additional ROW is also desirable on Beck Road.

We need to know existing and future ROW locations on the concept plan before we can agree to this.

Developer's right to include Project signage and landscaping within the dedicated right-of-way until such time as it is used for any widening of Latson or Beck Road, at which time such signage will be relocated at Developer's expense and must comply with State, County and local

law. In the event that the Road Commission should ever determine to (a) improve Latson Road adjacent to the Property, such as by widening the road with or without a median, and/or installation of street lighting, Developer agrees to participate in a special assessment district, or other mechanism mutually agreed upon by the parties, to pay its pro rata share of the costs of such road improvements along the frontage of Developer's Property on Latson Road and for the ongoing maintenance of the landscaping, lighting and other improvements (i.e. walkways) in the right-of-way or within the medians, if constructed. This Agreement constitutes the Developer's approval of including its Property within a special assessment district and approval of the purpose of the assessments, but Developer retains the right to object to or challenge the pro rata allocation of costs among benefitted properties to pay for such improvements and ongoing maintenance of the Improvements as permitted and in compliance with State law. The Developer's obligations hereunder shall be reflected in any condominium or other association agreement and shall run with the land.

This SAD language is not necessary State law provides for this.

9. **Project Amenities.** Project amenities, including pathways connecting the various commercial uses, along with seating areas, bike racks, etc. will be included with each site plan submitted for specific development projects with the Project. In connection with the installation of the traffic signal described in paragraph 6 above, Developer shall install a pedestrian crossing at Beck and Latson Roads.

along road frontage and

fund, install and assume maintenance responsibility for

10. **Off-Site Public Utilities.** The Property is served by public sewer and water. As part of the much larger Innovation Park PUD, the Innovation Park Developer worked with the

This PUD should include a similar Utility Agreement related to the provision of sanitary sewer to this area. This "Payment" was for different project.

Township on the planning, engineering and construction of sewer and water service extensions from north of I-96 from Grand Oaks Drive and Kohl's to points south of the railroad tracks abutting the Property (the "Utility Project"). As provided in the Innovation Park PUD Agreement, the Innovation Park Developer paid the cost of the Utility Project (the "Payment") and undertook the construction and served as construction manager for the Utility Project. The Utility Project, which will ultimately be owned and operated by the Genoa-Oceola Sewer and Water Authority (G-O) and the Marion, Howell, Oceola, and Genoa Sewer and Water Authority (MHOG), was constructed in conformance with the Authority's Engineering Design Standards and Connection Manual, including inspection and testing of the utilities. The Utility Project was intended to serve and has the capacity to serve the Property. The utility plans for connecting sewer and water service to the Property are attached as **Exhibit 7**. Final and more specific engineering details for connection of sewer and water service will be provided with the first site plan to be submitted for the Property. If a site plan for development of a building or use is pursued before the much larger Utility Project is constructed for development within overall Innovation Park PUD, the Parties will work cooperatively and in good faith for an ~~economically~~ ~~feasible~~ interim connection to the public utilities to serve the development in this Commercial PUD.

11. **Reservation of Utilities and Tap Fees.** The Township has allocated <sup>20</sup>100 sewer and water taps (residential equivalency units) capacity to serve the Property. If additional utility capacity is needed in the future to service potential higher generation utility users, the Township <sup>may</sup> ~~shall~~ allocate additional capacity to the extent such capacity is available in the utility systems. In consideration of, among other things, the **Payment**, for a period of ten (10) years following the Township's grant of final site plan and final engineering plan approval for the first phase of any

Payment was for a different project. A Utility Agreement to bring sewer to this parcel should be provided to include consideration of the "payment":

This rate shall be both for 10 years and/or only for the 20 allocated REU's. After 20 REU's the rate should be adjusted to market rate.

development in the Project, Developer shall be entitled to a sewer and tap fee in the amount of \$4,947 per REU for sewer taps and \$4,770 per REU for water taps. These rates have been provided because the Developer's affiliates fully funded the expansion and provision of sanitary sewer and water to serve the Property. Thereafter, the cost of sewer and water taps shall be the ordinary fee in effect at the time such additional water and sewer taps are requested.

12. **Perimeter and Internal Building Setbacks; Height Limitations.** All setback and height standards are set forth in the PUD Design Guidelines and, regardless of any deviation of the PUD Design Guidelines from any existing or future Zoning Ordinance standard or requirement, the PUD Design Guidelines shall govern and apply to the development of the Project. Modifications from such PUD Design Guidelines in connection with the final site planning and engineering for any building or group of buildings may be requested by the Developer and may be granted in the exercise of reasonable discretion by the Township Board upon recommendation of the Planning Commission and upon a showing that such modifications will result in a development consistent with the terms of this Agreement, the Exhibits hereto and the ICPUD Zoning District.

13. **Final Site Plan/Project Phasing.** The Project, including without limitation, Project roadways, amenities and on-site utilities associated with each phase, may proceed in multiple phases, with any phase being a single building or multiple buildings (a "Phase"), and multiple phases may proceed at the same time. The Project may be established as one or more business/commercial condominiums in accordance with the condominium standards of the Zoning Ordinance. In that event, condominium units or sites may be leased by Developer or sold to other parties, including end-user businesses. Any site or unit leased, sold or developed shall be subject to the terms and conditions of this Agreement, which shall run with the land as

described below, and will be subject to condominium documents and/or an agreement regarding covenants, easements and restrictions, in forms approved by the Township for consistency with this Agreement and applicable Township ordinances. The Township shall review such condominium or covenant agreements, and shall approve them to the extent they are consistent with the terms and conditions of this Agreement and other applicable Township ordinances. Any final site plan for a building or phase within the Property shall contain the information required in Article 10.08.02 of the Zoning Ordinance and such final site plan shall be approved if it is consistent with the terms of this Agreement and satisfies other Ordinance requirements. In the event of any conflict between the terms of this Agreement and Exhibits hereto and any current or future Ordinance provision of the Township, this Agreement and Exhibits hereto shall control.

14. **Maintenance Obligations.** The internal roads, signage, pedestrian amenities, lighting, entry features, storm drainage, sidewalks, landscaping and other common elements installed within the interior of development areas shall be initially maintained by the Developer until a condominium or other property owners' association is created and until such condominium or association takes over such maintenance responsibilities in accordance with the condominium or association agreements. Upon assumption of the association's responsibility of such maintenance, the Developer shall have no further obligation hereunder with respect to maintenance of the common improvements.

15. **Timing of Development.** The Commercial PUD Plan shall operate in effect as a master future land use plan for the Project and the following time periods shall apply to the Project:

a. **Expiration of PUD Agreement** – This Agreement shall expire in two (2) years if Developer has not submitted a final site plan for approval of a building or use

within the Commercial PUD Property. This time period may be extended by the Township Board in the exercise of reasonable discretion for up to an additional two (2) years if requested by the Developer in writing prior to the expiration of initial two-year period. An extension shall be granted if the Developer demonstrates good cause. ~~For purposes of this Agreement, a showing of good cause for an extension of time includes a showing of lack of market demand due to economic recession or other conditions, despite good faith and reasonable efforts by the Developer to market such units or sites within the Project.~~ Once a final site plan is approved for a building or project within the Commercial PUD Property and Developer commences construction, this Agreement shall not terminate except by mutual agreement of the Parties in writing.

b. **Expiration of Site Plans** – Individual site plans as required by Township Ordinance for structures and/or private roads and related infrastructure for each phase of the Project are valid for a period of three (3) years after final approval. The approved site plan must be constructed to substantial completion and issuance of a temporary certificate of occupancy within the three (3) years following final approval; otherwise the approval for that site plan is null and void unless an extension is granted by the Township Board following a recommendation by the Planning Commission. Developer shall be entitled to an extension if, as determined by the Planning Commission in the exercise of reasonable discretion, substantial progress has been made to complete the construction pursuant to a final site plan. Substantial progress is defined to include carrying out the terms of the final site plan in good faith, such as obtaining the necessary engineering approvals and permits for construction and, when permits have been issued, pursuing actual physical construction or development of the required improvements identified in the site

plan. Nothing in this paragraph is intended to preclude Developer from pursuing multiple site plans at the same time.

16. **Termination or Expiration of Commercial PUD Plan.** In the event this Agreement expires or terminates for any reason, the rezoning classification shall remain, and any change in the zoning must be by application to the Township and fully compliant with the laws of the State of Michigan. The expiration or termination of this Agreement for any reason does not result in the zoning reverting to its previous classification of Country Estates. Developer may at any time after expiration of the Commercial PUD Plan submit and pursue a new Commercial PUD Plan in accordance with the procedural requirements of the Zoning Ordinance in effect at the time of submission.

Delete this section 17. **Agreement Consistent With Police Powers.** The action of the Township in entering into this Agreement is based upon the understanding that many of the land use, design and environmental objectives of the Township are reflected in the design of the development as proposed and the Township is thus achieving its police power objectives and has not, by this Agreement, bargained away or otherwise compromised any of its police power objectives.

18. **Entire Agreement.** This Agreement, the exhibits attached hereto, if any, and the instruments which are to be executed in accordance with the requirements hereof set forth all the covenants, agreements, stipulations, promises, conditions, and understandings between the Township and the Developer concerning the Project as of the date hereof, and there are no covenants, agreements, stipulations, promises, conditions or understandings, either oral or written, between them other than as set forth herein.

19. **Relationship Of The Parties.** The relationship of the Township and the Developer shall be defined solely by the expressed terms of this Agreement, including the

implementing documents described or contemplated herein, and neither the cooperation of the parties hereunder nor anything expressly or implicitly contained herein shall be deemed or construed to create a partnership, limited or general, or joint venture between the Township and the Developer, nor shall any party or their agent be deemed to be the agent or employee of any other party to this Agreement.

Amendments shall follow the standards of Zoning Ordinance Section 10.11.

20. **Modification.** Except as provided in this Agreement, this Agreement can be modified or amended only by a written instrument expressly referring hereto and executed by the Township and the Developer, its successors and assigns. The PUD Design Guidelines are in effect a living document and may be updated or revised as follows to reflect specific site conditions, special projects or users, changes in market conditions and future trends and best practices in planning and design: (a) minor changes as determined by the Township's professional staff in the exercise of reasonable discretion may be approved administratively; and (b) major changes as determined by the Township's professional staff in the exercise of reasonable discretion shall be submitted to the Township Board for consideration and decision following a recommendation by the Planning Commission. Any change requires the mutual consent of the Township and Developer. To the extent the Property is subdivided in the future either through a site condominium or land division, modifications with respect to any individual parcel or site within the condominium may be made by the owner of the parcel or site and the Township, provided that any such modification does not adversely impact any other property within the Project area, and complies with the Zoning Ordinance.

Is this sentence necessary?

21. **Michigan Law To Control.** This Agreement and the rights and obligations of the parties hereunder shall be construed in accordance with Michigan law.



22. **Due Authorization.** The Township and the Developer each warrant and represent to the other that this Agreement and the terms and conditions thereof have been duly authorized and approved by, in the case of the Township, its Board of Trustees, and as to the Developer, by the appropriate officers or members of the companies constituting the Developer, and that the persons who have executed this Agreement below have been duly authorized to do so.

23. **Agreement To Run With The Land; Recording.** This Agreement shall be binding upon and inure to the benefit of the parties to this Agreement and their respective heirs, successors, assigns and transferees, and shall run with the Property. This Agreement shall be recorded by Developer at its expense with the office of the Livingston County Register of Deeds and a copy provided to the Township.

24. **Counterparts.** It is understood and agreed that this Agreement may be executed in several counterparts, each of which, for all purposes, shall be deemed to constitute an original and all of which counterparts, when taken together, shall be deemed to constitute one and the same agreement, even though all of the parties hereto may not have executed the same counterpart. Delivery via facsimile or PDF transmission of a counterpart of this Agreement as executed by the parties making such delivery shall constitute good and valid execution and delivery of this Agreement for all purposes.

**IN WITNESS WHEREOF**, the parties hereto have executed this Agreement on the date first set forth above.

*[Signatures on following pages]*

The parties hereto have executed this Agreement as of the year and date set forth above.

“DEVELOPER”

Latson Beck, LLC  
a Michigan limited liability company

By: \_\_\_\_\_

Its: \_\_\_\_\_

STATE OF MICHIGAN        )  
                                          ) ss.  
COUNTY OF OAKLAND     )

The foregoing instrument was acknowledged before me this \_\_\_ day of \_\_\_\_\_, 2024, by Todd Wyett, \_\_\_\_\_ of Latson Beck, LLC, a Michigan limited liability company, on behalf of the company.

\_\_\_\_\_  
Notary Public  
\_\_\_\_\_ County, Michigan  
Acting in Oakland County, Michigan  
My Commission Expires: \_\_\_\_\_

“DEVELOPER”

Covenant of Faith, LLC  
a Michigan limited liability company

By: \_\_\_\_\_

Its: \_\_\_\_\_

STATE OF MICHIGAN     )  
                                          ) ss.  
COUNTY OF OAKLAND    )

The foregoing instrument was acknowledged before me this \_\_\_ day of \_\_\_\_\_, 2024, by Todd Wyett, \_\_\_\_\_ of Covenant of Faith, LLC, a Michigan limited liability company, on behalf of the company.

\_\_\_\_\_  
Notary Public  
\_\_\_\_\_ County, Michigan  
Acting in Oakland County, Michigan  
My Commission Expires: \_\_\_\_\_

“TOWNSHIP”

**GENOA TOWNSHIP,**  
a Michigan municipal corporation

By: \_\_\_\_\_  
Its: Supervisor

STATE OF MICHIGAN        )  
                                          ) ss.  
COUNTY OF LIVINGSTON )

The foregoing instrument was acknowledged before me this \_\_\_\_\_ day of \_\_\_\_\_, 2024, by \_\_\_\_\_, Supervisor of Genoa Township, a Michigan municipal corporation, on behalf of the corporation.

\_\_\_\_\_  
Notary Public  
Livingston County, Michigan  
Acting in Livingston County, Michigan  
My Commission Expires: \_\_\_\_\_

and

By: \_\_\_\_\_  
Its: Clerk

STATE OF MICHIGAN        )  
                                          ) ss.  
COUNTY OF LIVINGSTON )

The foregoing instrument was acknowledged before me this \_\_\_\_\_ day of \_\_\_\_\_, 2024, by \_\_\_\_\_, Clerk of Genoa Township, a Michigan municipal corporation, on behalf of the corporation.

\_\_\_\_\_  
Notary Public  
Livingston County, Michigan  
Acting in Livingston County, Michigan  
My Commission Expires: \_\_\_\_\_

Drafted by and when recorded return to:  
Alan M. Greene, Esq.  
Dykema Gossett PLLC  
39577 Woodward Avenue, Suite 300  
Bloomfield Hills, MI 48304

**EXHIBIT 1**

(Parcel Map)

*[Exhibit to Planned Unit Development Agreement (Latson Road Commercial)]*


This should include a survey that shows the ROW.

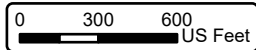
# Interchange Commercial PUD

Parcel Exhibit Map

Genoa Township, Livingston County, Michigan



 Interchange Commercial PUD



**EXHIBIT 2**

(Minutes of Township Board Meeting dated \_\_\_\_\_, 2024)

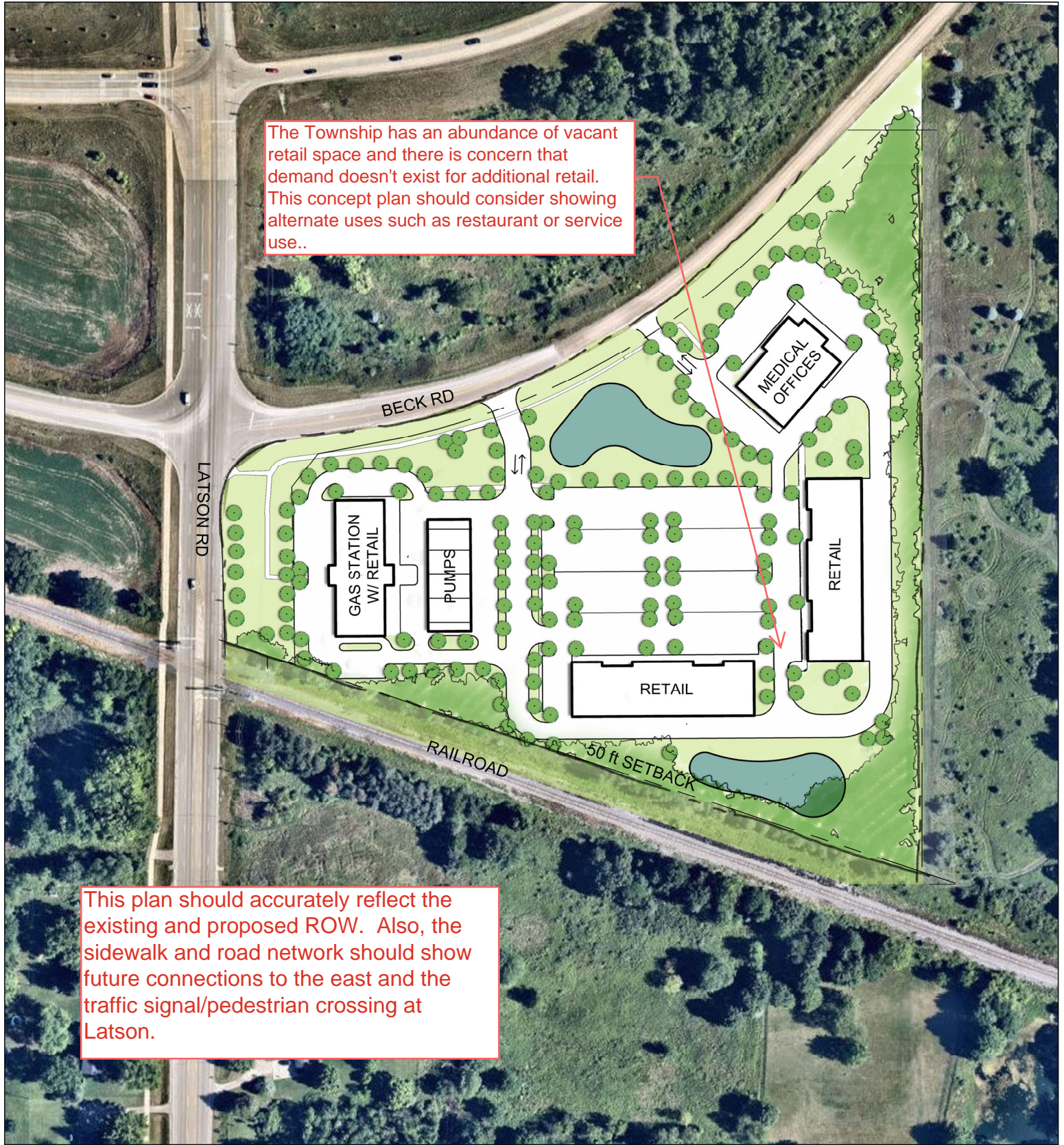
*[Exhibit to Planned Unit Development Agreement (Latson Road Commercial)]*



**EXHIBIT 3**

(Commercial PUD Concept Plan)

*[Exhibit to Planned Unit Development Agreement (Latson Road Commercial)]*



The Township has an abundance of vacant retail space and there is concern that demand doesn't exist for additional retail. This concept plan should consider showing alternate uses such as restaurant or service use..

This plan should accurately reflect the existing and proposed ROW. Also, the sidewalk and road network should show future connections to the east and the traffic signal/pedestrian crossing at Latson.

# MKSK

Landscape Architecture  
Urban Design  
Planning

462 SOUTH LUDLOW ALLEY  
COLUMBUS, OH 43215  
614.621.2796 MKSKSTUDIOS.COM

Drawing Title:	Project #	d23103
<b>SITE PLAN</b>	Date:	06.07.2024
Project:	Scale:	1 to 200
<b>COMMERCIAL PUD</b>		

**EXHIBIT 4**

(Table of Permitted and Prohibited Uses for Commercial Area)

*[Exhibit to Planned Unit Development Agreement (Latson Road Commercial)]*

# VERSA PUD: Commercial Use Table

P= Permitted; S= Special Land Use

Consider attaching to agreement

Note: Uses shall comply with Section 7.02.02, Use Conditions, in the Genoa Township Zoning Ordinance. Uses over 60,000 square feet of gross floor area require Special Land Use approval in accordance with the general and specific standards of Article 19 Special Land Uses.

<u>Types of Uses (terms as defined in the Zoning Ordinance)</u>	<u>Column 1</u>
<b>COMMERCIAL AND SERVICE</b>	
Medical offices -excluding clinics, and urgent care centers	P
Professional Offices	P
Motion picture theaters	P
Recreation (indoor) such as bowling alleys, skating rinks, arcades, indoor golf or softball, indoor shooting/archery ranges, excluding dome structures	P
Auto/gasoline service station, limited to one establishment within the PUD	S
Banks, credit unions, savings and loan establishments and similar financial institutions with up to 3 drive-through teller windows	P
Banks, credit unions, savings and loan establishments and similar financial institutions with more than 3 drive-through teller windows	S
Hotels including accessory convention/meeting facilities and restaurants	P
Health clubs, fitness centers, gyms and aerobic clubs	P
Micro-brewery, small distillery and small winery	P
Pet supplies or grooming	P
Pet day care center	S
Personal and business service establishments, performing services on the premises, but not including dry cleaning.	P
Pharmacies which may include drive through service	P
Standard restaurants and coffee shops	P
Restaurants and bars serving alcoholic beverages	P
Restaurants with open front windows	P
Restaurants with outdoor seating	P
Drive-through restaurants	P
Drive-in restaurants	P
Carry-out restaurants	P
Coffee Shop with drive-through	P
Brewpub	P
Retail establishments and shopping centers	P
Conference Centers	P

Use

This is a special land use in RCD. Consider if it should also be a special use here.

Consider allowing 1 of each highlighted use as a permitted use, with additional subject to SLU. This would be consistent with the development agreements on the north side of the interchange.

## LIST OF PROHIBITED USES

<b>Types of Uses</b>
Automobile, motorcycle, boat and recreational vehicle sales, new and used, including the leasing of such vehicles
Dry Cleaning Establishments
Outdoor commercial display, sales or storage
Kennel, commercial
Mini-storage
Auto/Truck Repair (Minor or Major)
Truck Stop

Self/Mini-Storage

**EXHIBIT 5**

(Excerpts from Zoning Ordinance)

*[Exhibit to Planned Unit Development Agreement (Latson Road Commercial)]*

**ARTICLE 10  
PLANNED UNIT DEVELOPMENT**

**Sec. 10.01 PURPOSE**

- 10.01.01 **Purpose.** The purpose of this Section is to permit the coordinated development on larger sites, protect significant natural features present which the property owner and Township wish to preserve, to provide the opportunity to mix compatible uses or residential types, or allow clustering of residential units to preserve common open space and natural features.
- 10.01.02 **Innovation in Land Use.** The PUD standards are provided as a design option to permit flexibility in the regulation of land development; to encourage innovation in land use, form of ownership and variety in design, layout, and type of structures constructed; to preserve significant natural features and open space; to promote efficient provision of public services and utilities; to minimize adverse traffic impacts; to provide adequate housing and employment; to encourage development of convenient recreational facilities; and to encourage the use and improvement of existing sites when the uniform regulations contained in other zoning districts alone do not provide adequate protection and safeguards for the site or its surrounding areas. The PUD standards are not intended to avoid the imposition of standards and requirements of other zoning classifications rather than to achieve the stated purposes herein set forth.
- 10.01.03 **Flexibility in Design.** For properties approved for PUD designation, these PUD standards provide the developer with flexibility in design and permit variation of the specific bulk, area, and in some specified situations the density requirements of this Ordinance on the basis of the total PUD plan, subject to the approval of the PUD plan by the Planning Commission and Township Board in accordance with the requirements as herein set forth.
- 10.01.04 **Types of PUD's.** This article provides for seven (7) types of PUD: a residential overlay, a planned industrial/corporate district, a mixed use PUD district, a redevelopment PUD, a non-residential PUD District, and two separate PUD Districts for the S. Latson Road interchange area. The residential PUD, planned industrial PUD and redevelopment PUD are overlay districts that include supplementary standards, which apply simultaneously, or replace, standards of the underlying residential zoning district. The mixed use PUD, non-residential PUD, and two interchange PUDs are separate zoning districts.

(as amended 12/31/06 and 09/04/18)

**Sec. 10.02 QUALIFYING CONDITIONS**

The following provisions shall apply to all planned unit developments:

- 10.02.01 **Single Ownership.** The planned unit development site shall be under the control of one owner or group of owners and shall be capable of being planned and developed as one integral unit.
- 10.02.02 **Initiated by Petition.** A PUD zoning classification may be initiated only by a petition.
- 10.02.03 **Minimum Site Area.** The site shall have a minimum area of twenty (20) acres of contiguous land, provided such minimum may be reduced by the Township Board as follows:
- (a) The minimum area requirement may be reduced to five (5) acres for sites served by both public water and sanitary sewer.
  - (b) The minimum lot area may be waived for sites zoned for commercial use (NSD, GCD or RCD) where the site is occupied by a nonconforming commercial, office or industrial building, all buildings on the site are proposed to be removed or rehabilitated and a use permitted within the underlying zoning district is proposed. The Township Board shall only permit the PUD on the smaller site where it finds that the flexibility in dimensional standards is necessary to allow for innovative design in redeveloping the site and an existing blighted situation will be eliminated. (as amended 12/31/06)
  - (c) Interchange Commercial and Campus PUDs: the Township Board may waive the minimum lot area where the design elements of a proposed development are integrated into and consistent with the broader Master Plan Latson Road Subarea Plans with compatible land uses (as amended 09/04/18)
- 10.02.04 **Benefits.** The PUD site plan shall provide one or more of the following benefits not possible under the standards of another zoning district, as determined by the Planning Commission:
- (a) preservation of significant natural or historic features;
  - (b) a complementary mixture of uses or a variety of housing types;
  - (c) common open space for passive or active recreational use;
  - (d) mitigation to offset impacts; or,
  - (e) redevelopment of a nonconforming site where creative design can address unique site constraints. (as amended 12/31/06)
- 10.02.05 **Sewer and Water.** The site shall be served by public sewer and public water. The Township may approve a residential PUD that is not served by public sewer or water, provided all lots shall be at least one (1) acre in area and the requirements of the County Health Department shall be met.

**Sec. 10.03 TYPES OF PUD ZONING DESIGNATION**

A property meeting the qualifying conditions may be rezoned to an appropriate PUD District, based on the standards shown in the following table and appropriate standards contained elsewhere in this Zoning Ordinance. The rezoning shall be concurrent with the approval of a PUD Conceptual Plan. The PUD designation shall be noted in the application, and on the Official Zoning Map upon approval.

District Name	Type of District	Permitted Uses	Special Land Uses	Additional Provisions
Residential Planned Unit Development (RPUD)	Overlay of a residential district	Open space or cluster housing projects with one or more types of residential uses	Same as underlying residential district	Sec. 10.03.01
Planned Industrial Parks (PID)	Overlay district of an Industrial District	Uses permitted in the Industrial and Office-Service Districts	Special land uses of the Industrial and Office-Service District	Sec. 10.03.02
Mixed Use Planned Unit Development (MU-PUD)	Separate zoning district	A mixture of public, residential, commercial, recreational or open space uses.	Special land uses of the zoning districts applicable to each PUD component.	Sec. 10.03.03
Redevelopment Planned Unit Development (RDPUD)	Overlay of a commercial district	Same as underlying district	Same as underlying district	Sec. 10.03.04
Non-residential Planned Unit Development (NR-PUD)	Separate zoning district	A mixture of public, office, commercial, light industrial, recreational and open space uses. See Sec. 10.03.05(c).	Special land uses of the zoning districts applicable to each PUD component.	Sec. 10.03.05
Interchange Commercial PUD (ICPUD)	Separate zoning district	See Sec. 10.03.06 c	Special land uses of the General Commercial or Regional Commercial District, as may be approved by the Township, except those specifically listed in Sec. 10.03.06 c	Sec. 10.03.06
Interchange Campus PUD (CAPUD)	Separate zoning district	See Sec. 10.03.06 c	See Sec. 10.03.06 c	Sec. 10.03.06

(as amended 12/31/06 and 09/04/18)

**10.03.01 Residential PUD**

- (a) **Density:** Residential density shall be determined by a parallel plan that illustrates how the site could be developed as a conventional subdivision or site plan, meeting all applicable township and county zoning and subdivision requirements. The Township shall review the design and determine the number of buildable lots that could be feasibly constructed, taking into consideration any wetlands or other non-buildable land. This number shall be the maximum number of dwelling units allowable for the RPUD. Where the underlying zoning is multiple family, density shall be determined based upon the underlying zoning district and the definition of density. Where the Township Master Plan recommends a different zoning district that the current zoning, a rezoning of the underlying zoning district consistent with the Master Plan may be considered concurrently with the Residential PUD overlay.
- (b) **Dimensional Standards:** The dimensional standards of the underlying zoning district shall be complied with, provided the lot area, lot width and setback requirements may be reduced with the resultant area preserved as open space. A table shall be provided on the site plan indicating the cumulative reduction in lot areas and the corresponding



amount of open space being preserved. Wetland setbacks may not be reduced. The Planning Commission may approve an RPUD without public water and sewer provided all lots shall be at least one (1) acre in area and the requirements of the County Health Department are met.

- (c) Open Space: All land within an RPUD that is not devoted to a residential unit, roadway or other improvement shall be set aside as common open space for recreation or conservation. The amount of open space shall be at least equal to the total area that proposed lots are reduced below the underlying zoning's minimum lot area; provided a minimum of twenty five percent (25%) of the site shall be open space. Common open space shall be planned in locations that are visible and accessible. The open space shall contain some form of active recreational facility such as a play-area. The common open space shall be located to preserve significant natural features, central to the residents of the development, along the county road frontage, adjacent to adjoining residential or to connect open spaces throughout the development. The open space along the exterior public roads shall generally have a depth of at least one hundred (100) feet, either landscaped or preserved in a natural wooded condition. The PUD agreement shall set forth open space protection measures as provided for in section 10.05.04.

**10.03.02 Planned Industrial District (PID)**

- (a) Dimensional Standards: All buildings, structures, accessory structures and parking areas shall meet the minimum setback standards of the Industrial District, as specified in the Table of Dimensional Standards, along the exterior boundaries of the PID site. Internal setbacks shall be determined by the Planning Commission during review of the PID concept plan. Maximum building height shall be consistent with the standards for the Industrial District.
- (b) Lot Areas: Minimum lot area shall be two (2) acres except up to twenty five percent (25%) of the total number of lots may be between one and one-half (1-1/2) and two (2) acres in area.
- (c) Design Standards: Buildings shall utilize high quality architecture and landscaping that create a research and office-park environment with primary use of masonry material, such as brick, stone or split face block, and glass on buildings and landscaping along internal roadways and around the perimeter of the PID. Metal paneling and plain concrete masonry units shall constitute no more than twenty-five percent (25%) of the facades of buildings visible from the internal roadway or any adjoining public roadway. (as amended 12/31/06)

**10.03.03 Mixed Use PUD**

- (a) Uses: A mixed use PUD shall include a mixture of uses that are considered by the Planning Commission to be consistent with the Master Plan. A concept plan shall be prepared for the PUD that divides the PUD into components for various uses. Each component of the PUD shall be designated as a specific zoning district (e.g. Medium Density Residential or Office-Service). Areas devoted to each type of use shall be designated on the PUD Concept Plan. The concept plan may provide for vertical mixture of uses, such as office or residential above commercial. The mixed use PUD can be a mixture of housing types such as single family and multiple family or a mixture of uses such as residential and non-residential. The Planning Commission

shall determine the appropriate mixture of uses and how much of the PUD land area shall be occupied by residential uses, nonresidential uses, recreational area, or open space. The Planning Commission shall make this determination based upon the concept plan's ability to provide an integrated mixture of uses, maintain compatibility with surrounding uses, and meet the standards of section 10.07. The list of permitted uses shall be established by the Planning Commission in the PUD agreement. Not more than fifty percent (50%) of the PUD acreage shall be devoted to commercial, office or industrial and not less than fifty percent (50%) of the PUD acreage shall be devoted to open space, preserved natural features or residential use.

- (b) **Open Space:** A minimum twenty five percent (25%) of the site shall be open space. Such open space shall be dispersed throughout the site and linked through greenway or pedestrian corridors or located along road frontages. A minimum of 50% of the required open space shall be usable upland area.
- (c) **Dimensional Requirements:** All area and bulk dimensional standards shall comply with the dimensional standards for the associated zoning district designated on the PUD concept plan. To encourage flexibility and creativity consistent with the intent of the PUD, the Township may permit specific departures from the requirements of the Zoning Ordinance as a part of the approval process. Any regulatory modification shall be approved through a finding by the Township that the deviation shall result in a higher quality of development than would be possible using conventional zoning standards. Residential portions of a PUD shall comply with section 10.03.01.
- (d) **Parking.** To encourage a true integration of mixed uses and improved efficiency in land use, the Planning Commission may permit the overlap in parking requirements between uses that have alternating peak-parking demands or where the mixture of uses on a site would result in multi-purpose trips. Approval for the parking reduction shall be based upon documentation submitted by the applicant indicating the types of uses, intensity and characteristics of the parking demands for such uses.

**10.03.04 Redevelopment PUD**

- (a) A redevelopment PUD overlay shall only be applied to sites that have been previously developed for the purpose of a commercial, office, or industrial use, where redevelopment of the site will be an enhancement to the site and surrounding area, where all buildings on the site are proposed to be removed or renovated and a use permitted within the underlying zoning district is proposed. The redevelopment PUD shall only be applied to a site where the Township determines that flexibility in dimensional standards is necessary to allow for innovative design in redeveloping a site with constraints and where a clear public benefit is being derived.
- (b) To encourage flexibility and creativity consistent with the intent of the PUD, the Township may permit specific departures from the requirements of the Zoning Ordinance as a part of the approval process. Any regulatory modification shall be approved through a finding by the Township that the deviation shall result in a higher quality of development than would be possible using conventional zoning standards. A parallel plan shall be provided showing how the site could be redeveloped without the use of the PUD to allow the Planning Commission to evaluate whether the modifications to dimensional standards are the minimum necessary to allow redevelopment of the site, while still meeting the spirit and intent of the ordinance.

- (c) A table shall be provided on the site plan that specifically details all deviations from the zoning regulations. This specification should include ordinance provisions from which deviations are sought, the reasons the deviations are necessary and mechanisms to be utilized to mitigate any impacts. Only those deviations consistent with the intent of this ordinance shall be considered. As a condition of approving such deviations, the Township may attach such additional conditions deemed necessary for the protection of the public health, safety, and welfare in lieu of the regulations. (as amended 12/31/06)

**10.03.05 Non-residential Planned Unit Developments**

- (a) Size of Uses:
  - (1) A maximum sixty percent (60%) of the site, exclusive of public rights of way shall contain retail commercial uses such as shopping centers or freestanding retail/department stores including areas required for storm water, setbacks, parking and landscaping associated with such uses. The remainder of the site shall include open space, manufacturing, research and development, office, lodging, restaurants and/or entertainment related uses.
  - (2) No more than two retail uses shall have an individual floor area of 100,000 square feet or more, and no other individual commercial use shall have a floor area over 60,000 square feet.
- (b) A minimum twenty five percent (25%) of the site shall be open space. Such open space shall be dispersed throughout the site and linked through greenway or pedestrian corridors. Open space is defined as undisturbed areas of key natural features, landscaped open space or pedestrian plaza areas, which commonly include outdoor seating and gathering areas. Detention areas shall comprise no more than 50% of the required open space and if visible from the roadway, parking lot, residential dwellings, primary entrances to buildings or other predominant views shall only be counted toward this requirement if designed to provide a natural appearance as described below.
- (c) Permitted Uses: All uses permitted by right or by special land use approval in the Commercial, Office and Public and Recreational Facilities Districts (NSD, OS, GCD, RCD and PRF) are permitted by right or special use under the PUD. Permitted uses shall also include Manufacturing Research or Research and Development Uses, defined as low intensity industrial uses that include a large office or laboratory component and that manufacture, package, assemble or treat finished or semi finished products from previously prepared material but do not process raw materials. The following are exceptions to the list of permitted uses:
  - (1) Auto sales, new and used
  - (2) Auto/gasoline service stations of any type, principal or accessory
  - (3) Auto maintenance or repair establishment of any type
  - (4) Automobile wash, automatic or self serve

- (5) Banquet halls, assembly halls, dance halls, private clubs, fraternal order halls, lodge halls or similar places of assembly except where accessory to a permitted office or lodging use
- (6) Carnivals, fairs, commercial cider mills and amusement parks
- (7) Churches
- (8) Convenience stores with gasoline sales
- (9) Permanent or temporary dome structures
- (10) Fruit stands (outdoor sales of fruit and nursery goods) except when accessory to a permitted use
- (11) Kennels, of any kind
- (12) Laundromats
- (13) Leasing or sales or display of trucks, trailers, boats, recreational vehicles, construction equipment and similar vehicles
- (14) Mini storage warehouses
- (15) Outdoor commercial display, sales, storage or temporary staging of items as a principal or accessory use, unless screened from public view
- (16) Outdoor private recreation facilities such as, but not limited to, miniature golf, driving ranges, batting cages, go cart tracks, and in line skating rinks
- (17) Restaurants with drive through facilities, except Township Board may approve up to one upon determination that the project shall be integrated into the design concept for the overall PUD
- (18) Educational establishments including public schools, parochial schools, vocational trade schools, colleges, universities and commercial schools such as dance academies or martial arts studios
- (19) Industrial uses, except for a research and development uses, and micro breweries associated with a restaurant
- (20) Any other use not specifically authorized under the appropriate zoning district

(d) Traffic Circulation, Operations and Access

- (1) A traffic impact study shall be provided as described in Article 18. Such study shall evaluate the impact of the project at each access point and existing major intersections where volumes from the PUD are projected increase daily or hourly volumes by 5% or more. The traffic study shall include methods to mitigate impacts, and describe timing and responsibility for funding such improvements.

- (2) Access shall be limited to one major entrance along any arterial, excluding an entrance designed solely for truck traffic. Additional access points shall only be considered if spaced at least 500 feet apart and a traffic impact study demonstrates overall traffic operations and safety will be improved.
  - (3) Access points shall be at least 600 feet from the intersection of arterial roadways or interchange ramps provided the spacing may modified by the Township, with input from road agency staff, to minimize conflicts with traffic operations at intersections or existing access points, or to meet signal spacing standards if it is determined the access may require signalization.
  - (4) Main access points shall be spaced from existing signalized intersections to ensure proper spacing and progression if the main access point is signalized in the future. The site design shall direct traffic flow to use the main access points.
  - (5) Interior drives shall provide circulation between uses.
  - (6) Stacking or queuing depth at site access points shall be sufficient to accommodate expected peak hour volumes without conflict to inbound or internal circulation.
  - (7) Additional right of way shall be provided to accommodate improvements to the existing arterial roadway system that are planned or required to mitigate traffic associated with the PUD.
  - (8) A pedestrian circulation system shall be provided throughout the site and along existing arterials.
- (e) Site Design. The following site elements shall be provided:
- (1) An extensively landscaped greenbelt shall be provided along existing public streets. Said greenbelt shall include closely spaced street trees and hedge rows to screen the parking lot. Low, undulating (horizontal and vertical) berms or an architectural feature (decorative stone or brick wall, wrought iron fencing, or combination) may be permitted.
  - (2) Site design and landscaping shall diminish the prominence of parking lots as viewed from public streets
  - (3) A Township entranceway landmark shall be provided near the intersection of any arterial streets or expressway ramps. The type and design of said landmark shall be determined as part of the conceptual plan approval.
  - (4) Pedestrian gathering and seating plazas, greenways and tree lined drives shall be within parking lots and throughout the site to provide an inviting pedestrian environment, protection of the pedestrian from vehicular circulation for improve traffic operations and views.
  - (5) One parking lot tree shall be provided for each 2000 square feet of paved parking, including aisles, service areas, driveways and drives. At least 1/2 of

the parking lot trees shall be within the parking lot inside islands or medians. A majority of the islands shall be a minimum 18 feet wide. Landscape areas shall be irrigated.

- (6) Ornamental lighting shall be provided along arterials and throughout major circulation drive within the site.
  - (7) Other site amenities to create a pedestrian scale environment shall be provided such as bike racks, benches, information kiosks, art, planters or streetscape elements to separate mainline buildings from the parking lots.
  - (8) Any detention areas visible from the roadway, parking lot, residential dwellings, primary entrances to buildings or other predominant views shall have a maximum 6:1 slope and be designed to have a natural appearance, such as variable shape, natural arrangement of landscape materials, aerated fountains, and use of boulder accent walls or other similar design features.
  - (9) Unless otherwise provided in the PUD agreement, Signs shall comply with the standards of Article 16, provided sign types and materials shall be consistent with the overall architectural design of the PUD, and all freestanding signs shall be monument type with a base to match the building materials and landscaping around the sign integrated into the overall landscape plan. Wall and monument signs shall be channel cut letters on non illuminated background panels. Temporary window signs shall be prohibited.
- (f) Architecture. Information on architecture and building design (elevations or perspectives, materials and description of design standards) shall be submitted with the concept plan and comply with the following:
- (1) Architecture throughout the development shall be compatible based on a design theme established with the Concept Plan and described in the PUD Agreement.
  - (2) Buildings shall utilize high quality architecture with variable building lines, peaked roofs, architectural accents, and brick facades. Peaked roof lines shall not be designed to create false, parapet style facades.
  - (3) The depth of the front building line shall be varied to break up the building massing.
  - (4) The predominant material utilized on facades that are visible from a public right of way or parking lots shall be brick. Other materials may be used for architectural accents, provided such materials shall have the appearance of wood or cut or cast stone.
  - (5) A building or buildings shall face (front facade or side elevation with appearance of a front facade) the intersection of existing arterial streets. The building(s) shall have distinct architecture that creates a prominent landmark at the intersection, with no loading or utility areas that face the intersection. There shall be a landscaped plaza in front of the building or between buildings. Parking shall be behind this building where practical.

- (g) Utilities. The Concept Plan shall include a Utility Master Plan, based on guidelines provided by the Township Engineer. The Utility Master Plan shall show connection points to existing utilities, and concepts for the layout, size and phasing of utilities.

**10.03.06 Interchange Planned Unit Developments (Commercial and Campus)**

- (a) Intent. The intent of the Interchange PUDs is to promote comprehensive and long-term planning of appropriate land uses, innovative architectural design, high quality building materials, and a walkable environment for pedestrians.
- (b) Master Plan and Subarea Plans. All Interchange PUD proposals shall demonstrate conformance to the land use, site design, and access management strategies and recommendations contained within the Genoa Township Master Plan and Subarea Plans.
- (c) Land Use.

- (1) ICPUD: permitted land uses include restaurants (fast food, sit-down, and take out), auto/gasoline service stations, retail/service, hotels, entertainment (movie theaters, indoor commercial recreation, etc.), conference centers, financial institutions, and offices. The Township may permit additional compatible uses as part of the approval process. The list of permitted uses proposed for a development shall be included in the PUD Agreement for review and approval by the Township. All proposed uses shall comply with the conditions of Section 7.02.02.

- (2) CAPUD: The intent of the CAPUD district is to provide locations in the Township to accommodate offices, laboratories, and related "high tech" uses, involved in such activities as engineering, design, research and development, robotics research, prototype development, demonstration and display laboratories, testing laboratories, and other research and high technology activities of similar character and intensity. On a limited basis, complementary uses are permitted, such as restaurants that primarily serve employees in the immediate area.

It is intended that such uses be located in attractive buildings on amply landscaped, carefully planned sites, and preserving significant natural features. The activities of such uses do not generate offensive external impacts and operations that generate high levels of noise, heat or glare, air pollution, odors, wastewater, or truck traffic, are not considered appropriate in this district. The list of permitted uses proposed for a development shall be included in the PUD Agreement for review and approval by the Township.

- a. Principal permitted uses include :
  - i. Research and development facilities.
  - ii. Research and support laboratories.
  - iii. Offices for the following occupations: executive, medical, dental, administrative, and professional, including architecture, planning, engineering and engineering sales.

- iv. Hospitals, clinics and medical research facilities.
  - v. Colleges, universities, and other institutions of higher learning.
  - vi. Corporate and technical education and training facilities.
  - vii. Multimedia production facilities.
  - viii. Microbrewer or small distiller.
  - ix. Data processing and computer centers, including computer programming and software development, training, and service of electronic data processing equipment.
  - x. Essential public services and structures, not including buildings and storage yards.
  - xi. Accessory uses, buildings, and structures customarily incidental to any of the above. Examples include security work, administration offices, and storage and distribution incidental to the primary use of the site.
- b. Special land uses include:
- i. Any permitted use over 40,000 square feet.
  - ii. Prototype manufacturing facilities for engineering, laboratory, scientific, electronic, and research instruments and equipment.
  - iii. Light industrial uses where activities involve high technology research and development type uses.
  - iv. Indoor commercial recreation or fitness centers (excluding dome structures).
  - v. Arenas, stadiums, and skating rinks.
  - vi. Accessory restaurants, personal and business service uses that are intended to primarily serve the occupants and patrons of the principal use; provided that, any such uses shall be an incidental use. Permitted accessory restaurant and service uses shall be limited to the following:
    - 1. Personal and business service establishments as identified in Table 7.02 that are intended to serve workers and visitors in the district, such as dry cleaning establishments, travel agencies, tailor shops, and similar establishments.
    - 2. Restaurants, cafeterias, and other places serving food and beverages which are permitted by right in the NSD.



- c. Compatible Uses: A land use which is not cited by name as a permitted or special land use may be permitted upon determination by the Township Board, following a recommendation by the Planning Commission that such use is clearly similar in nature and has the same character and intensity as those uses listed in this district as either principal permitted uses or special land uses. In making such a determination, all of the following shall be considered:
    - i. Specific characteristics of the use in question shall be compared with the characteristics of the uses which are permitted. Such characteristics shall include, but are not limited to, truck and vehicular traffic generation, types of services offered, types of goods produced, methods of operation, impacts from noise, air contaminants, odor, heat, fire hazards, and water contaminants, and building and site characteristics.
    - ii. The proposed use shall be compatible and in accordance with the goals, objectives and policies of the Genoa Township Master Plan and promote the intent of the development agreement and Section 10.03.06.
    - iii. The land use shall not impair the use and development of other nearby properties.
    - iv. If a proposed use is determined to be similar to and compatible with uses in the district the Planning Commission shall decide whether the proposed use shall be permitted by right, as a special land use, or as a permitted accessory use. The Planning Commission shall have the authority to establish additional standards and conditions under which a use may be permitted in the district.
  - d. Required conditions. Except as otherwise noted, buildings and uses in the CAPUD shall comply with the following requirements:
    - i. All uses and business activities shall comply with the use conditions of 7.02.02, 8.02.02, and the performance standards in article 13.05.
    - ii. All business activity shall be conducted within a completely enclosed building, unless otherwise specified. Outdoor storage shall be prohibited.
    - iii. Any indoor storage must be clearly accessory to the principal permitted use.
    - iv. Notwithstanding the limitations on outside storage, commercially used or licensed vehicles used in the normal operation of a permitted use may be parked on the site in the rear only.
- (d) Dimensional Standards: All buildings, structures, accessory structures and parking areas shall meet the minimum setback standards of the Industrial District, Section 8.03.01 for the CAPUD and the Regional Commercial District, Section 7.03.01 for the ICPUD as specified in the Table of Dimensional Standards, along the exterior boundaries of the site. Internal setbacks and maximum building height shall be determined by the Planning

Commission during review of the PUD concept plan. To encourage flexibility and creativity consistent with the intent of the PUD, the Township may permit specific departures from the dimensional requirements of the Zoning Ordinance as a part of the approval process. Any regulatory modification shall be approved through a finding by the Township that the deviation shall result in a higher quality of development than would be possible using conventional zoning standards.

- (e) Site Design. All Interchange PUD proposals shall comply with the standards of Section 10.03.05 e above.
- (f) Architecture. All Interchange PUD proposals shall comply with the standards of Section 10.03.05 f above. The Planning Commission may allow for alternative innovative high quality exterior façade materials such as fiber cement and metal panels for buildings in the CAPUD district to create a research and office-park environment provided that the materials proposed to be used are found by the Planning Commission to be in keeping with the intent and purpose of this Section, in consideration of the character of surrounding uses and the design recommendations of the master plan.
- (g) Access Management and Connectivity.

(1) ICPUD:

- a. No access points other than Beck Road are permitted along South Latson Road between the interchange and the rail line.
- b. Development shall incorporate shared access points to limit the number of driveways along Beck Road and shall comply with Section 15.06 Access Management.
- c. Acceptable road levels of service (LOS) shall be maintained by careful access management strategies and road improvements.
- d. Sites shall be designed to incorporate cross-access easements and connectivity for vehicular, bicycle, and foot traffic.

(2) CAPUD:

- a. The primary access to the area west of South Latson Road, south of the railroad, shall be aligned with Sweet Road.
- b. Secondary access points shall be limited and/or restricted. Restricted driveways shall be designed to be intuitive with minimal signage. All access points shall be aligned with access points across the road and shall be separated from other intersections and access points on the same side of the road by at least 500 feet.
- c. Sites shall be designed to incorporate frontage roads, service roads, and cross-access easements to allow connectivity for vehicular, bicycle, and foot traffic. The use of landscaped boulevards is encouraged.

- (h) Utilities. The Concept Plan shall include a Utility Master Plan, based on guidelines provided by the Township Engineer. The Utility Master Plan shall show connection

points to existing utilities, and adjacent properties where appropriate and concepts for the layout, size, and phasing of utilities, which shall include water, sanitary sewer and stormwater controls.

- (i) Future Transition Area. Appropriately timed incremental southward expansion of the CAPUD is anticipated. Evaluation factors for expansion include the following considerations:
  - (1) The amount and capacity of undeveloped land remaining within the growth framework areas shall be analyzed and a determination shall be made that additional land area is needed to justify expanding boundaries.
  - (2) Projected population growth within the Township and demand for additional land areas for development.
  - (3) Present and planned sanitary sewer capacity.
  - (4) The capacity and condition of the road system.
  - (5) The ability of the Township, County and other public agencies to provide necessary services to the new growth areas and the additional resulting population.
  - (6) Impact on public health, safety and welfare.
  - (7) Changes to conditions considered at the time of the subarea plan.
  - (8) Inclusion of integrated open space for active and passive recreation.
  - (9) Environmental constraints and sensitivity.
  - (10) Adverse impact to adjacent or nearby property.
  - (11) Sensitive transitions to residential and agricultural land can be achieved.
  - (12) Other relevant criteria deemed appropriate by the Township.

(as amended 09/04/18)

#### **Sec. 10.04 APPLICATION AND REVIEW PROCEDURE**

##### **10.04.01 Process for rezoning to appropriate PUD designation, Conceptual PUD Plan, Environmental Impact Statement and PUD Agreement.**

- (a) An optional pre-application workshop with the Planning Commission may be requested by the applicant to discuss the appropriateness of a PUD concept, solicit feedback and receive requests for additional materials supporting the proposal. An applicant desiring such a workshop shall request placement on the Planning Commission agenda.
- (b) The applicant shall prepare and submit to the Zoning Administrator a request for rezoning to the appropriate PUD designation. The application shall include all

Conceptual Submittal items listed in Section 10.05 and shall be submitted in accordance with the procedures and requirements set by resolution of the Township Board.

- (c) The Planning Commission shall review the rezoning request, the Conceptual PUD Site Plan, the Impact Statement and PUD Agreement, conduct a public hearing, and make a recommendation to the Township Board and Livingston County Planning Commission based on the review standards of Section 10.07. Notice of public hearing shall be provided for in accordance with section 21.05.
- (d) Within thirty (30) days following receipt of a recommendation from the Planning Commission, the Livingston County Planning Commission shall conduct a public hearing on the requested PUD rezoning and make a recommendation for approval or denial to the Township Board.
- (e) The applicant shall make any revisions to incorporate conditions noted by the Planning Commission and submit the required copies to the Zoning Administrator to provide sufficient time for review prior to the Township Board meeting.
- (f) Within ninety (90) days following receipt of a recommendation from the Planning Commission and Livingston County, the Township Board shall conduct a public hearing on the requested PUD rezoning, Conceptual PUD Site Plan and PUD Agreement and either approve, deny or approve with a list of conditions made part of the approval. Notice of public hearing shall be provided for in accordance with section 21.05. The Township Board may require a resubmittal of the application reflecting the conditions for approval by the Zoning Administrator, and Township consultants if appropriate. (as amended 12/31/06)

10.04.02 **Expiration:** Approval of the Conceptual PUD Site Plan by the Township Board shall confer upon the owner the right to proceed through the subsequent planning phase for a period not to exceed two (2) years from date of approval. If application for Final PUD Site Plan approval is not requested within this time period, resubmittal of a new PUD concept plan and application shall be required. The Township Board may extend the period up to an additional two (2) years, if requested in writing by the applicant prior to the expiration date.

10.04.03 **Process for Final PUD Site Plan(s)**

- (a) The applicant shall submit the required copies of all necessary information meeting the requirements of Section 10.06 of this ordinance to the Zoning Administrator at least thirty (30) days prior to the Planning Commission meeting at which the Planning Commission shall first review the request. If the PUD involves a platted subdivision, the Final Site Plan may be processed concurrently as a Preliminary Plat.
- (b) Upon submission of all required materials and fees, the Planning Commission shall review the Final PUD Plan, the Impact Statement, and PUD Agreement and make a recommendation to the Township Board based on the review standards of Section 10.08.
- (c) The applicant shall make any revisions to incorporate conditions noted by the Planning Commission and submit the required copies to the Zoning Administrator to provide sufficient time for review prior to the Township Board meeting.

- (d) Within ninety (90) days following receipt of a recommendation from the Planning Commission, the Township Board shall conduct a public hearing on the requested Final PUD Plan, the Environmental Impact Statement, and PUD Agreement and either approve, deny or approve with a list of conditions made part of the approval. The Township Board may require a resubmittal of the application reflecting the conditions for approval by the Zoning Administrator, and Township consultants if appropriate. (as amended 3/5/10)
- (e) If the Final PUD Site Plan was approved with conditions, the applicant shall submit a revised site plan to the Zoning Administrator for approval prior to the issuance of any building permits.

**Sec 10.05 CONCEPTUAL SUBMITTAL REQUIREMENTS**

The purpose of the conceptual review is to provide a mechanism whereby the applicant can obtain a substantial review of the proposed project in order to prepare final site engineering and architecture plans, and to execute necessary agreements between the applicant and the Township. The required number of copies of each of the following items shall be submitted by the applicant or as required by the Township:

- 10.05.01 Current proof of ownership of the land to be utilized or evidence of a contractual ability to acquire such land, such as an option or purchase agreement.
- 10.05.02 A completed application form, supplied by the Zoning Administrator, and an application fee. A separate escrow deposit may be required for administrative charges to review the PUD submittal.
- 10.05.03 An Impact Assessment meeting the requirements of Article 18. A traffic impact study may be required at the discretion of the Township or as otherwise stated in this ordinance which meets the requirements of Article 18.
- 10.05.04 A complete PUD Agreement for review which shall:
  - (a) Set forth the conditions upon which the approval is based, with reference to the approved Site Plan or Plat Plan and Impact Statement and a description of all deviations from Township regulations that have been requested and approved.
  - (b) When open space or common areas are indicated in the PUD plan for use by the residents, the open space or common areas shall be conveyed in fee, placed under a conservation easement or otherwise committed by dedication to an association of the residents, and the use shall be irrevocably dedicated in perpetuity and retained as open space for park, recreation, conservation or other common uses.
  - (c) Set forth a program and financing for maintaining common areas and features, such as walkways, signs, lighting and landscaping.
  - (d) Assure that trees and woodlands will be preserved as shown on the site plan, or replaced on a caliper for caliper basis.
  - (e) Assure the construction, improvement and maintenance of all streets and necessary utilities (including public water, wastewater collection and treatment) to mitigate the impacts of the PUD project through construction by the developer, bonds or other

satisfactory means, for any and all phases of the PUD. In the case of phased PUD's this requirement shall be reviewed at the time of any final site plan approval.

- (f) Address any other concerns of the Township regarding construction and maintenance.
- 10.05.05 Sheet size of submitted drawings shall be at least 24-inches by 36 inches, with graphics at an engineer's scale.
- 10.05.06 Cover Sheet providing:
- (a) the applicant's name;
  - (b) the name of the development;
  - (c) the preparer's name and professional seal of architect, engineer, surveyor or landscape architect indicating license in the State of Michigan;
  - (d) date of preparation and any revisions;
  - (e) north arrow;
  - (f) property lines and dimensions;
  - (g) complete and current legal description and size of property in acres;
  - (h) small location sketch of the subject site and area within one-half mile; and scale;
  - (i) zoning and current land use of applicant's property and all abutting properties and of properties across any public or private street from the PUD site;
  - (j) lot lines and all structures on the property and within one-hundred (100) feet of the PUD property lines;
  - (k) location of any access points on both sides of the street within one-hundred (100) feet of the PUD site along streets where access to the PUD is proposed.
- 10.05.07 A Plan Sheet(s) labeled Existing Site Conditions, including the location of existing buildings and structures, rights-of-way and easements, significant natural and historical features, existing drainage patterns (by arrow), surface water bodies, floodplain areas, wetlands over two acres in size, the limits of major stands of trees and a tree survey indicating the location, species and caliper of all trees with a caliper over eight (8) inches, measured four feet above grade. This sheet shall also illustrate existing topography of the entire site at two (2) foot contour intervals and a general description of grades within one-hundred (100) feet of the site. A reduced copy of this sheet may be included in the Impact Statement.
- 10.05.08 For projects with a residential component, a concept plan that illustrates how the site could be practically developed under current zoning standards. This drawing may be used to determine the base density of the project.
- 10.05.09 A Conceptual PUD Site Plan Sheet including:

- (a) Conceptual layout of proposed land use, acreage allotted to each use, residential density overall and by underlying zoning district (calculations shall be provided for both overall and useable acreage), building footprints, structures, roadways, parking areas, drives, driveways, pedestrian paths, gathering areas and identification signs. Calculations of the size of uses to confirm compliance with Section 10.03.04 for the Non-residential PUD option.

Note: Useable area is total area less public road rights-of-way, year-round surface water bodies, and MDNR regulated wetlands.

- (b) Building setbacks and spacing.
- (c) General location and type of landscaping proposed (evergreen, deciduous, berm, etc.) noting existing trees over eight inches in caliper to be retained, and any woodlands that will be designated as “areas not to be disturbed” in development of the PUD.
- (d) A preliminary layout of contemplated storm water drainage, detention pond location, water supply and wastewater disposal systems, any public or private easements, and a note of any utility lines to be removed.
- (e) Calculations to demonstrate compliance with minimum open space requirements shall be provided.
- (f) Preliminary architectural design information shall be provided to the satisfaction of the Township.
- (g) If a multi-phase Planned Unit Development is proposed, identification of the areas included in each phase. For residential uses identify the number, type, and density proposed by phase.
- (h) A Utility Master Plan shall be required based on guidelines provided by the Township Engineer. The Utility Master Plan shall show connection points to existing utilities, and concepts for layout, size and phasing of utilities.

**Sec. 10.06 FINAL PUD SITE PLAN SUBMITTAL REQUIREMENTS**

The final submittal shall include the required number of copies of each of the following items:

- (a) All materials required by Article 18, Site Plan Review, including an Impact Statement and Traffic Impact Statement as required.
- (b) A hydrologic impact assessment describing the existing ground and surface water resources including, but not limited to, a description of the water table, direction of groundwater flow, recharge and discharge areas, lake levels, surface drainage, floodplains, and water quality as well as the projected impact of the proposed development on such resources, in particular impacts associated with water supply development, wastewater disposal, and storm water management.
- (c) A final copy of the approved PUD Agreement that meets the requirements outlined in Section 10.05.04.

- (d) Non-Residential Projects: Additional information required for a complete review under the standards of Section 10.03.04.
- (e) Any other additional information deemed appropriate by the Township.

**Sec. 10.07 STANDARDS FOR APPROVAL OF CONCEPTUAL PUD SITE PLAN**

10.07.01 **Standards for Approval.** Based upon the following standards, the Planning Commission may recommend denial, approval, or approval with conditions, and the Township Board may deny, approve, or approve with conditions the proposed planned unit development.

- (a) The planned unit development meets the qualification requirements.
- (b) The uses proposed shall have a beneficial effect, in terms of public health, safety, welfare, or convenience, on present and future potential surrounding land uses. The uses proposed will not adversely affect the public utility and circulation system, surrounding properties, or the environment. The public benefit shall be one which could not be achieved under the regulations of the underlying district alone, or that of any other zoning district.
- (c) The planned unit development is generally consistent with the goals, objectives and land use map of the Master Plan.
- (d) Judicious effort has been used to preserve significant natural and historical features, surface and underground water bodies and the integrity of the land.
- (e) Public water and sewer facilities are available or shall be provided for by the developer as part of the site development. The Planning Commission may approve an RPUD without public water and sewer, provided all lots shall be at least one (1) acre in area and the requirements of the County Health Department are met.
- (f) Safe, convenient, uncongested, and well-defined vehicular and pedestrian circulation within and to the site is provided. Roads and driveways shall comply with the Township Subdivision Control Ordinance, Livingston County Road Commission standards and the private road regulations of Article 15, as applicable. Drives, streets and other elements shall be designed to discourage through traffic, while promoting safe and efficient traffic operations within the site and at its access points. The site shall provide for inter-connection of roads and the future integration of circulation between adjacent sites.
- (g) Common open space shall be provided including natural areas, community greens, plazas and recreation areas. The open space and all other elements shall be in an appropriate location, suitably related to each other, the site and surrounding lands. The common open space may either be centrally located along the road frontage of the development, located to preserve significant natural features, or located to connect open spaces throughout the development. Connections with adjacent open space, public land or existing or planned pedestrian/bike paths may be required by the Township. Grading in the open space shall be minimal, with the intent to preserve existing significant topographic features, where such resources exist.
- (h) Any deviations from the applicable zoning regulations are reasonable and meet the intent of this Article.



- 10.07.02 **Conditions.** The Township Board may impose additional reasonable conditions to: 1) insure that public services and facilities affected by a Planned Unit Development will be capable of accommodating increased service and facility loads caused by the Planned Unit Development, 2) protect the natural environment and conserve natural resources and energy, 3) insure compatibility with adjacent uses of land, and 4) promote the use of land in a socially and economically desirable manner.

**Sec. 10.08 FINAL PUD SITE PLAN APPROVAL STANDARDS**

Based upon the following standards, the Planning Commission may recommend denial, recommend approval, or approval with conditions, and the Township Board may deny, approve with conditions the proposed planned unit development.

- 10.08.01 **Consistency with Preliminary PUD.** The Final PUD Plan and associated documents shall be reviewed for consistency with the approved Conceptual PUD Plan, PUD Agreement and associated documents and any conditions required by the Township.
- 10.08.02 **Final Site Plan Review.** The Final PUD Plan and associated documents shall be reviewed in accordance with Article 18 Site Plan Review, Township Subdivision Regulations, Township Condominium Ordinance and any other applicable regulatory document.
- 10.08.03 **Non-residential.** Non-residential PUD projects shall be reviewed for compliance with the standards set forth in Section 10.03.04.
- 10.08.04 **Conditions.** The Township may impose additional reasonable conditions to: 1) insure that public services and facilities affected by a Planned Unit Development will be capable of accommodating increased service and facility loads caused by the Planned Unit Development, 2) protect the natural environment and conserve natural resources and energy, 3) insure compatibility with adjacent uses of land, and 4) to promote the use of land in a socially and economically desirable manner.
- 10.08.05 **Phases.** For a PUD that is being developed in phases, final site plan approval for each phase shall be conditioned upon continued compliance of all phases with the Conceptual PUD Plan and PUD Agreement, as may be amended by the Township. The Township Board may postpone the approval of any final site plan for subsequent phases until previously approved phases of the PUD are brought into compliance with the requirements of the Conceptual PUD Plan and PUD Agreement.

**Sec. 10.09 SCHEDULE OF CONSTRUCTION**

- 10.09.02 **Construction.** Final site plan approval of a PUD, PUD phase or a building within a PUD shall be effective for a period of three (3) years. Further submittals under the PUD procedures shall be accepted for review upon a showing of substantial progress in development of previously approved phases, or upon a showing of good cause for not having made such progress.
- 10.09.04 **Residential Phasing.** In the development of a PUD, the percentage of one-family dwelling units under construction, or lots sold, shall be at least in the same proportion to the percentage of multiple family dwelling units under construction at any one time, provided that this Section shall be applied only if one-family dwelling units comprise twenty-five (25%) percent or more of the total housing stock proposed for the PUD. Non-residential structures

designed to serve the PUD residents shall not be built until the PUD has enough dwelling units built to support such non-residential use. The Planning Commission may modify this requirement in their conceptual or final submittal review process.

**Sec. 10.10 APPEALS AND VIOLATIONS**

- 10.10.01 **Zoning Board of Appeals:** The Zoning Board of Appeals shall have the authority to hear and decide appeal requests by individual lot owners for variances from the Genoa Township Zoning Ordinance following final approval of the PUD. However, the Zoning Board of Appeals shall not have the authority to reverse the decision of the Township Board on a PUD concept, or final site plan, change any conditions placed by the Planning Commission, or Township Board or grant variances to the PUD site plan, written PUD agreement or the requirements of this article.
- 10.10.02 **Violations:** A violation of the PUD plan or agreement shall be considered a violation of this Ordinance.

**Sec. 10.11 AMENDMENTS AND DEVIATIONS FROM APPROVED FINAL PUD SITE PLAN**

- 10.11.01 **Deviations following approval:** Deviations following approval of the Final PUD Site Plan may occur only when an applicant or property owner who was granted Final PUD Site Plan approval notifies the Zoning Administrator of the proposed amendment to such approved site plan in writing, accompanied by a site plan illustrating the proposed change. The request shall be received prior to initiation of any construction in conflict with the approved Final PUD Site Plan.
- 10.11.02 **Procedure:** Within fourteen (14) days of receipt of a request to amend the Final PUD Site Plan, the Zoning Administrator shall determine whether the change is major, warranting review by the Planning Commission, or minor, allowing administrative approval, as noted below.
- 10.11.03 **Minor changes:** The Zoning Administrator may approve the proposed revision upon finding the change would not alter the basic design nor any conditions imposed upon the original plan approval by the Planning Commission. The Zoning Administrator shall inform the Planning Commission of such approval in writing. The Zoning Administrator shall consider the following when determining a change to be minor.
- (a) For residential buildings, the size of structures may be reduced; or increased by five percent (5%), provided the overall density of units does not increase and the minimum square footage requirements are met.
  - (b) Gross floor area of non-residential buildings may be decreased; or increased by up to five percent (5%) or 10,000 square feet, whichever is smaller.
  - (c) Floor plans may be changed if consistent with the character of the use.
  - (d) Horizontal and/or vertical elevations may be altered by up to five percent (5%).
  - (e) Relocation of a building by up to five (5) feet, if consistent with required setbacks and other standards.
  - (f) Designated "Areas not to be disturbed" may be increased.

- (g) Plantings approved in the Final PUD Landscape Plan may be replaced by similar types of landscaping on a one-to-one or greater basis. Any trees to be preserved which are lost during construction may be replaced by at least two (2) trees of the same or similar species.
- (h) Improvements or slight relocation of site access or circulation, such as inclusion of deceleration lanes, boulevards, curbing, pedestrian/bicycle paths, etc.
- (i) Changes of building materials to another of higher quality, as determined by the Zoning Administrator.
- (j) Slight modification of sign placement or reduction of size.
- (k) Internal rearrangement of parking lot which does not affect the number of parking spaces or alter access locations or design.
- (l) Changes required or requested by the Township, County or state for safety reasons.

10.11.04 **Major Changes:** Where the Zoning Administrator determines the requested amendment to the approved Final PUD Site Plan is major, resubmittal to the Planning Commission shall be required. Should the Planning Commission determine that the modifications to the Final PUD Site Plan significantly alter the intent of the Conceptual PUD Site Plan, a revised conceptual PUD Site Plan shall be submitted according to the procedures outlined in Section 10.04 illustrating the modification shall be required.

**EXHIBIT 6**

(Commercial PUD Design Guidelines)

*[Exhibit to Planned Unit Development Agreement (Latson Road Commercial)]*

# LATSON ROAD COMMERCIAL PUD DESIGN GUIDELINES

GENOA TOWNSHIP, MICHIGAN

UPDATED AUGUST 21, 2024

## COMMERCIAL DEVELOPMENT INTENT

These guidelines are intended to illustrate the design quality anticipated with the commercial PUD. The "Owner" of the PUD or subsequent purchaser of land will be responsible for providing these guidelines to design professionals who will be involved in the preparation of site plans. Specific compliance will be described in more detail with a site plan that will be submitted to the Township for approval.

In general these guidelines include the following components:

1. A description of architecture supplemented with photographs from similar developments to illustrate the general outcomes expected consistent with the standards to support a deviation from the Township's standards that would otherwise apply.
2. Efforts to share access to reduce the number of driveways and provide good traffic operations along Latson Road and Beck Road.
3. Additional lighting standards to reduce lighting impacts on adjacent homes to the east.
4. Site design and landscaping shall diminish the prominence of parking lots as viewed from public streets.
5. Pedestrian gathering and seating plazas, greenways and tree lined drives shall be within parking lots and throughout the site to provide an inviting pedestrian environment. These areas will also provide protection of the pedestrian from vehicular circulation for improved traffic operations and views.



GENOA TOWNSHIP, MI

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Map should show correct property.

LAND DEVELOPER:  
Latson Beck, LLC and Latson South, LLC  
326 E. Fourth Street, Suite 200, Royal Oak, Michigan 48067

Consider prohibition on limited access driveways.

## COMMERCIAL DESIGN GUIDELINES

### A. Setbacks

- Design for development needs to ensure that building placement is generally oriented towards the street to encourage walkability and a pedestrian-friendly environment.

### B. Parking and Access

- Development within such areas should occur within a planned, integrated commercial setting. Site design for parking areas and access points will promote safe and efficient circulation throughout the site and with adjacent parcels.
- Access roads shall be a minimum of 26 feet wide FOC and 30 ft inside turning radius (50 ft outside) for emergency vehicle access.
- The amount of parking required for individual uses may be reduced to be efficient so that the peak parking demand is accommodated.
- Parking lots should be connected to promote shared parking and reduce the number of curb cuts and overall amount of impervious surface area.

### C. Pedestrian Amenities

- Uses shall be connected with an interior sidewalk system so that pedestrians can walk between the uses and have access to the sidewalk on the west side of Latson Road.

Sidewalks should also be provided along road frontage

### D. Landscaping

- Plant consistent and plentiful native vegetation to provide an attractive entry into the southern part of Genoa Township and provide generous interior landscape that serves as a buffer between the buildings and parking lots as well as adjacent land uses.
- Street trees planted shall consist of no more than 10% of a single species, no more than 20% of any genus, and no more than 30% of any tree family.

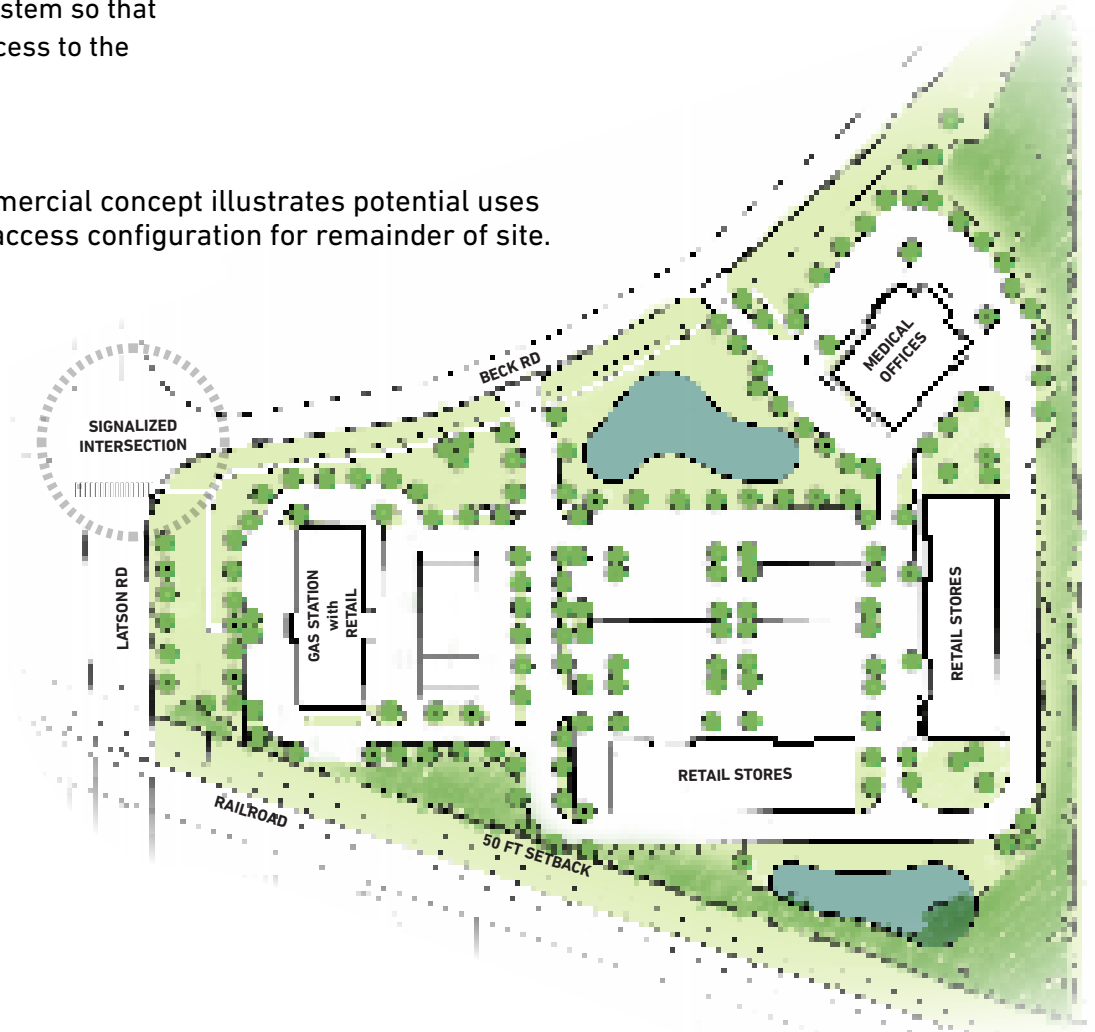
### E. Architecture

- Commercial architecture design guidelines are described in detail on the following page.

### F. Uses Permitted

- Uses allowed in the interchange commercial area may include retail stores, restaurants, drive-through restaurants, a gas station, hotels, and similar commercial uses.

Commercial concept illustrates potential uses and access configuration for remainder of site.



## COMMERCIAL DESIGN GUIDELINES

### COMMERCIAL DIMENSIONAL STANDARDS

#### Minimum setbacks:

Front Yard	70 feet (or 35 feet if no parking is located in the front yard)
Side Yard	20 feet for each side plus an additional 0.5 feet per foot of height over 45 feet tall <sup>1</sup>
Rear Yard	50 feet
Parking Lot	20 feet front, 10 feet side and rear
Maximum Height	45 feet or 3 stories
Maximum Height of Hotel	57 feet or 4 stories <sup>2</sup>

<sup>1</sup> Proposed new standard to provide greater side setbacks for taller buildings.

<sup>2</sup> As a Special Land Use, the Hotel may be increased to 65 feet or 5 stories, provided minimal distance from adjacent residential home is 500 feet and the Township determines the design is compatible with residential in the area in terms of views and lighting.

## COMMERCIAL ARCHITECTURAL DESIGN GUIDELINES

The following guidelines apply to all commercial types within the Commercial PUD and are required to comply with 10.03.05(f) of the Genoa Township Zoning Ordinance. These guidelines promote and enforce high-quality architectural design for building sides, including a gas station (see precedent photo), visible from a road or parking lot. Retail uses are anticipated to be predominantly 1 to 2 story pitched roofed buildings. Buildings shall utilize high quality architecture with variable building lines, peaked roofs, architectural accents, and brick facades. Peaked roof lines shall not be designed to create false, parapet style facades.

### A. General Design Theme.

- These architectural requirements are generally intended to provide consistent architectural quality among buildings and other improvements within the Latson Road corridor.
- These guidelines are intended to generate architectural cohesion, however some architectural variation is allowed that is consistent with the overall design theme.
- All structures shall be thoughtfully designed in a manner that visually and functionally complements the existing context.

### B. Building Elevations.

- If more than one story, a different architectural treatment may be employed on the ground floor facade than on the upper floors to enhance the experience of visitors/patrons.
- All building facades shall have a defined base or foundation, a middle or modulated wall, and a top formed by a pitched roof or three-dimensional cornice.
- The predominant material utilized on facades that are visible from a public right of way or parking lots shall be brick. Other materials may be used for architectural accents, provided such materials shall have the appearance of wood or cut or cast stone.
- A building or buildings shall face (front facade or side elevation with appearance of a front facade) the intersection of existing arterial streets. The building(s) shall have distinct architecture that creates a prominent landmark at the intersection, with no loading or utility areas that face the intersection. There shall be a landscape plaza in front of the building or between buildings. Parking shall be behind this building where practical.
- Excluding windows, doorways, and associated decorative trim, **75%** of the total area (square feet) of the front facade of commercial buildings shall be brick. This also includes facades visible from Latson Road and the site parking lots.
- The following items are prohibited: Texture 1-11, aluminum siding or asbestos or asphalt shingles shall not be used on the exterior walls.
- Building facades, which are ninety (90) feet or greater in length, shall be designed with offsets (projecting or recessed) at intervals of not greater than sixty (60) feet.

Consider 80% Brick

Beck Road

Consider replacing these graphics with better images of projects we seek to emulate. Straight ahead views showing architectural detail would be preferred.

- Offsets may be met with setbacks of the Building Facade and/or with architectural elements (i.e. arcades, columns, piers, and pilasters), if such architectural elements meet the minimum offset requirements of this requirement.

### C. Roofs.

#### 1. Pitched Roofs:

- Shall be simply and symmetrically pitched and only in the configuration of gables and hips, with pitches ranging from 4:12 to 14:12.
- If standing seam panels are used then they shall be: 1) gray, black, or dark brown; and 2) made of a non-reflective material.
- Modulation of the roofs and/or roof lines shall be required in order to eliminate the appearance of box-shaped buildings.

### D. Lighting and Signs

#### 1. Site Lighting

- Site lighting, within the commercial area, shall be LED based, consistent in style, color, design in accordance with the Township Zoning Ordinance standards, and be dark sky certified.
- All site lighting fixtures shall have a maximum height of thirty (30) feet. The maximum light levels on these properties shall not exceed 10 footcandles on average (common with new LED lighting systems), except the fueling area for a gas station is allowed an average of 12 footcandles. Lighting will otherwise be in accordance with the Township Zoning Ordinance lighting standards.
- With the exception of low intensity architectural lighting, exterior wall mounted lights and pole mounted lights shall incorporate overhead cutoffs or fixtures that direct the light downward.

2. Retail signs and other signs shall conform with the Township Ordinances.
3. Wall signs shall be channel cut letters.

### E. Pedestrian Amenities

- Uses shall be connected with an interior sidewalk system so that pedestrians can walk between the uses.
- If there is a connection across the railroad tracks that is approved by the railroad operator, sidewalks will be installed on the east side of Latson Road.

Pedestrian Crossing at Latson Road and Beck Road to connect to existing pathway on the west side of Latson.

Also include sidewalks along road frontage.

## COMMERCIAL DESIGN GUIDELINES BUILDING DESIGN PRECEDENTS



Example of a gas station adhering to greater design standards.



Newer hotels along the I-96 corridor that demonstrate higher quality building design.



Example of channel cut wall signage



## COMMERCIAL OUTDOOR LIGHTING STANDARDS

The purpose and intent of the Outdoor Lighting standards is to:

- Minimize light trespass onto adjacent properties
- Help eliminate artificial lighting that contributes to “sky glow “and disrupts the natural quality of the nighttime sky
- Provide a safe nighttime environment

Any future site plan within the PUD shall be required to submit an outdoor lighting plan to abide by the standards set forth in this section. The site plan shall contain a photometric layout for the exterior lighting which may subsequently be waived if there is no parking area present on the site. Standards generally apply throughout the PUD, but flexibility may be allowed.

The following outdoor lighting types shall be exempt from the provisions of this section:

- Emergency lighting
- Temporary lighting for performance areas, construction sites and community festivals.
- Seasonal and holiday lighting provided that the lighting does not create direct glare onto other properties or upon the public rights-of-way.

The following outdoor lighting types shall be prohibited:

- Floodlights or swivel luminaires designed to light a scene or object to a level greater than its surroundings. No fixtures may be positioned at an angle to permit light to be emitted horizontally or above the horizontal plane.
- Unshielded lights that are more intense than 2,250 lumens or a 150 watt incandescent bulb.
- Search lights and any other device designed solely to light the night sky except those used by law enforcement authorities and civil authorities.
- Laser source light or any similar high intensity light when projected above the horizontal plane.
- Mercury vapor lights.
- Metal halide lights, unless used for outdoor sport facilities.
- Quartz lights.
- Neon/LED Strip Lights.

be

This is a very low number. Did you mean 10 footcandles?

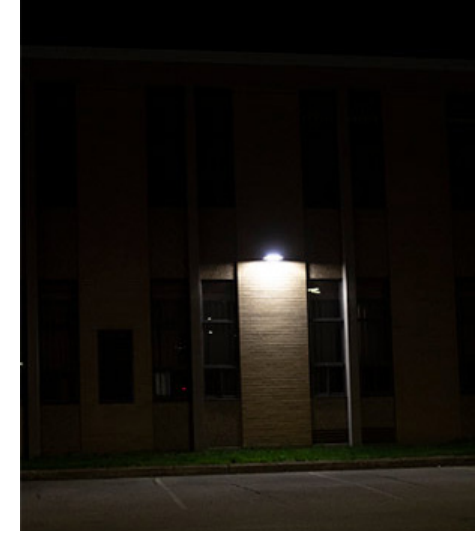
Outdoor Lighting Design Standards – Internal to the Site:

- Direct or reflected outdoor lighting shall be designed and located to be confined to the site for which it is accessory. The maximum lighting levels at the property lines of any other property shall not exceed 0.1 footcandles at residential lot line, 1 at non-residential lot line.
- Lighting of building facades shall be from the top and directed downward with full cut-off shielding.
- The average lighting values for areas intended to be lit shall not exceed 1.0 footcandles on average. The uniformity ratio (maximum to minimum) for all parking lots shall not exceed the current IESNA RP-20 uniformity ratio guideline. (Note: Current guideline is 15:1)
- Lighting fixtures shall meet the township maximum height of 30 feet and 10 footcandles with the following exceptions:
  1. The Township may permit maximum light levels of 12 footcandles on average (common with new LED lighting systems), designed to have no spillover onto adjacent properties.
  2. Provided that when lighting is adjacent to, and visible from, abutting residential properties, the maximum height of lighting poles shall be 20 feet unless the Township approves taller poles with a demonstration that it is an overall better lighting design in terms of aesthetics.
  3. Site lighting for non-residential uses shall not exceed 1.0 footcandles on average when a use is not open for business.

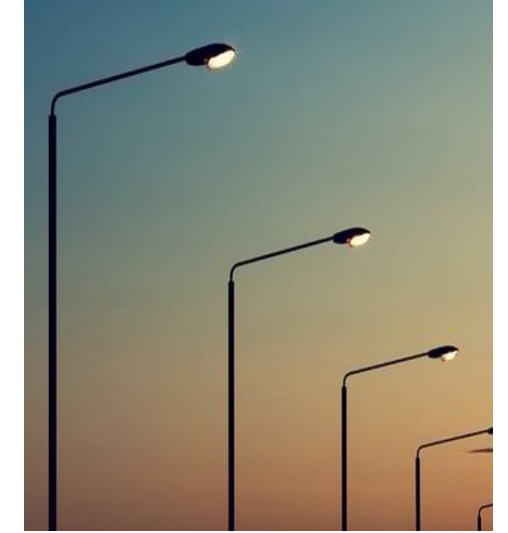
Outdoor Lighting Design Standards – Public Street Lighting:

- Streetlights in the public rights-of-way shall be the minimum necessary to provide adequate illumination for public safety and be designed to direct lighting downward onto the public rights-of-way.
- Public street illumination shall use the most current American National Standard Practice for Roadway Lighting ANSI/IESNA RP-08 for all public street lighting.

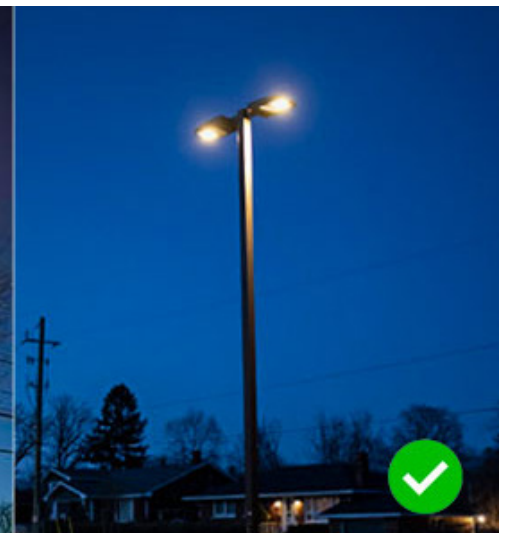
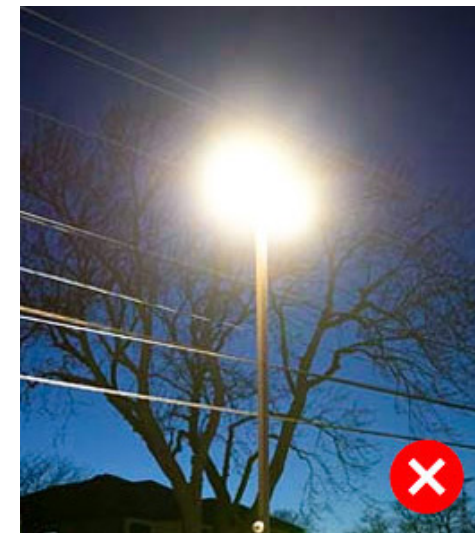
## COMMERCIAL OUTDOOR LIGHTING STANDARDS



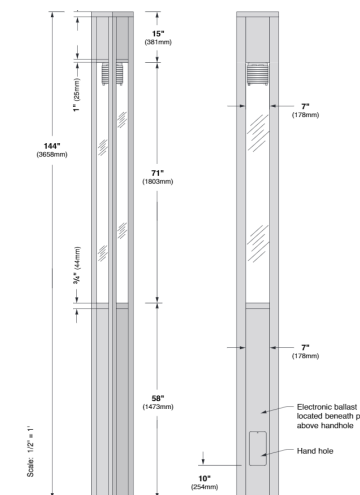
Example of dark sky building-fixed luminaire.



Roadway lighting to follow Township and other roadway regulation minimums



Unshielded lights versus downward shielded



Recommended ornamental pedestrian-scale lighting for northern entry on Latson Rd.

## PARKING LOT LANDSCAPING

- **Required Parking Area Landscaping** shall be in accordance with Section 12.02.04 Required Parking Area Landscaping of the Genoa Township Zoning Ordinance.
- Off-street parking areas containing ten (10) or more parking spaces shall be provided with landscaping in accordance with the following table. A minimum of one-third (1/3) of the trees shall be placed on the interior parking area and the remaining may be placed surrounding the parking lot within 18 feet.

MINIMUM TREES IN THE PARKING AREA	
10 - 100 spaces:	1 Canopy tree and 100 sq. ft. of landscaped area per 10 spaces.
101 - 200 spaces:	1 Canopy tree and 100 sq. ft. of landscaped area per 12 spaces.
201 spaces or more:	1 Canopy tree and 100 sq. ft of landscaped area per 15 spaces.

## BUFFER ZONE LANDSCAPING

- Buffers and landscaping may be reduced or waived if woodlands are preserved to achieve the intent.

### Commercial Buffer Yard Requirements:

- For commercial uses adjacent to other commercial uses:
  - Minimum width: 10 feet
  - 1 canopy or evergreen tree or 4 shrubs per each twenty (20) linear feet along the property line, rounded upward

### Buffering Between Residential and Commercial Uses.

- For commercial uses adjacent to residential uses:
  - Minimum width: 50 feet
  - 6 foot high continuous wall or 4 foot high berm, landscaped detention pond or preservation of natural woodlot.
  - 1 canopy tree, 2 evergreen trees and 4 shrubs per each twenty (20) linear feet along the property line, rounded upward

### Notes:

- Existing quality trees (hickory, oak, maple) with a caliper of at least eight (8) inches shall count as two (2) trees toward the buffer requirements.
- Canopy trees shall have a minimum caliper of 2.5 inches at the time of planting.
- Evergreens shall have a minimum height of six (6) feet at the time of planting.
- At least 50% of the shrubs shall be 24 inches tall at planting, with the remainder over 18 inches.

## STATE REGULATED WETLANDS

- An undisturbed natural setback shall be maintained twenty-five (25) feet from a MDEQ determined/regulated wetland. Trails and recreational areas may be allowed in the wetland setback.
- Demarcation signs will be included for the natural feature setback areas.

## LATSON ROAD AND BECK ROAD LANDSCAPING

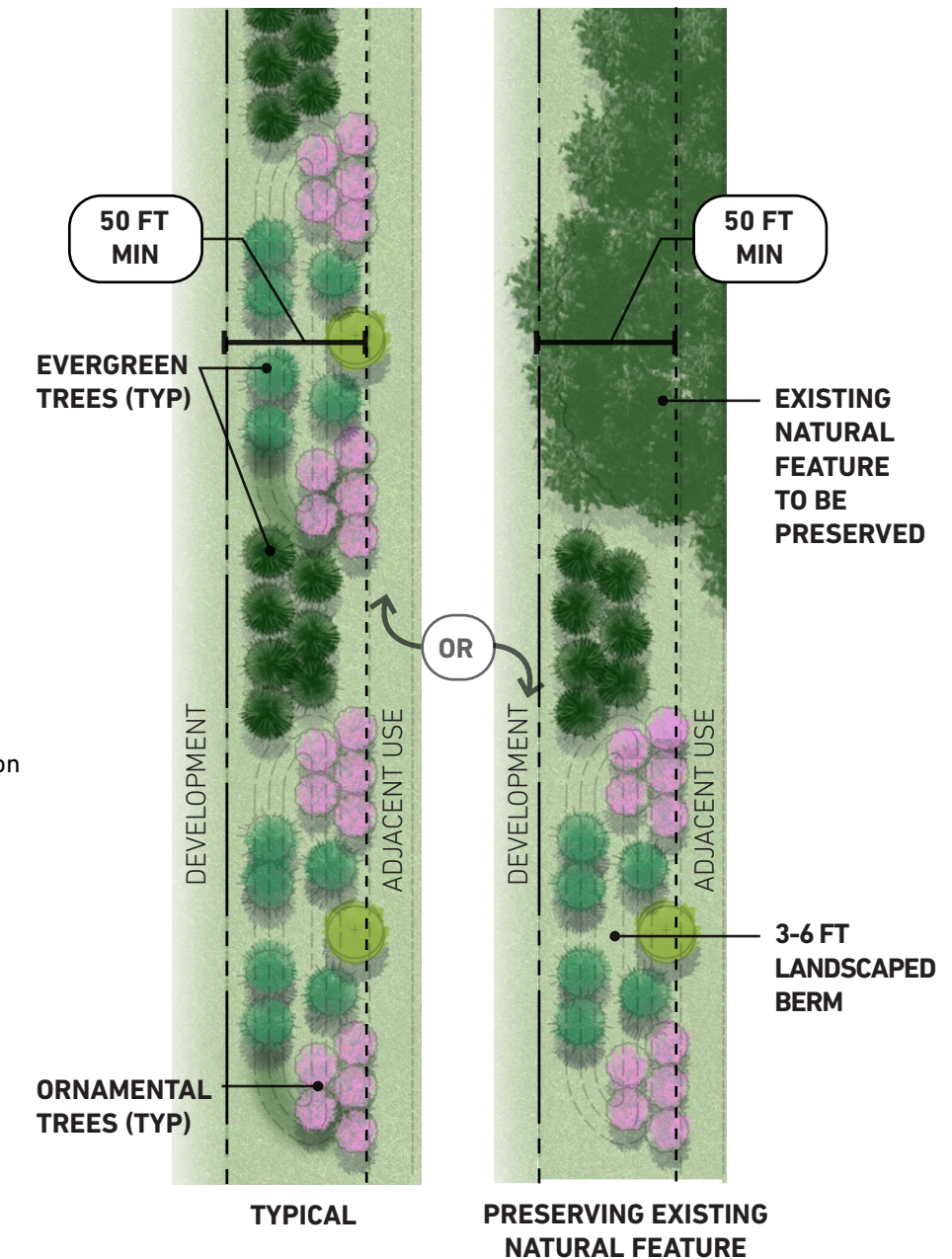
- Street trees shall meet the Township minimum planting requirements along Latson and Beck Roads.
- Latson Road frontage landscaping shall be the same in this area as proposed in the campus to the south.
- Beck Road frontage landscaping shall comply with Township Ordinance for greenbelt along street frontage or greater.
- Landscaping shall include an enhanced greenbelt along the road frontage with low undulating architectural feature such as decorative stone or brick wall, wrought iron fencing, or a combination.
- Landscaping shall meet the requirements of Section 10.03.05(e) (8) of the Township Zoning Ordinance with respect to detention areas.
- Landscaping will help minimize visibility of the gas station canopy.



Commercial concept illustrates potential uses and access configuration.

## LANDSCAPE DESIGN GUIDELINES

### 50 FT LANDSCAPE BUFFER TO ADJACENT USES

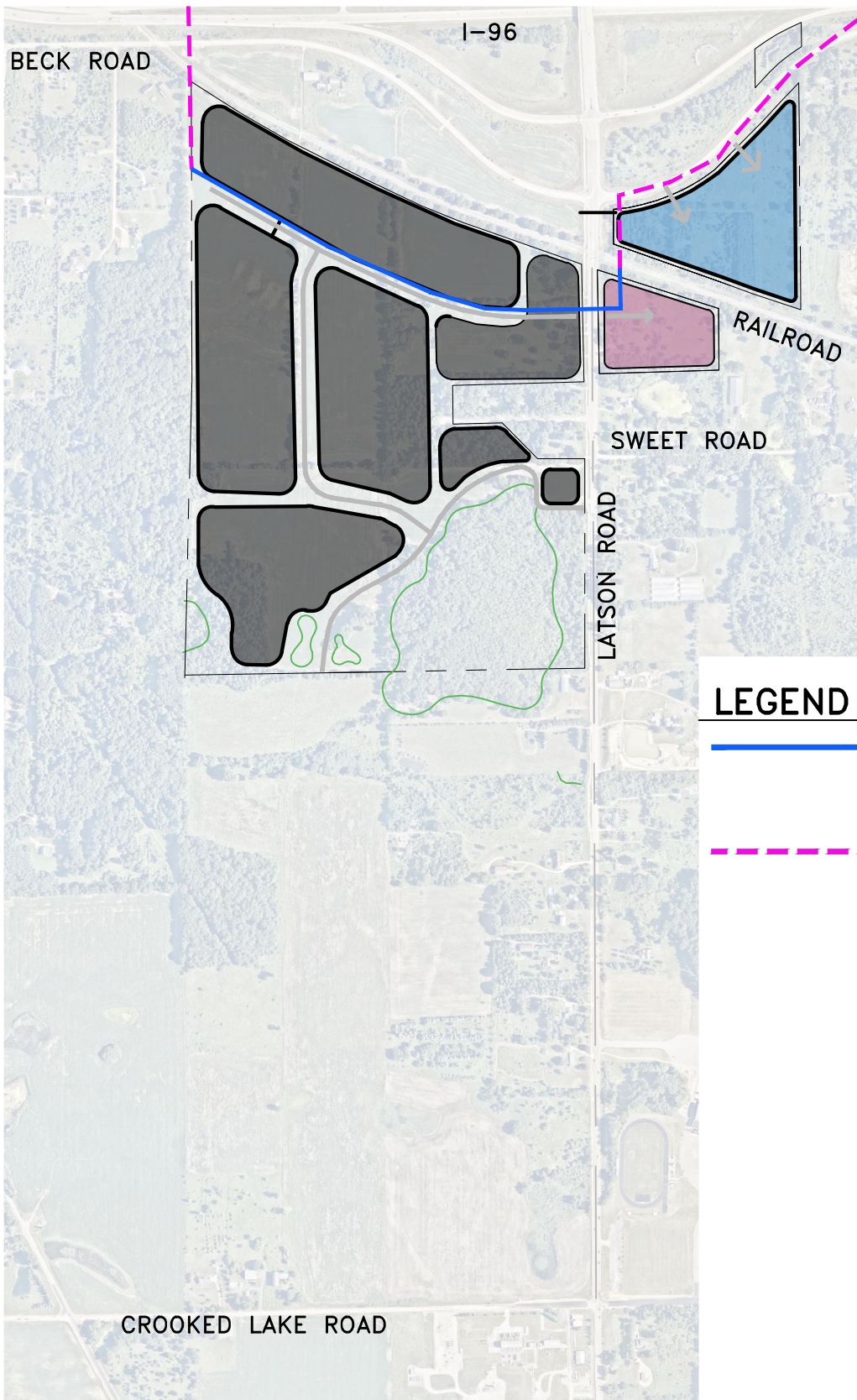


**EXHIBIT 7**

(Utility Plans)

102984.000185 4883-5210-1323.1

*[Exhibit to Planned Unit Development Agreement (Latson Road Commercial)]*



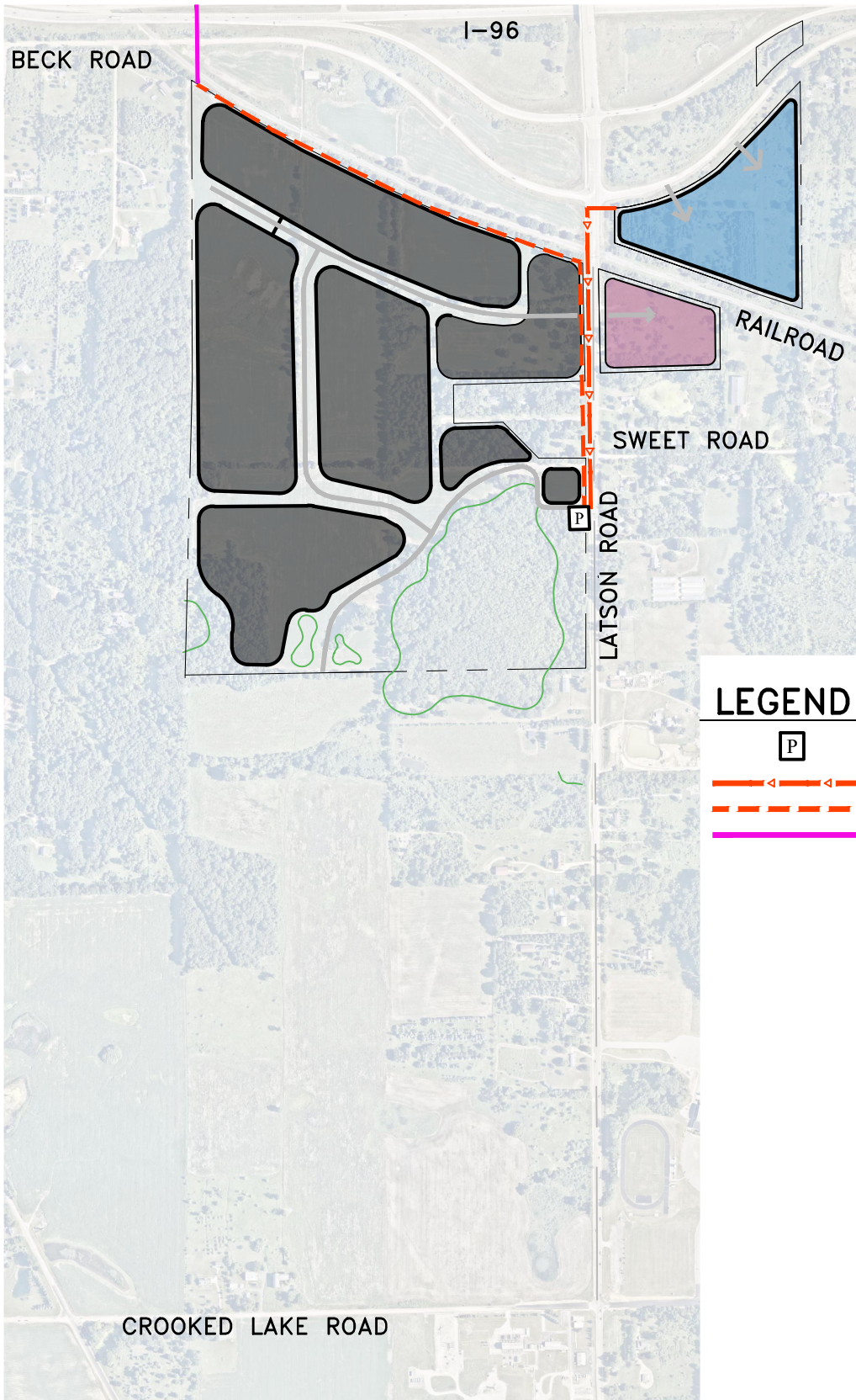
**LEGEND**

- PR WATERMAIN  
(TO BE INSTALLED AS PART OF THE LATSON ROAD PUD)
- - - - - EX WATERMAIN





**WATERMAIN CONCEPT**

NOTE:  
WATER MAIN TO BE A COMBINATION OF 8" AND 12" IN DIAMETER

PROJECT NO.: 16001784  
DATE: 2024-01-04



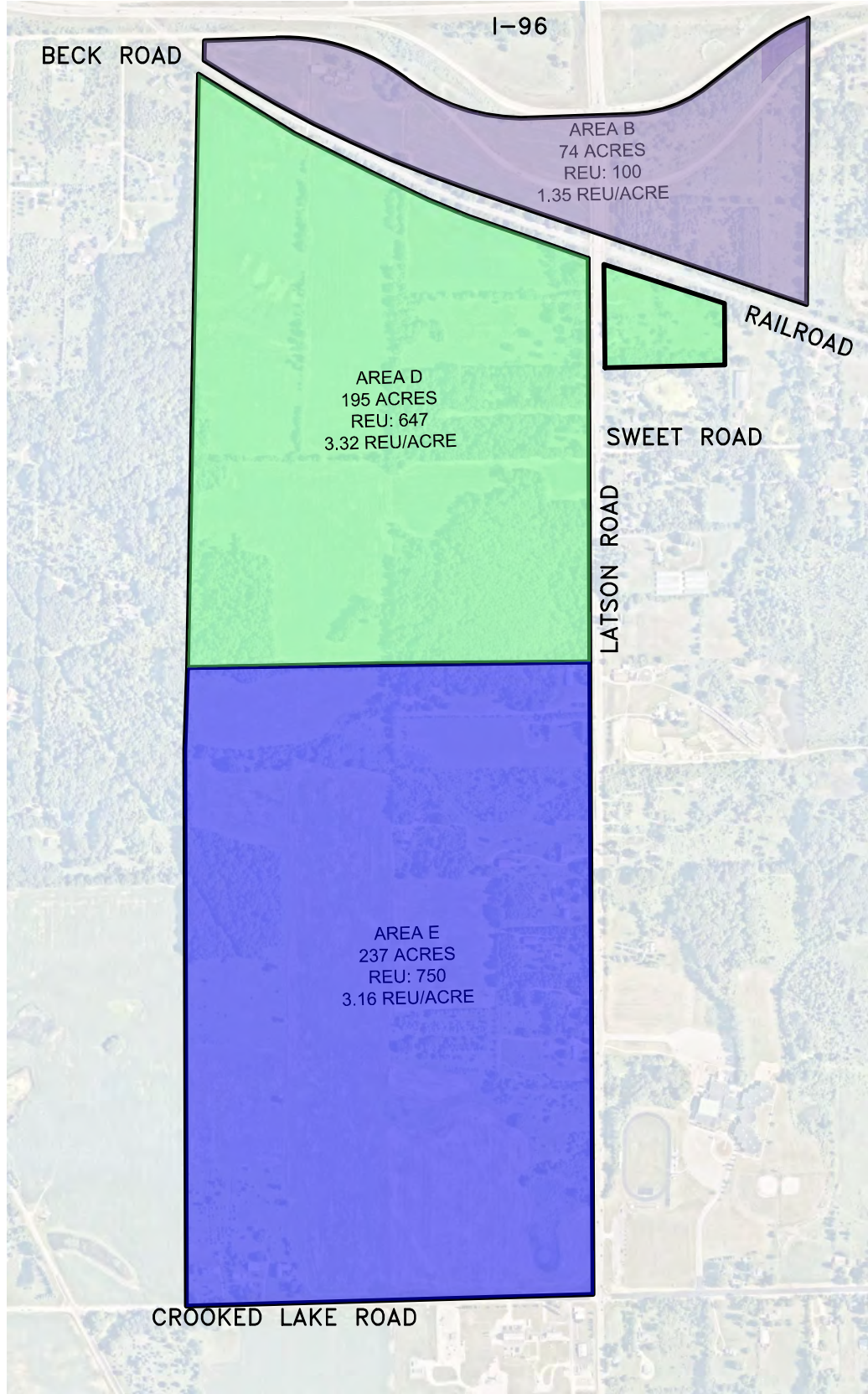
**LEGEND**

-  PUMP STATION
-  PR GRAVITY SEWER
-  PR FORCE MAIN
-  EX 8" FORCE MAIN

**SANITARY SEWER CONCEPT**

PROJECT NO.: 16001784  
 DATE: 2024-01-04

NOTE:  
 SANITARY SEWER TO BE A COMBINATION OF 8"  
 AND 10" GRAVITY SEWER AND 8" FORCE MAIN



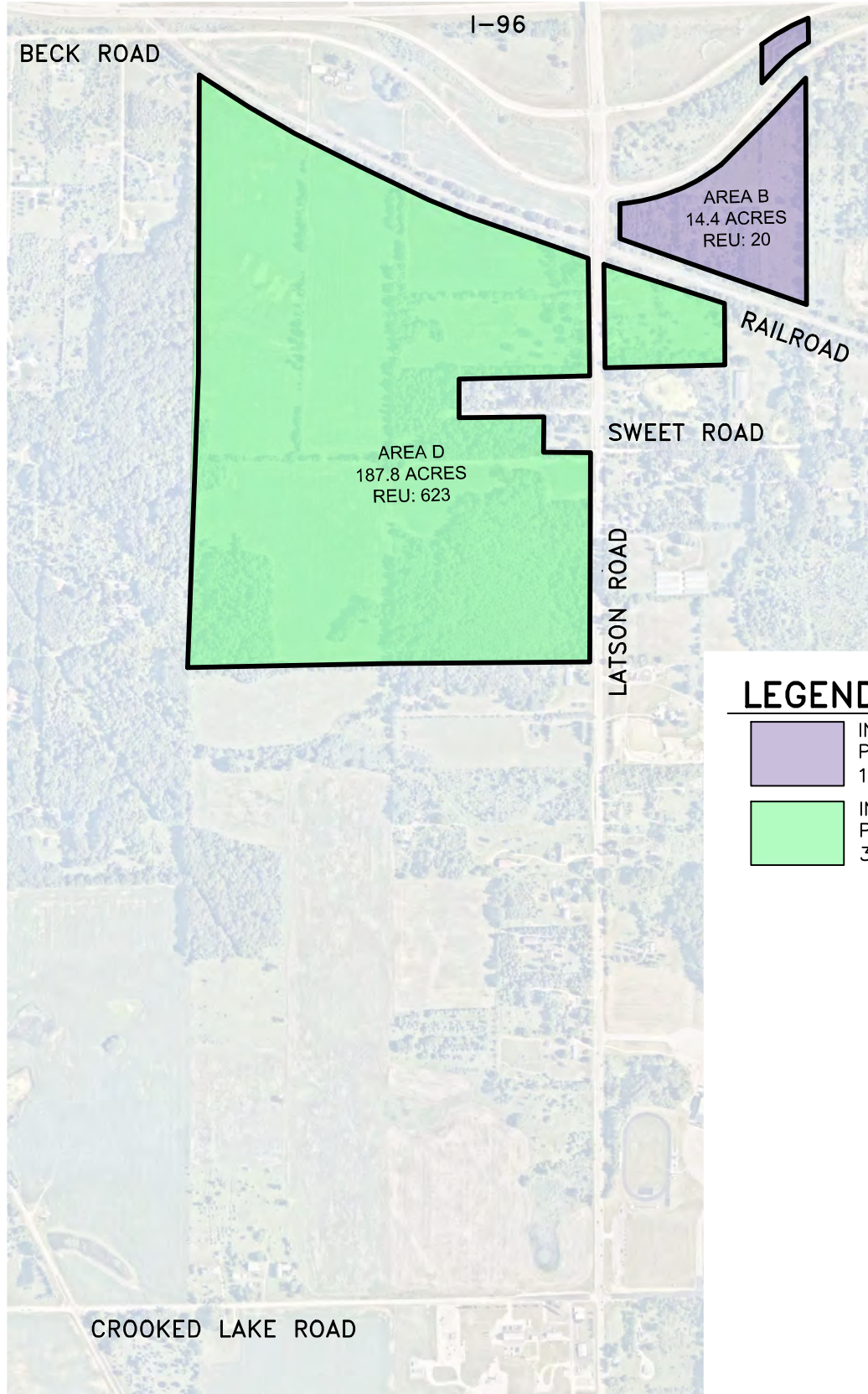
**SOUTH LATSON ROAD SERVICE AREA**

PROJECT NO.: 16001784  
DATE: 2023-08-15



**ATWELL**

866.850.4200 [www.atwell-group.com](http://www.atwell-group.com)



**LEGEND**

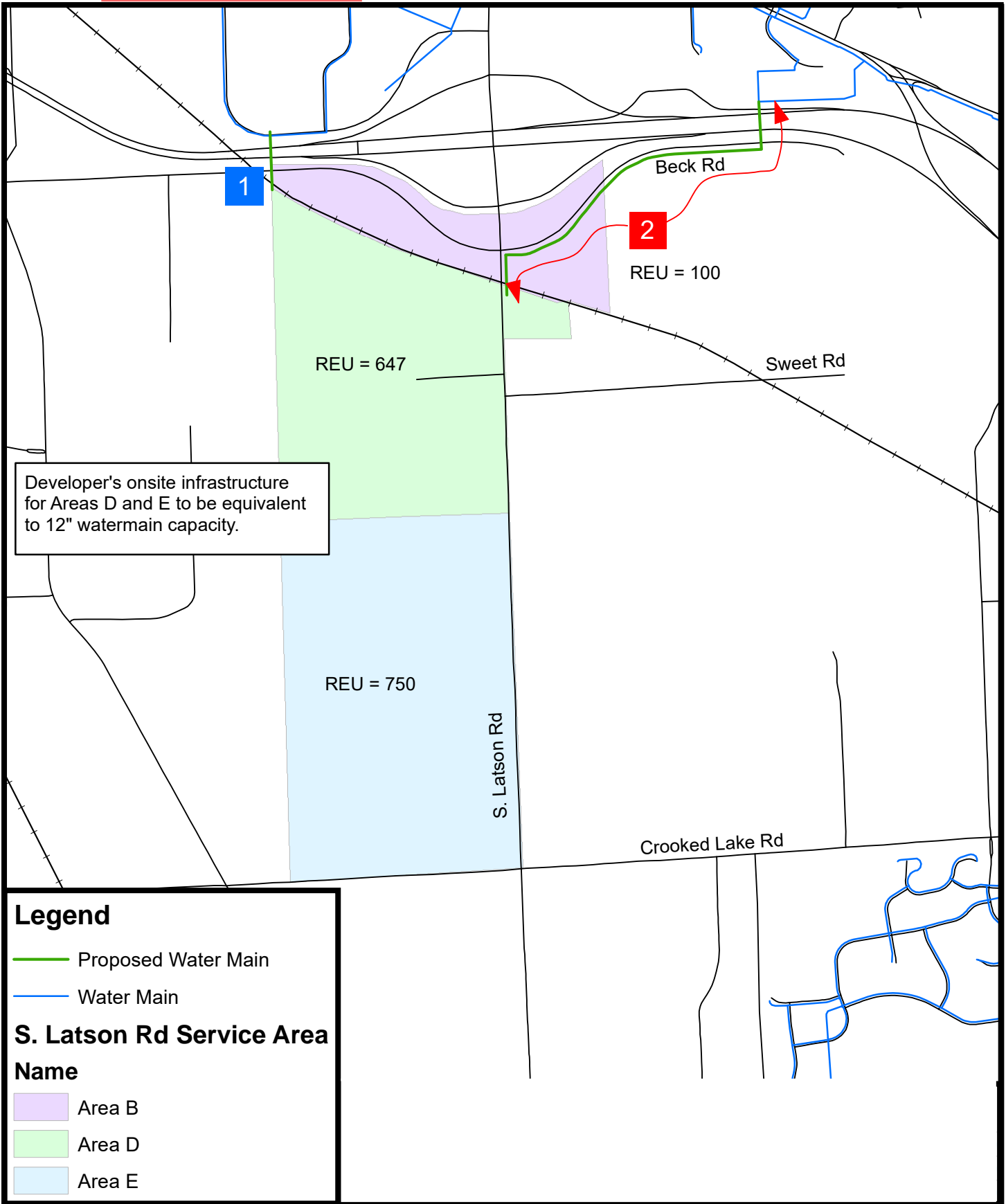
- INTERCHANGE COMMERCIAL  
PUD – AREA B  
1.35 REU/ACRE
- INNOVATION INTERCHANGE  
PUD – AREA D  
3.32 REU/ACRE

**INTERCHANGE COMMERCIAL PUD  
REU ALLOCATION**

PROJECT NO.: 16001784  
DATE: 2024-06-07

**ATWELL**  
866.850.4200 www.atwell-group.com

This graphic is outdated.



1 inch = 1,500 feet

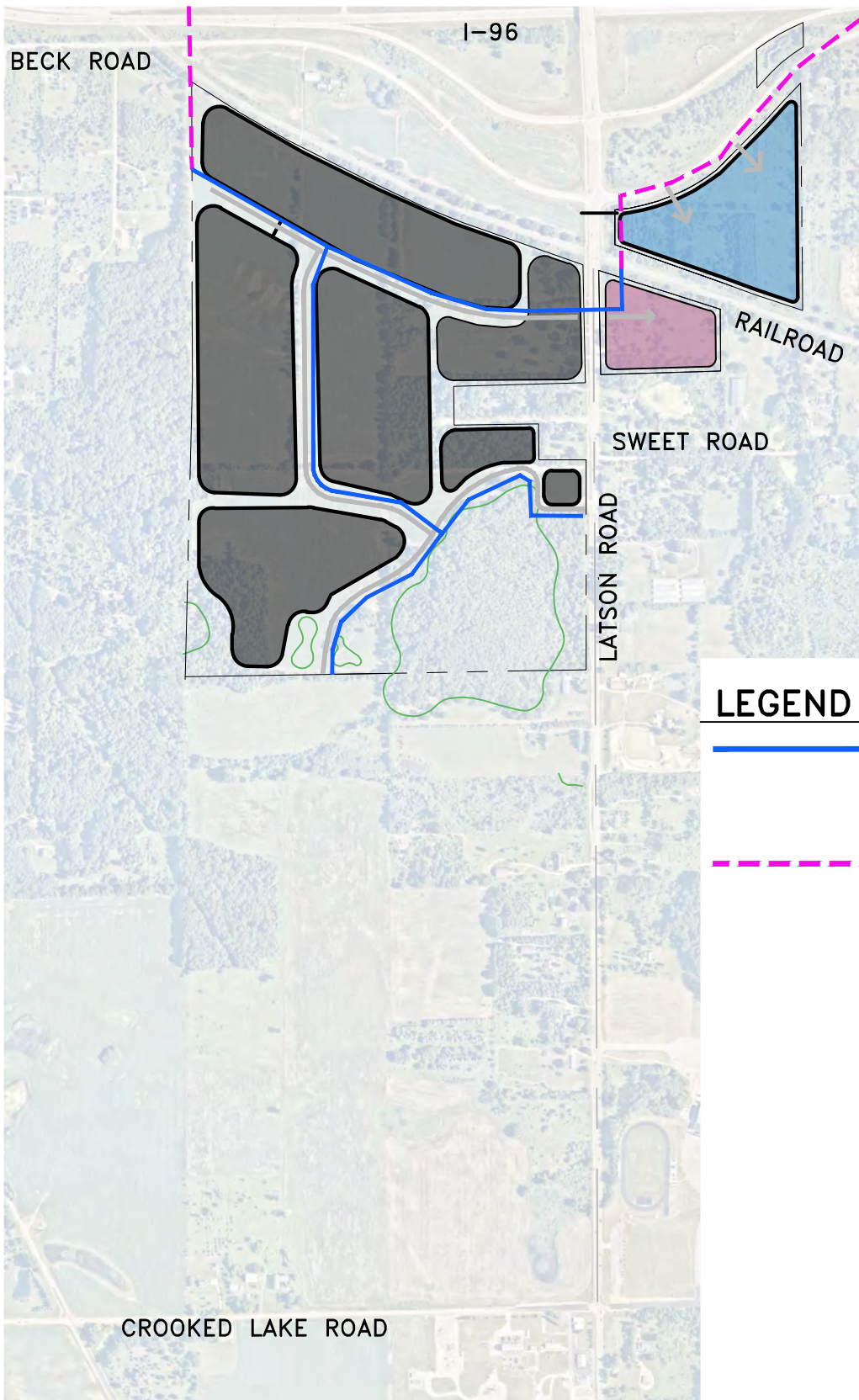
**Figure 1**  
**Water Distribution Infrastructure**

Date: 3/2/2018

Note: This is a graphical representation of the required improvements. Final routing and location will be required during the design phase.







**LEGEND**

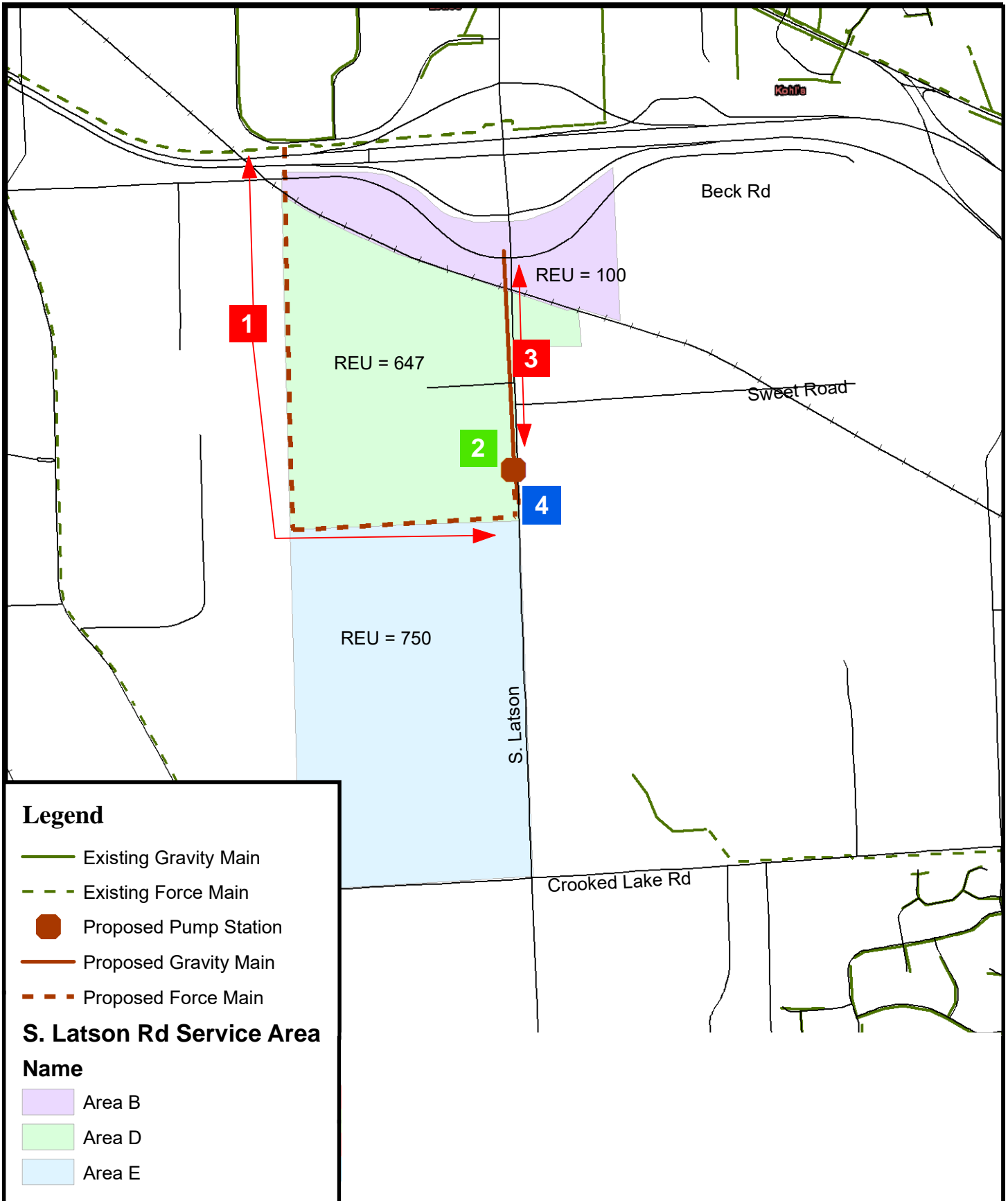
- PR WATERMAIN  
(TO BE INSTALLED AS PART OF THE INNOVATION INTERCHANGE PUD)
- - - - EX WATERMAIN

**WATERMAIN CONCEPT**

NOTE:  
WATER MAIN TO BE A COMBINATION OF 8" AND 12" IN DIAMETER

PROJECT NO.: 16001784  
DATE: 2024-06-07

**ATWELL**  
866.850.4200 [www.atwell-group.com](http://www.atwell-group.com)  
201



1 inch = 1,500 feet

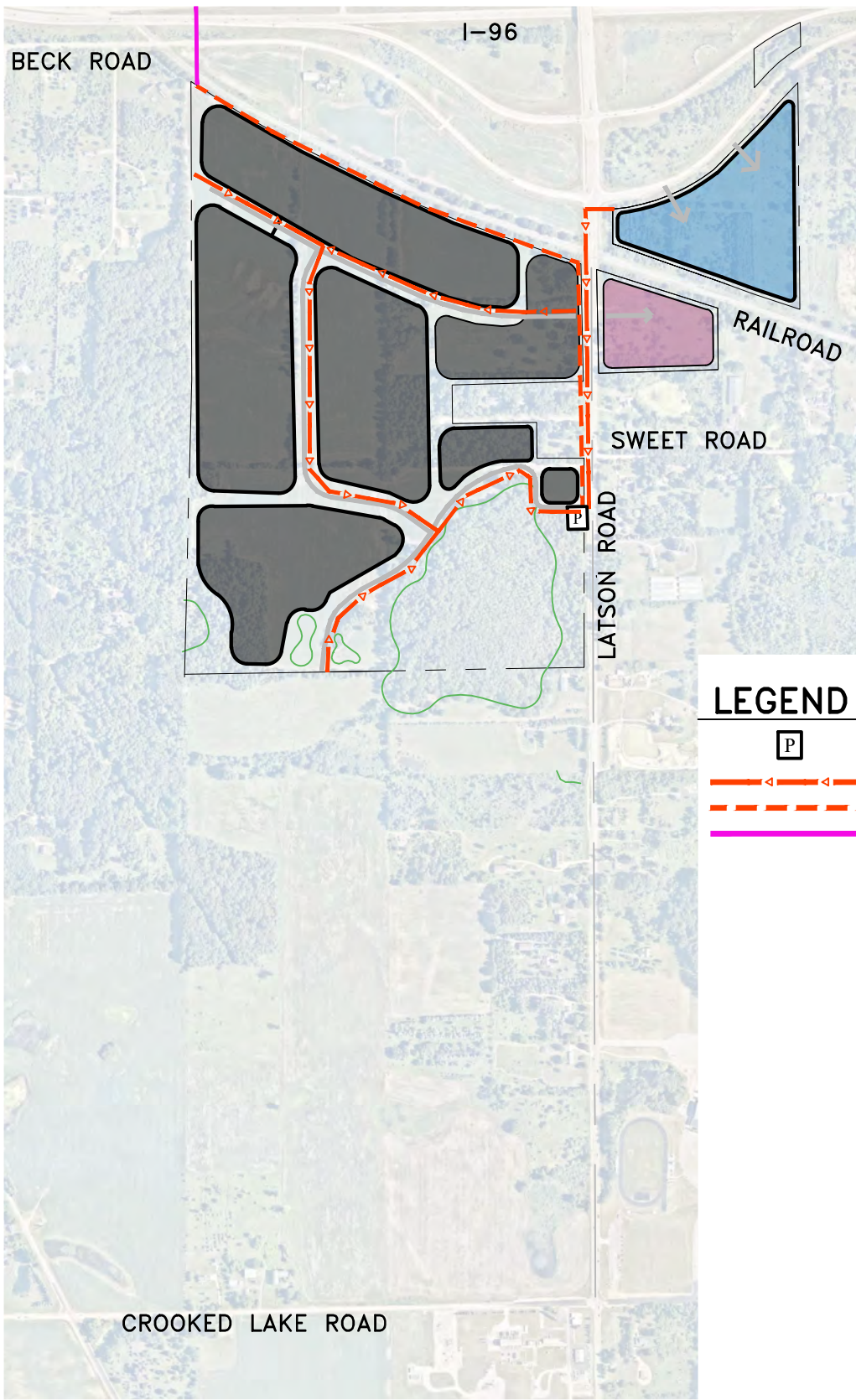
**Figure 2**

Date: 3/2/2018





**Sanitary Sewer Collection Infrastructure**

Note: This is a graphical representation of the required improvements. Final routing and location will be required during the design phase





**LEGEND**

-  PUMP STATION
-  PR GRAVITY SEWER
-  PR FORCE MAIN
-  EX 8" FORCE MAIN

**SANITARY SEWER CONCEPT**

PROJECT NO.: 16001784  
 DATE: 2024-06-07

NOTE:  
 SANITARY SEWER TO BE A COMBINATION OF 8"  
 AND 10" GRAVITY SEWER AND 8" FORCE MAIN

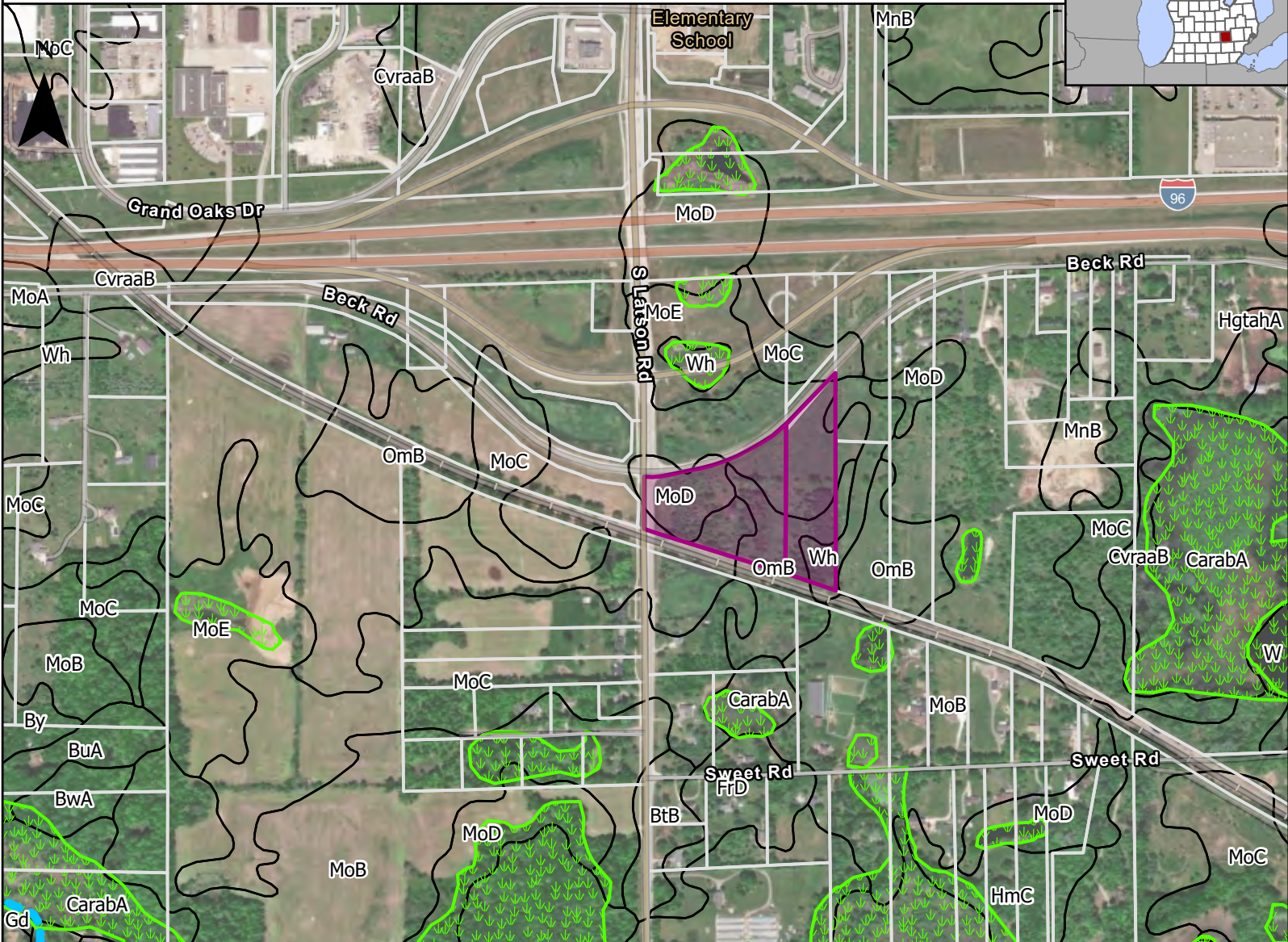


**ATWELL**  
 866.850.4200 www.atwell-group.com

# Interchange Commercial PUD

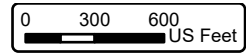
Site Map - Soils and Wetlands

Genoa Township, Livingston County, Michigan



Symbol	Name
BtB	Boyer-Oshtemo loamy sands, 2 to 6 percent slopes
BuA	Brady loamy sand, 0 to 2 percent slopes
FoB	Fox sandy loam, 2 to 6 percent slopes
Gd	Gilford sandy loam, 0 to 2 percent slopes, gravelly subsoil
MnB	Metea loamy sand, 2 to 6 percent slopes
MoB	Wawasee loam, 2 to 6 percent slopes
MoC	Wawasee loam, 6 to 12 percent slopes
MoD	Miami loam, 12 to 18 percent slopes
MoE	Miami loam, 18 to 25 percent slopes
OmB	Owosso-Miami sandy loams, 2 to 6 percent slopes
Wh	Washtenaw silt loam
CvraaB	Conover loam, 0 to 4 percent slopes
CarabA	Carlisle muck, 0 to 2 percent slopes

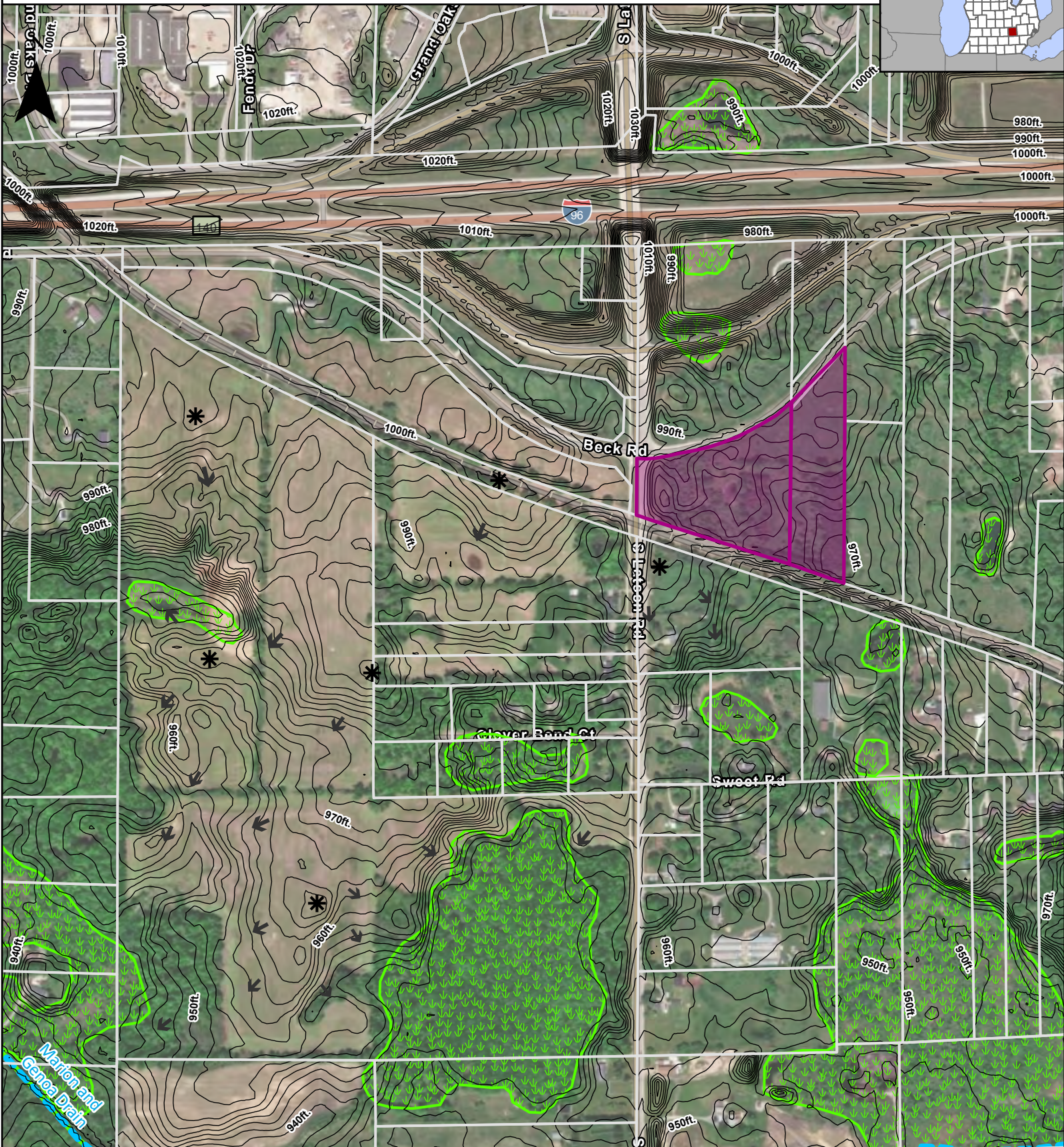
Interchange Commercial PUD  
 Parcel Boundary  
 NWI Wetlands  
 Soil Types



# Interchange Commercial PUD


## Topography & Natural Features

Genoa Township, Livingston County, Michigan



 Interchange Commercial PUD

 Parcel Boundary

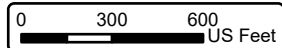
 Local High Point

 NWI Wetlands

 2 Ft Contour

 Estimated Flow Direction

 Watercourse NHD



# MEMO

VIA EMAIL [todd@versacos.com](mailto:todd@versacos.com)

**To:** Latson South, LLC

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**From:** Julie M. Kroll, PE, PTOE  
Salman Ahmad  
Fleis & VandenBrink

---

**Date:** July 26, 2024  
Revised August 23, 2024

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**Re:** Latson Road Interchange Commercial PUD  
Genoa Township, Michigan  
Traffic Impact Study

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## 1 INTRODUCTION

This memorandum presents the revised results of the Traffic Impact Study (TIS), previously submitted on July 26, 2024, for the proposed Latson Road Commercial Planned Unit Development (PUD) in Genoa Township, Michigan. The project site is located on undeveloped property, generally in the southeast quadrant of the Latson Road & Beck Road intersection, as shown on the attached **Figure 1**. The proposed development includes the construction of approximately 15-Acres of property for a commercial PUD project. The project site consists of two (2) adjacent parcels; an approximately 9-acre parcel (designated as Tax ID No. 11-09-300-46) and an approximately 6-acre parcel (designated as Tax ID No. 11-09-300-001). Site access for the property is provided via Beck Road, no access to Latson Road is proposed with this development plan.

The proposed project includes rezoning the 9-acre parcel to Interchange Commercial PUD (ICPUD), the 6-acre parcel is currently zoned ICPUD. The proposed ICPUD zoning would permit the development of a variety of land uses on the property. For purposes of this evaluation, a convenience store with fueling stations and a retail commercial shopping plaza were assumed to represent a conservative evaluation of the potential traffic impacts of the site associated with the proposed ICPUD zoning.

The scope of this study was developed based on Fleis & VandenBrink's (F&V) knowledge of the study area, understanding of the development program, accepted traffic engineering practice and information published by the Institute of Transportation Engineers (ITE). The study analyses were completed using Synchro/SimTraffic (Version 12). Sources of data for this study include F&V subconsultant Quality Counts, LLC (QC), Livingston County Road Commission (LCRC), ITE, the Michigan Department of Transportation (MDOT), the Southeast Michigan Council of Governments (SEMCOG), and information provided by the developer.

## 2 BACKGROUND

### 2.1 EXISTING ROAD NETWORK

Vehicle transportation for the proposed development is provided via Latson Road; with regional transportation being provided via I-96, located just north of the project site. The lane use and traffic control at the study intersections are shown on the attached **Figure 2** and the study roadways are further described below. For the purposes of this study, all minor streets, freeway ramps, and driveways are assumed to have an operating speed of 25 miles per hour (mph), unless otherwise noted.

27725 Stansbury Boulevard, Suite 195  
Farmington Hills, MI 48334

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F: 248.536.0079

206  
[www.fveng.com](http://www.fveng.com)

**Latson Road** runs in the north / south directions, adjacent to the west side of the project site. The study section of Latson Road has a prima facie speed limit of 55-mph and is under the jurisdiction of LCRC. Latson Road is classified as a *Minor Arterial* and has an AADT volume of approximately 9,400 vpd (SEMCOG 2018), south of I-96. The study section of roadway north of Cloverbend Road, provides a typical five-lane cross-section, with two (2) lanes of travel in each direction and a center TWLTL. South of Cloverbend Road, Latson Road narrows to provide a typical two-lane cross-section, with one (1) lane of travel in each direction, widening at the Crooked Lake Road intersection to provide exclusive left-turn lanes in both directions.

**I-96** runs in the east / west directions, north of the project site. I-96 has an Average Annual Daily Traffic (AADT) volume of approximately 56,000 (SEMCOG 2018) vehicles per day (vpd), is classified as an *Interstate*, and is under the jurisdiction of MDOT. The study section of roadway has a posted speed limit of 70-mph and provides a six-lane, median divided cross-section, with three (3) lanes of travel in each direction. At the intersection of Latson Road & EB I-96 Exit-Ramp, the eastbound approach provides dual (2) left-turn lanes and an exclusive right-turn lane. At the intersection of Latson Road & WB I-96 exit-ramp, the westbound approach provides an exclusive left-turn lane and dual (2) right-turn lanes.

**Beck Road** runs in the east / west directions, adjacent to the north side of the project site. Beck Road is under the jurisdiction of LCRC and has prima facie speed limit of 55-mph. Beck Road is classified as a *Local Road* and provides a typical two-lane cross-section, with one (1) lane of travel in each direction; exclusive left-turn lanes are provided on both approaches to Latson Road. Beck Road is paved for approximately 500-ft east and west of Latson Road; however, beyond the paved section, Beck Road is a gravel roadway.

## 2.2 EXISTING TRAFFIC VOLUMES

F&V subconsultant QC collected Turning Movement Count (TMC) data on Tuesday May 2, 2023<sup>1</sup>, during the AM (7:00 AM-9:00 AM) and PM (3:00 PM-6:00 PM) peak periods at the following study intersections:

- Latson Road & Beck Road
- Latson Road & EB I-96 Ramps
- Latson Road & WB I-96 Ramps

During collection of the turning movement counts, Peak Hour Factors (PHFs), pedestrian and bike volumes, and commercial truck percentages were recorded and used in the traffic analysis. Through volumes were carried through the study roadway network and balanced at the proposed site driveway locations. Therefore, the traffic volumes used in the analysis and shown on the attached traffic volume figures may not match the raw traffic volumes shown in the data collection.

The weekday AM and PM peak hours for the adjacent roadway network were observed to generally occur between 8:00 AM to 9:00 AM and 4:30 PM to 5:30 PM, respectively. F&V collected an inventory of existing lane use and traffic controls, as shown on the attached **Figure 2**. Additionally, F&V obtained the current traffic signal timing information from MDOT and LCRC.

The existing 2024 peak hour traffic volumes used in the analysis are shown on the attached **Figure 3**. All applicable background data referenced in this memorandum is attached.

## 3 EXISTING CONDITIONS (2024)

Existing peak hour vehicle delays and Levels of Service (LOS) were calculated at the study intersection using Synchro/SimTraffic (Version 12) traffic analysis software. This analysis was based on the existing lane use and traffic control shown on the attached **Figure 2**, the existing peak hour traffic volumes shown on the attached **Figure 3**, and the methodologies presented in the *Highway Capacity Manual, 7<sup>th</sup> Edition* (HCM7).

Descriptions of LOS "A" through "F", as defined in the HCM7, are attached. Typically, LOS D is considered acceptable, with LOS A representing minimal delay, and LOS F indicating failing conditions. Additionally, SimTraffic network simulations were reviewed to evaluate network operations and vehicle queues. The results of the existing conditions analysis are attached and summarized in **Table 1**.

The results of the existing conditions analysis indicates that all approaches and movements at the study intersections are currently operating acceptably, at LOS D or better during both the AM and PM peak hours.

---

<sup>1</sup> An annual growth rate of 0.72% was applied to the 2023 traffic volumes, in order to forecast the existing 2024 traffic volumes used in the study.

Review of the SimTraffic network simulations at all of the remaining study intersections indicates acceptable traffic operations throughout the study roadway network during both the AM and PM peak hours.

**Table 1: Existing Intersection Operations**

Intersection	Control	Approach	Existing Conditions			
			AM Peak		PM Peak	
			Delay (s/veh)	LOS	Delay (s/veh)	LOS
Latson Road & WB I-96 Ramps	Signalized	WBL	33.0	C	26.8	C
		WBR	38.1	D	33.8	C
		NBL	1.1	A	5.2	A
		NBT	0.3	A	0.4	A
		SBT	7.2	A	17.2	B
		SBR	8.0	A	20.1	C
		<b>Overall</b>	<b>7.6</b>	<b>A</b>	<b>15.3</b>	<b>B</b>
Latson Road & EB I-96 Ramps	Signalized	EBL	33.8	C	33.5	C
		EBR	29.4	C	30.3	C
		NBT	5.1	A	5.4	A
		NBR	4.8	A	4.7	A
		SBL	2.2	A	2.4	A
		SBT	0.1	A	0.2	A
		<b>Overall</b>	<b>13.7</b>	<b>B</b>	<b>11.8</b>	<b>B</b>
Latson Road & Beck Road	Stop (Minor)	EBL	12.5	B	17.1	C
		EBTR	0.0*	A	8.9	A
		WBL	0.0*	A	0.0*	A
		WBTR	9.4	A	9.8	A
		NBL	0.0*	A	0.0*	A
		SBL	8.3	A	8.3	A

\* Indicates no vehicle volume present.

#### 4 BACKGROUND GROWTH

The Southeast Michigan Council of Governments (SEMCOG), the multi-jurisdictional agency responsible for the transportation planning in Southeast Michigan, maintains the regional transportation planning models and provides information regarding projected growth rates along roadways throughout their jurisdiction. The SEMCOG traffic volume forecast models were utilized to calculate background growth rates on the adjacent study sections of Latson Road for use in this analysis; indicating the following growth rates, compounded annually, from 2020 to 2050. This information was used to determine the applicable growth rate to project the existing 2024 traffic volumes to the build-out year of 2029. The growth rates for the study corridors determined by the SEMCOG forecast models are attached and summarized in **Table 2**.

**Table 2: SEMCOG Growth Rates**

Road	Limits	Growth Rate
Latson Road	Chilson Road to Crooked Lake Road	0.72%
Latson Road	Crooked Lake Road to I-96	0.68%

In addition to background growth, the following future developments were also considered in the background conditions analysis. The following developments were identified by the Township to account for traffic that will be generated by approved developments within the vicinity of the study area.



- St. Joseph Mercy Health Center Expansion
- Latson Road PUD

Therefore, a conservative annual growth rate of **0.72%** per year was utilized for the study roadway network. It is anticipated that a percentage of the expected growth along Latson Road will be generated by the proposed development and the background developments. However, in order to provide a more conservative evaluation, the full growth rate was applied to the study intersections.

The site-generated trips were obtained for the background development from the Traffic Impact Study (TIS) completed; the TIS excerpts are attached for reference. The background development trips were added to the existing traffic volumes, after applying the background growth rate, in order to forecast the background 2029 peak hour traffic volumes **without the proposed development**, as shown on the attached **Figure 4**.

## 5 BACKGROUND CONDITIONS (2029)

### 5.1 INTERSECTION ANALYSIS

Background peak hour vehicle delays and LOS **without the proposed development** were calculated at the study intersections based on the existing lane use and traffic control shown on the attached **Figure 2**, the background peak hour traffic volumes shown on the attached **Figure 4**, and the methodologies presented in the HCM7. The results of the background conditions analysis are attached and summarized in **Table 3**.

**Table 3: Background Intersection Operations**

Intersection	Control	Approach	Existing Conditions (2024)				Background Conditions (2029)				Difference			
			AM Peak		PM Peak		AM Peak		PM Peak		AM Peak		PM Peak	
			Delay (s/veh)	LOS	Delay (s/veh)	LOS	Delay (s/veh)	LOS	Delay (s/veh)	LOS	Delay (s/veh)	LOS	Delay (s/veh)	LOS
Latson Road & WB I-96 Ramps	Signal	WBL	33.0	C	26.8	C	37.9	D	27.0	C	4.9	C→D	0.2	-
		WBR	38.1	D	33.8	C	34.9	C	33.8	C	-3.2	D→C	0.0	-
		NBL	1.1	A	5.2	A	5.8	A	54.5	D	4.7	-	49.3	A→D
		NBT	0.3	A	0.4	A	0.4	A	0.6	A	0.1	-	0.2	-
		SBT	7.2	A	17.2	B	14.6	B	18.1	B	7.4	A→B	0.9	-
		SBR	8.0	A	20.1	C	15.1	B	21.4	C	7.1	A→B	1.3	-
		<b>Overall</b>	<b>7.6</b>	<b>A</b>	<b>15.3</b>	<b>B</b>	<b>12.1</b>	<b>B</b>	<b>19.0</b>	<b>B</b>	<b>4.5</b>	<b>A→B</b>	<b>3.7</b>	<b>-</b>
Latson Road & EB I-96 Ramps	Signal	EBL	33.8	C	33.5	C	29.3	C	32.9	C	-4.5	-	-0.6	-
		EBR	29.4	C	30.3	C	36.4	D	31.2	C	7.0	C→D	0.9	-
		NBT	5.1	A	5.4	A	7.5	A	6.6	A	2.4	-	1.2	-
		NBR	4.8	A	4.7	A	7.0	A	5.7	A	2.2	-	1.0	-
		SBL	2.2	A	2.4	A	7.0	A	11.6	B	4.8	-	9.2	A→B
		SBT	0.1	A	0.2	A	0.3	A	0.3	A	0.2	-	0.1	-
		<b>Overall</b>	<b>13.7</b>	<b>B</b>	<b>11.8</b>	<b>B</b>	<b>14.5</b>	<b>B</b>	<b>12.1</b>	<b>B</b>	<b>0.8</b>	<b>-</b>	<b>0.3</b>	<b>-</b>
Latson Road & Beck Road	Stop (Minor)	EBL	12.5	B	17.1	C	23.8	C	30.1	D	11.3	B→C	13.0	C→D
		EBTR	0.0*	A	8.9	A	0.0*	A	9.0	A	0.0*	-	0.1	-
		WBL	0.0*	A	0.0*	A	0.0*	A	0.0*	A	0.0*	-	0.0*	-
		WBTR	9.4	A	9.8	A	10.0	B	11.8	B	0.6	A→B	2.0	A→B
		NBL	0.0*	A	0.0*	A	0.0*	A	0.0*	A	0.0*	-	0.0*	-
		SBL	8.3	A	8.3	A	8.8	A	10.0	B	0.5	-	1.7	A→B

\* Indicates no vehicle volume present *Note: Decreased delays are the result of improved progression and/or HCM weighting methodologies.*

The results of the background conditions analysis indicates that all approaches and movements at the study intersections are expected to continue operating acceptably, at LOS D or better during both peak periods, in a manner similar to the existing conditions analysis, with some minor increases in delays.

Review of SimTraffic microsimulations indicates generally acceptable operations, throughout the study roadway network during the AM peak hour; however, during the PM peak hour, long vehicle queues are present for the left-turn movements at both of the I-96 Freeway Ramps along Latson Road.

The delays and queueing along Latson Road at the I-96 Freeway Ramps are the result of the background developments and expected growth throughout the study area; these vehicle queues were not observed to dissipate and were typically present throughout the peak hour.

### 5.2 BACKGROUND IMPROVEMENTS

In order to improve the projected background vehicle queue lengths at the study intersections, mitigation measures were investigated, including: geometric improvements and traffic control modifications. The results of the evaluation indicates that the following mitigation measures may be necessary to accommodate the background growth rate and future developments; these should be evaluated as part of the site plan approval and permitting process.

#### Latson Road & WB I-96 Ramps

- Provide permissive/protected northbound left-turn phasing with vehicle detection.
- Increase the network-wide cycle length to 90-seconds for all signals along the Latson Road corridor.

#### Latson Road & EB I-96 Ramps

- Provide permissive/protected southbound left-turn phasing with vehicle detection.
- Increase the network-wide cycle length to 90-seconds for all signals along the Latson Road corridor.

The results of the background conditions with improvement analysis are attached and summarized in **Table 4**. Results of the background improvements analysis, with the implementation of the recommended mitigation measures, indicates that all approaches and movements are expected to continue operating acceptably, at LOS D or better, during both peak periods.

Review of SimTraffic network simulations, also indicate acceptable operations during both peak periods. Occasional periods of vehicle queues were observed at the signalized study intersections; however, these queues were observed to be serviced within each cycle length, leaving no residual vehicle queueing.

**Table 4: Background Intersection Operations with Improvements**

Intersection	Control	Approach	Background Conditions				Background w/ IMP				Difference			
			AM Peak		PM Peak		AM Peak		PM Peak		AM Peak		PM Peak	
			Delay (s/veh)	LOS	Delay (s/veh)	LOS	Delay (s/veh)	LOS	Delay (s/veh)	LOS	Delay (s/veh)	LOS	Delay (s/veh)	LOS
Latson Road & WB I-96 Ramps	Signal	WBL	37.9	D	27.0	C	43.5	D	31.7	C	5.6	-	4.7	-
		WBR	34.9	C	33.8	C	39.9	D	49.4	D	5.0	C→D	15.6	C→D
		NBL	5.8	A	54.5	D	4.9	A	10.0	A	-0.9	-	-44.5	D→A
		NBT	0.4	A	0.6	A	0.3	A	0.5	A	-0.1	-	-0.1	-
		SBT	14.6	B	18.1	B	0.4	A	10.8	B	-14.2	B→A	-7.3	-
		SBR	15.1	B	21.4	C	0.9	A	13.9	B	-14.2	B→A	-7.5	C→B
		<b>Overall</b>	<b>12.1</b>	<b>B</b>	<b>19.0</b>	<b>B</b>	<b>6.8</b>	<b>A</b>	<b>14.7</b>	<b>B</b>	<b>-5.3</b>	<b>B→A</b>	<b>-4.3</b>	<b>-</b>
Latson Road & EB I-96 Ramps	Signal	EBL	29.3	C	32.9	C	34.2	C	40.1	D	4.9	-	7.2	C→D
		EBR	36.4	D	31.2	C	44.7	D	36.0	D	8.3	-	4.8	C→D
		NBT	7.5	A	6.6	A	18.2	B	14.9	B	10.7	A→B	8.3	A→B
		NBR	7.0	A	5.7	A	16.9	B	12.8	B	9.9	A→B	7.1	A→B
		SBL	7.0	A	11.6	B	9.9	A	9.5	A	2.9	-	-2.1	B→A
		SBT	0.3	A	0.3	A	0.3	A	0.2	A	0.0	-	-0.1	-
		<b>Overall</b>	<b>14.5</b>	<b>B</b>	<b>12.1</b>	<b>B</b>	<b>19.9</b>	<b>B</b>	<b>16.6</b>	<b>B</b>	<b>5.4</b>	<b>-</b>	<b>4.5</b>	<b>-</b>

Intersection	Control	Approach	Background Conditions				Background w/ IMP				Difference			
			AM Peak		PM Peak		AM Peak		PM Peak		AM Peak		PM Peak	
			Delay (s/veh)	LOS	Delay (s/veh)	LOS	Delay (s/veh)	LOS	Delay (s/veh)	LOS	Delay (s/veh)	LOS	Delay (s/veh)	LOS
Latson Road & Beck Road	Stop (Minor)	EBL	23.8	C	30.1	D	23.7	C	29.8	D	-0.1	-	-0.3	-
		EBTR	0.0*	A	9.0	A	0.0*	A	9.0	A	0.0*	-	0.0	-
		WBL	0.0*	A	0.0*	A	0.0*	A	0.0*	A	0.0*	-	0.0*	-
		WBTR	10.0	B	11.8	B	10.0	B	11.8	B	0.0	-	0.0	-
		NBL	0.0*	A	0.0*	A	0.0*	A	0.0*	A	0.0*	-	0.0*	-
		SBL	8.8	A	10.0	B	8.8	A	10.0	B	0.0	-	0.0	-

\* Indicates no vehicle volume present

## 6 SITE TRIP GENERATION

The number weekday peak hour (AM and PM) and daily vehicle trips that would be generated by the proposed development was forecast based on data published by ITE in the *Trip Generation Manual, 11<sup>th</sup> Edition* and the *ITE Trip Generation Handbook, 3<sup>rd</sup> Edition*. The end user(s) for the proposed ICPUD have not been identified at this time and may include a variety of potential developments that are approved as part of the ICPUD zoning. For purposes of this evaluation, a convenience store with fueling station and a retail commercial shopping plaza were assumed to represent a conservative evaluation of the potential traffic impacts of the site associated with the proposed ICPUD zoning. The site trip generation forecast utilized for this TIS is summarized in **Table 5**.

**Table 5: Site Trip Generation Summary**

Land Use	ITE Code	Amount	Units	Average Daily Traffic (vpd)	AM Peak Hour (vph)			PM Peak Hour (vph)		
					In	Out	Total	In	Out	Total
Shopping Plaza (40-150k SF)	821	53,000	SF	3,579	57	35	92	135	140	275
Pass-By	40% PM			716	0	0	0	55	55	110
	New Trips			2,863	57	35	92	80	85	165
Gas Station with Convenience Market	945	10	VFP	3,458	158	158	316	135	134	269
Pass-By	76% AM, 75% PM			2,611	120	120	240	101	101	202
	New Trips			847	38	38	76	34	33	67
	Total Trips			7,037	215	193	408	270	274	544
	Total Pass-By			3,327	120	120	240	156	156	312
	Total New Trips			3,710	95	73	168	114	118	232

As is typical of commercial developments, a portion of the trips generated are from vehicles that are already on the adjacent roadways and will pass the site on the way from an origin to their ultimate destination. Therefore, not all traffic at the site driveways is necessarily new traffic added to the street system. This percentage of the trips generated by the development are considered “pass-by” trips, which are already present within the adjacent street system. These trips are therefore reduced from the total external trips generated by a study site. The pass-by trips for this site were applied to Latson Road and were considered as either pass-by or diverted link, depending on the proposed site access location. The percentage of pass-by trips used in this analysis was determined based on the rates published by ITE in the *Trip Generation Manual, 11<sup>th</sup> Edition*.

## 7 SITE TRIP DISTRIBUTION

The vehicular trips that would be generated by the proposed development were assigned to the study roadway network based on the proposed site access plan and driveway configurations, the existing peak hour traffic patterns in the adjacent roadway network, and the methodologies published by ITE. The ITE trip distribution methodology assumes that new trips will access the development, then return to their direction of origin, whereas pass-by trips will enter and exit the development, then continue in their original direction of travel. The site trip distributions utilized in this analysis are summarized in **Table 6**.

**Table 6: Site Trip Distribution**

To/From	Via	Commercial		Commercial Pass-By	
		AM	PM	AM	PM
North	Latson Road	12%	7%	59% (NB)	45% (NB)
South	Latson Road	4%	4%	41% (SB)	55% (SB)
East	Grand River Avenue	8%	17%		
	I-96	26%	33%		
	Crooked Lake Road	1%	2%		
West	Grand River Avenue	8%	10%		
	I-96	41%	27%		
<b>Total</b>		<b>100%</b>	<b>100%</b>		

The vehicular traffic volumes shown in **Table 5** were distributed to the study network according to the distribution shown in **Table 6**. The site-generated trips shown on the attached **Figure 5** were added to the background peak hour traffic volumes shown on the attached **Figure 4**, in order to calculate the future peak hour traffic volumes with the addition of the proposed development, as shown on the attached **Figure 6**.

## 8 FUTURE CONDITIONS (2029)

### 8.1 INTERSECTION ANALYSIS

Future peak hour vehicle delays and LOS *with the proposed development* were calculated based on the proposed lane use and traffic control shown on the attached **Figure 2**, the proposed site access plan, the future peak hour traffic volumes shown on the attached **Figure 6**, and the methodologies presented in the HCM7. The results of the future conditions analysis are attached and summarized in **Table 7**.

The results of the future conditions analysis indicates that all study intersection approaches and movements will continue to operate acceptably, at LOS D or better during both peak periods, in a manner similar to the background conditions analysis, with increases in delays and the following additional impacts to LOS:

#### Latson Road & WB I-96 Ramps

- During the PM peak hour: The northbound left-turn movement is expected to operate at LOS F. Review of SimTraffic network simulations indicates that long vehicle queues were observed for the northbound left-turn movement, similar to those observations made during the background conditions analysis.

#### Latson Road & EB I-96 Ramps

- The Synchro intersection LOS analysis indicates acceptable operations during both peak periods. However, review of SimTraffic network simulations indicates that long vehicle queues were observed for the southbound left-turn movement, similar to those observations made during the background conditions analysis.

#### Latson Road & Beck Road

- During the AM and PM peak hours: The eastbound and westbound left-turn movements are expected to operate at LOS F.

Review of SimTraffic network simulations indicates generally acceptable operations throughout the study roadway network, during the AM peak hour; however, long vehicle queues were observed during the PM peak hour, which were present throughout the entire peak period.

**Table 7: Future Intersection Operations**

Intersection	Control	Approach	Background Conditions				Future Conditions				Difference			
			AM Peak		PM Peak		AM Peak		PM Peak		AM Peak		PM Peak	
			Delay (s/veh)	LOS	Delay (s/veh)	LOS	Delay (s/veh)	LOS	Delay (s/veh)	LOS	Delay (s/veh)	LOS	Delay (s/veh)	LOS
Latson Road & WB I-96 Ramps	Signal	WBL	37.9	D	27.0	C	37.4	D	27.9	C	-0.5	-	0.9	-
		WBR	34.9	C	33.8	C	33.1	C	33.6	C	-1.8	-	-0.2	-
		NBL	5.8	A	54.5	D	8.9	A	107.8	F	3.1	-	53.3	D→F
		NBT	0.4	A	0.6	A	0.4	A	0.7	A	0.0	-	0.1	-
		SBT	14.6	B	18.1	B	15.6	B	18.5	B	1.0	-	0.4	-
		SBR	15.1	B	21.4	C	15.8	B	21.4	C	0.7	-	0.0	-
		<b>Overall</b>	<b>12.1</b>	<b>B</b>	<b>19.0</b>	<b>B</b>	<b>12.6</b>	<b>B</b>	<b>24.4</b>	<b>C</b>	<b>0.5</b>	<b>-</b>	<b>5.4</b>	<b>B→C</b>
Latson Road & EB I-96 Ramps	Signal	EBL	29.3	C	32.9	C	27.1	C	32.8	C	-2.2	-	-0.1	-
		EBR	36.4	D	31.2	C	39.2	D	32.9	C	2.8	-	1.7	-
		NBT	7.5	A	6.6	A	8.6	A	6.9	A	1.1	-	0.3	-
		NBR	7.0	A	5.7	A	8.0	A	6.1	A	1.0	-	0.4	-
		SBL	7.0	A	11.6	B	11.3	B	19.1	B	4.3	A→B	7.5	-
		SBT	0.3	A	0.3	A	0.4	A	0.3	A	0.1	-	0.0	-
		<b>Overall</b>	<b>14.5</b>	<b>B</b>	<b>12.1</b>	<b>B</b>	<b>15.1</b>	<b>B</b>	<b>12.7</b>	<b>B</b>	<b>0.6</b>	<b>-</b>	<b>0.6</b>	<b>-</b>
Latson Road & Beck Road	Stop (Minor)	EBL	23.8	C	30.1	D	76.2	F	242.5	F	52.4	C→F	212.4	D→F
		EBTR	0.0*	A	9.0	A	0.0*	A	8.8	A	0.0*	-	-0.2	-
		WBL	0.0*	A	0.0*	A	60.8	F	715.3	F	60.8	A→F	715.3	A→F
		WBTR	10.0	B	11.8	B	12.5	B	17.7	C	2.5	-	5.9	B→C
		NBL	0.0*	A	0.0*	A	0.0*	A	0.0*	A	0.0*	-	0.0*	-
		SBL	8.8	A	10.0	B	9.6	A	12.0	B	0.8	-	2.0	-

\* Indicates no vehicle volume present *Note:* Decreased delays are the result of improved progression and/or HCM weighting methodologies.

## 8.2 FUTURE IMPROVEMENTS

In order to improve traffic operations to a LOS D or better for all intersection approaches and movements under future conditions, mitigation measures were investigated. These mitigation measures included signal timing adjustments, geometric improvements, and traffic control modifications. The proposed improvements and their impact to intersection operations are summarized below.

The mitigation measures that were identified for the **Background (No Build) conditions** was evaluated with the projected future traffic volumes. The future intersection operations with the improvements identified under the background conditions analysis were determined to operate well, and no further mitigation measures are recommended at the Latson Road & I-96 EB/WB Ramps intersections.

### Latson Road & Beck Road

A signal warrant analysis was performed at the study intersections of Latson Road & Beck Road. The *Michigan Manual on Uniform traffic Control Devices (MMUTCD)* documents eight warrants by which traffic signal control may or should be considered. Warrant 2 (4-Hour Vehicular Volume) was evaluated for the study intersection, based on the future traffic volumes. The results of the signal warrant analyses are discussed below and summarized in **Table 8**; the signal warrant charts are attached for reference.

The results of the signal warrant analysis indicates that the study intersection of Latson Road & Beck Road is expected to meet the Warrant 2 (Four-Hour).

**Table 8: Signal Warrant Analysis Summary**

Intersection	Signal Warrants		Future Conditions
Latson Road & Beck Road	Warrant 2: Four-Hour	Hours Met	4
		Warrant Met	YES

**8.3 SUMMARY**

The following potential mitigations were evaluated with the addition of the ICPUD. These were identified based upon the projected background and the potential land uses evaluated. Further evaluation should be performed when known end users are proposed, in order to determine if/when these mitigations should be implemented.

**Latson Road & Beck Road**

- Intersection signalization  
*No geometric improvements are necessary along Beck Road, as the existing approaches currently provide adequate paved left-turn lane storage, in order to accommodate the anticipated traffic volumes. The traffic signal should be designed to accommodate future pedestrian connectivity on Latson Road.*

**Latson Road & WB I-96 Ramps**

- Provide permissive/protected northbound left-turn phasing with vehicle detection.
- Increase the network-wide cycle length to 90-seconds for all signals along the Latson Road corridor.

**Latson Road & EB I-96 Ramps**

- Provide permissive/protected southbound left-turn phasing with vehicle detection.
- Increase the network-wide cycle length to 90-seconds for all signals along the Latson Road corridor.

The results of the future conditions with improvements analysis are attached and summarized in **Table 9**.

**Table 9: Future Intersection Operations with Improvements**

Intersection	Control	Approach	Future Conditions				Future w/ IMP				Difference			
			AM Peak		PM Peak		AM Peak		PM Peak		AM Peak		PM Peak	
			Delay (s/veh)	LOS	Delay (s/veh)	LOS	Delay (s/veh)	LOS	Delay (s/veh)	LOS	Delay (s/veh)	LOS	Delay (s/veh)	LOS
Latson Road & WB I-96 Ramps	Signal	WBL	37.4	D	27.9	C	43.1	D	32.9	C	5.7	-	5.0	-
		WBR	33.1	C	33.6	C	37.8	D	49.2	D	4.7	C→D	15.6	C→D
		NBL	8.9	A	107.8	F	5.5	A	8.7	A	-3.4	-	-99.1	F→A
		NBT	0.4	A	0.7	A	0.4	A	0.5	A	0.0	-	-0.2	-
		SBT	15.6	B	18.5	B	0.5	A	2.4	A	-15.1	B→A	-16.1	B→A
		SBR	15.8	B	21.4	C	1.0	A	4.5	A	-14.8	B→A	-16.9	C→A
		<b>Overall</b>	<b>12.6</b>	<b>B</b>	<b>24.4</b>	<b>C</b>	<b>6.9</b>	<b>A</b>	<b>11.2</b>	<b>B</b>	<b>-5.7</b>	<b>B→A</b>	<b>-13.2</b>	<b>C→B</b>
Latson Road & EB I-96 Ramps	Signal	EBL	27.1	C	32.8	C	31.6	C	39.9	D	4.5	-	7.1	C→D
		EBR	39.2	D	32.9	C	49.1	D	38.9	D	9.9	-	6.0	C→D
		NBT	8.6	A	6.9	A	7.3	A	0.8	A	-1.3	-	-6.1	-
		NBR	8.0	A	6.1	A	7.0	A	0.7	A	-1.0	-	-5.4	-
		SBL	11.3	B	19.1	B	10.3	B	7.2	A	-1.0	-	-11.9	B→A
		SBT	0.4	A	0.3	A	0.3	A	0.3	A	-0.1	-	0.0	-
		<b>Overall</b>	<b>15.1</b>	<b>B</b>	<b>12.7</b>	<b>B</b>	<b>17.0</b>	<b>B</b>	<b>10.8</b>	<b>B</b>	<b>1.9</b>	<b>-</b>	<b>-1.9</b>	<b>-</b>

Intersection	Control	Approach	Future Conditions				Future w/ IMP				Difference			
			AM Peak		PM Peak		AM Peak		PM Peak		AM Peak		PM Peak	
			Delay (s/veh)	LOS	Delay (s/veh)	LOS	Delay (s/veh)	LOS	Delay (s/veh)	LOS	Delay (s/veh)	LOS	Delay (s/veh)	LOS
Latson Road & Beck Road	Stop (Minor)	EBL	76.2	F	242.5	F	42.7	D	42.6	D	-33.5	E→D	-199.9	F→D
		EBTR	0.0*	A	8.8	A	0.0*	A	28.7	C	0.0*	-	19.9	A→C
		WBL	60.8	F	715.3	F	32.0	C	32.0	C	-28.8	F→C	-683.3	F→C
		WBTR	12.5	B	17.7	C	40.7	D	41.9	D	28.2	B→D	24.1	C→D
	Signal [IMP]	NBL	0.0*	A	0.0*	A	0.0*	A	0.0*	A	0.0*	-	0.0*	-
		[NBTR]	Free				0.5	A	2.0	A	N/A			
		SBL	9.6	A	12.0	B	1.0	A	8.1	A	-8.6	-	-3.9	B→A
		[SBTR]	Free				0.8	A	2.7	A	N/A			
		[Overall]	N/A				7.9	A	9.5	A	N/A			

\* Indicates no vehicle volume present

The results indicates that all approaches and movements at the study intersection are expected to operate at LOS D or better during both the AM and PM peak hours. Review of SimTraffic network simulations indicates acceptable operations during both peak periods, with improved delays and significantly reduced vehicle queues throughout the study roadway network.

#### 8.4 POTENTIAL RAILROAD CONFLICT EVALUATION (BECK ROAD)

The existing Beck Road intersection is located approximately 340-feet north of the railroad tracks, with an effective northbound queue length of 240-feet. The identified mitigation measures included the recommendation to install a fully actuated and coordinated traffic signal at the study intersection of Latson Road & Beck Road; therefore, the intersection was further evaluated, in order to ensure that the future intersection operations, with the implementation of the recommended improvements, will not impact the railroad tracks. The results of the analysis are summarized below in **Table 10**.

**Table 10: Queue Length Summary (Future IMP)**

Intersection	Approach	AM Peak Hour		PM Peak Hour		Available Queue Length (ft)	Exceeds Queue Length
		Average Queue (ft)	95% Queue (ft)	Average Queue (ft)	95% Queue (ft)		
Latson Road & Beck Road	NBL	0	0	0	0	240	No
	NBT	9	30	98	186	240	No
	NBTR	24	64	121	206	240	No

Key findings from this evaluation:

- The existing Beck Road location has adequate distance from the influence area of the railroad tracks to accommodate the projected northbound queue lengths along Latson Road.
- The recommended improvements include signalization. This signal should include communication and pre-emption with the railroad crossing operations.

## 9 CONCLUSIONS

*The conclusions of this TIS are as follows:*

### 9.1 EXISTING CONDITIONS (2024)

- The results of the existing conditions analysis indicates that all approaches and movements at the study intersections currently operate acceptably, at LOS D or better during both the AM and PM peak hours.
- Review of the SimTraffic network simulations indicates acceptable traffic operations throughout the study roadway network during both peak periods.

### 9.2 BACKGROUND GROWTH

- An annual background growth rate of **0.72%** per year was utilized to project the collected 2023 traffic volumes to the existing year of 2024 and the buildout year of 2029.
- In addition to background traffic growth, the following background developments were identified and were included within the background traffic volumes:
  - St. Joseph Mercy Health Center Expansion
  - Latson Road PUD

### 9.3 BACKGROUND CONDITIONS (2029)

- The results of the background conditions analysis indicates that all approaches and movements at the study intersections are expected to continue operating acceptably, at LOS D or better during both peak periods, in a manner similar to the existing conditions analysis, with some minor increases in delays.
- Review of SimTraffic microsimulations indicates generally acceptable operations, throughout the study roadway network during the AM peak hour; however, during the PM peak hour, long vehicle queues are present for the left-turn movements along Latson Road at both of the I-96 Freeway Ramps.
  - The delays and queueing along Latson Road at the I-96 Freeway Ramps are the result of the background developments and expected growth throughout the study area; these vehicle queues were not observed to dissipate and were typically present throughout the peak hour.

### 9.4 BACKGROUND IMPROVEMENTS

- Mitigation measures were identified and were determined to adequately mitigate the projected background vehicle queue lengths at the study intersection. As developments progress throughout the area, the following mitigation measures were identified and may be necessary to accommodate the background growth and future development plans; these should be evaluated as part of the site plan approval and permitting process.

#### **Latson Road & WB I-96 Ramps**

- Provide permissive/protected northbound left-turn phasing with vehicle detection.
- Increase the network-wide cycle length to 90-seconds for all signals along the Latson Road corridor.

#### **Latson Road & EB I-96 Ramps**

- Provide permissive/protected southbound left-turn phasing with vehicle detection.
- Increase the network-wide cycle length to 90-seconds for all signals along the Latson Road corridor.



## 9.5 FUTURE CONDITIONS (2029)

- The results of the future conditions analysis indicates that all study intersection approaches and movements will continue to operate acceptably, at LOS D or better during both peak periods, in a manner similar to the background conditions analysis, with increases in delays and the following additional impacts to LOS:

### Latson Road & WB I-96 Ramps

- During the PM peak hour: The northbound left-turn movement is expected to operate at LOS F. Review of SimTraffic network simulations indicates that long vehicle queues were observed for the northbound left-turn movement, similar to the background conditions analysis observations.

### Latson Road & EB I-96 Ramps

- The Synchro intersection LOS analysis indicates acceptable operations during both peak periods. However, review of SimTraffic network simulations indicates that long vehicle queues were observed for the southbound left-turn movement, similar to background conditions.

### Latson Road & Beck Road

- During the AM and PM peak hours: The eastbound and westbound left-turn movements are expected to operate at LOS F.

Review of SimTraffic network simulations indicates generally acceptable operations during the AM peak hour; however, long vehicle queues were observed during the PM peak hour and were present throughout the entire peak period.

## 9.6 FUTURE IMPROVEMENTS

- The mitigation measures identified for the Background (No Build) conditions were evaluated with the projected future traffic volumes. The future intersection operations with the improvements identified under the background conditions analysis were determined to operate well, and no further mitigation measures are recommended at the Latson Road & I-96 EB/WB Ramps intersections.

### Latson Road & Beck Road

- Provide intersection signalization. No geometry improvements are necessary along Beck Road, as the existing approaches currently provide adequate paved left-turn lane storage, in order to accommodate the anticipated traffic volumes. The traffic signal should be designed to accommodate future pedestrian connectivity on Latson Road.

### 10 RECOMMENDATIONS

The following potential mitigations were evaluated with the addition of the ICPUD. These were identified based upon the projected background conditions and the potential land uses evaluated in this study. Further evaluation should be performed when known end users are proposed, in order to determine if/when these mitigation measures should be implemented.

Recommended Mitigation Measures	Existing 2024	Background 2029	Future 2029
<b>Latson Road &amp; WB I-96 Ramps</b>			
<ul style="list-style-type: none"> <li>Provide permissive/protected northbound left-turn phasing with vehicle detection</li> </ul>		✓	
<ul style="list-style-type: none"> <li>Increase the network-wide cycle length to 90-seconds for all signals along the Latson Road corridor</li> </ul>		✓	
<b>Latson Road &amp; EB I-96 Ramps</b>			
<ul style="list-style-type: none"> <li>Provide permissive/protected southbound left-turn phasing with vehicle detection</li> </ul>		✓	
<ul style="list-style-type: none"> <li>Increase the network-wide cycle length to 90-seconds for all signals along the Latson Road corridor</li> </ul>		✓	
<b>Latson Road &amp; Beck Road</b>			
<ul style="list-style-type: none"> <li>Install a fully actuated and coordinated traffic signal with permissive/protected southbound left-turn phasing</li> </ul>			✓

Any questions related to this memorandum, study, analysis, and results should be addressed to Fleis & VandenBrink.



I hereby certify that this engineering document was prepared by me or under my direct personal supervision and that I am a duly licensed Professional Engineer under the laws of the State of Michigan.

*Julie M. Kroll*

**Julie M. Kroll**  
 2024.08.23 16:42:53  
 -04'00'

- Attachments:** Figures 1 – 6  
 Traffic Volume Data  
 SEMCOG Data  
 Signal Timing Permit  
 Background Growth & Background Development Data  
 Synchro / SimTraffic Results  
 Signal Warrants

# Interchange Commercial Concept PUD

August 27, 2024

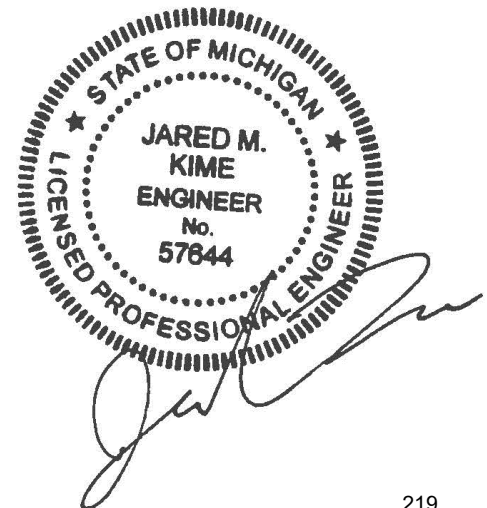


Prepared By:



## Sheet Index:

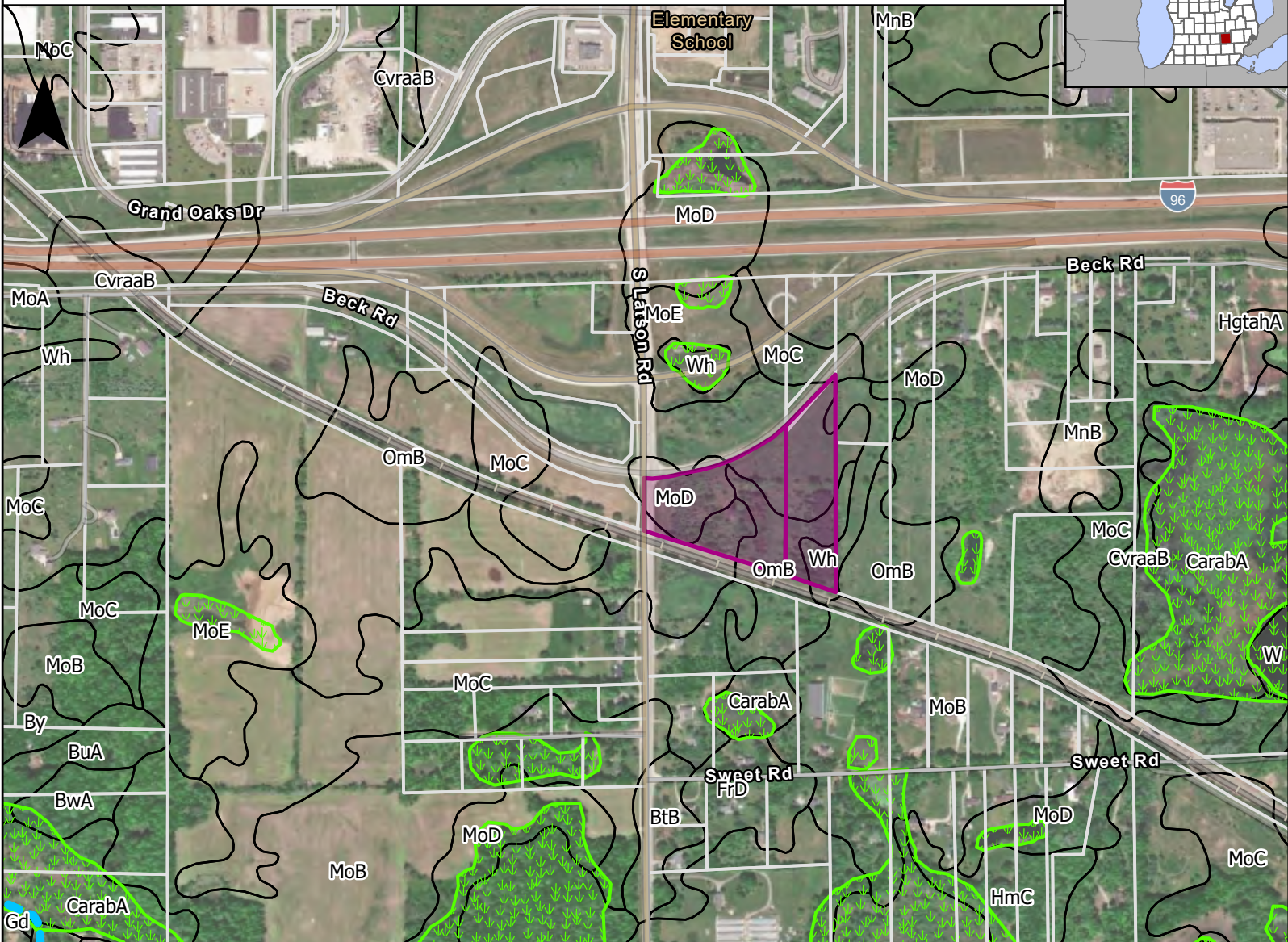
- Cover Page
- Soils and Wetlands Site Map
- Topography and Natural Features Site Map
- PUD Concept Plan
- Water Main Concept Map
- Sanitary Sewer Concept Map



# Interchange Commercial PUD

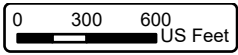
Site Map - Soils and Wetlands

Genoa Township, Livingston County, Michigan



Symbol	Name
BtB	Boyer-Oshtemo loamy sands, 2 to 6 percent slopes
BuA	Brady loamy sand, 0 to 2 percent slopes
FoB	Fox sandy loam, 2 to 6 percent slopes
Gd	Gilford sandy loam, 0 to 2 percent slopes, gravelly subsoil
MnB	Metea loamy sand, 2 to 6 percent slopes
MoB	Wawasee loam, 2 to 6 percent slopes
MoC	Wawasee loam, 6 to 12 percent slopes
MoD	Miami loam, 12 to 18 percent slopes
MoE	Miami loam, 18 to 25 percent slopes
OmB	Owosso-Miami sandy loams, 2 to 6 percent slopes
Wh	Washtenaw silt loam
CvraaB	Conover loam, 0 to 4 percent slopes
CarabA	Carlisle muck, 0 to 2 percent slopes

Interchange Commercial PUD  
 Parcel Boundary  
 NWI Wetlands  
 Soil Types

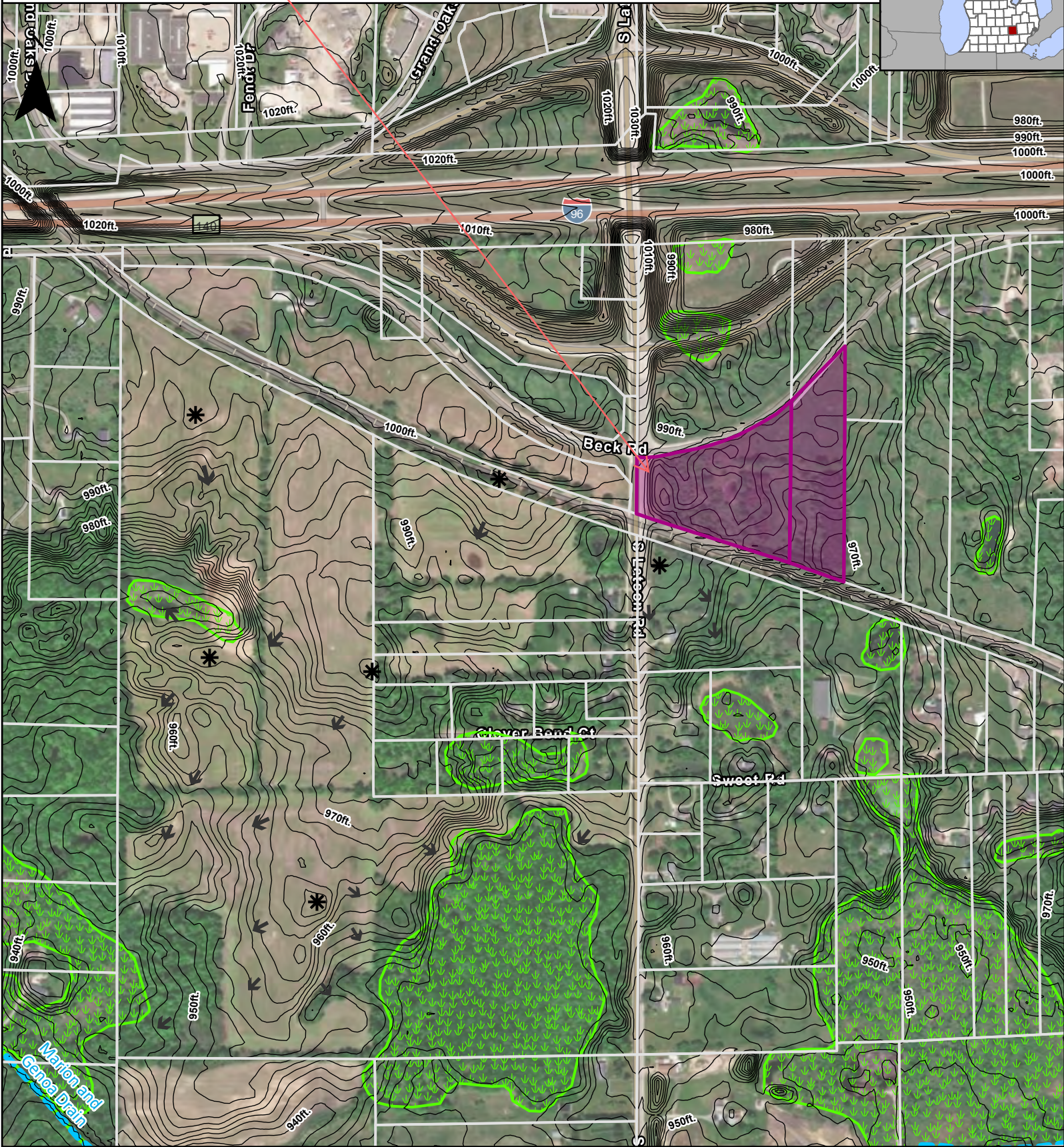


Does the ownership of this property extend to the center of the road?

# Interchange Commercial PUD

## Topography & Natural Features

Genoa Township, Livingston County, Michigan



Interchange Commercial PUD

Parcel Boundary

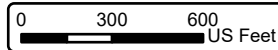
Local High Point

NWI Wetlands

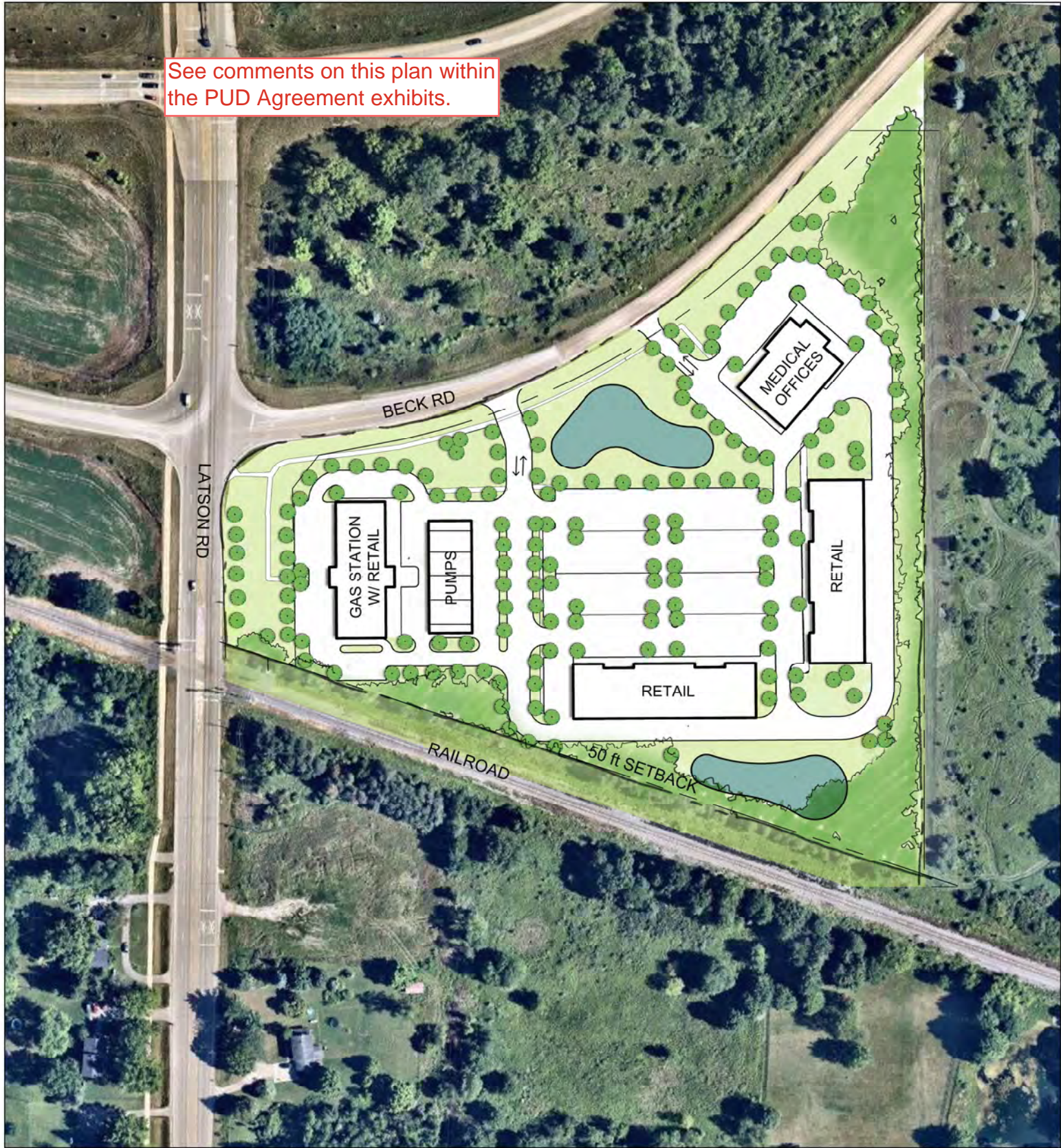
2 Ft Contour

Estimated Flow Direction

Watercourse NHD



See comments on this plan within the PUD Agreement exhibits.



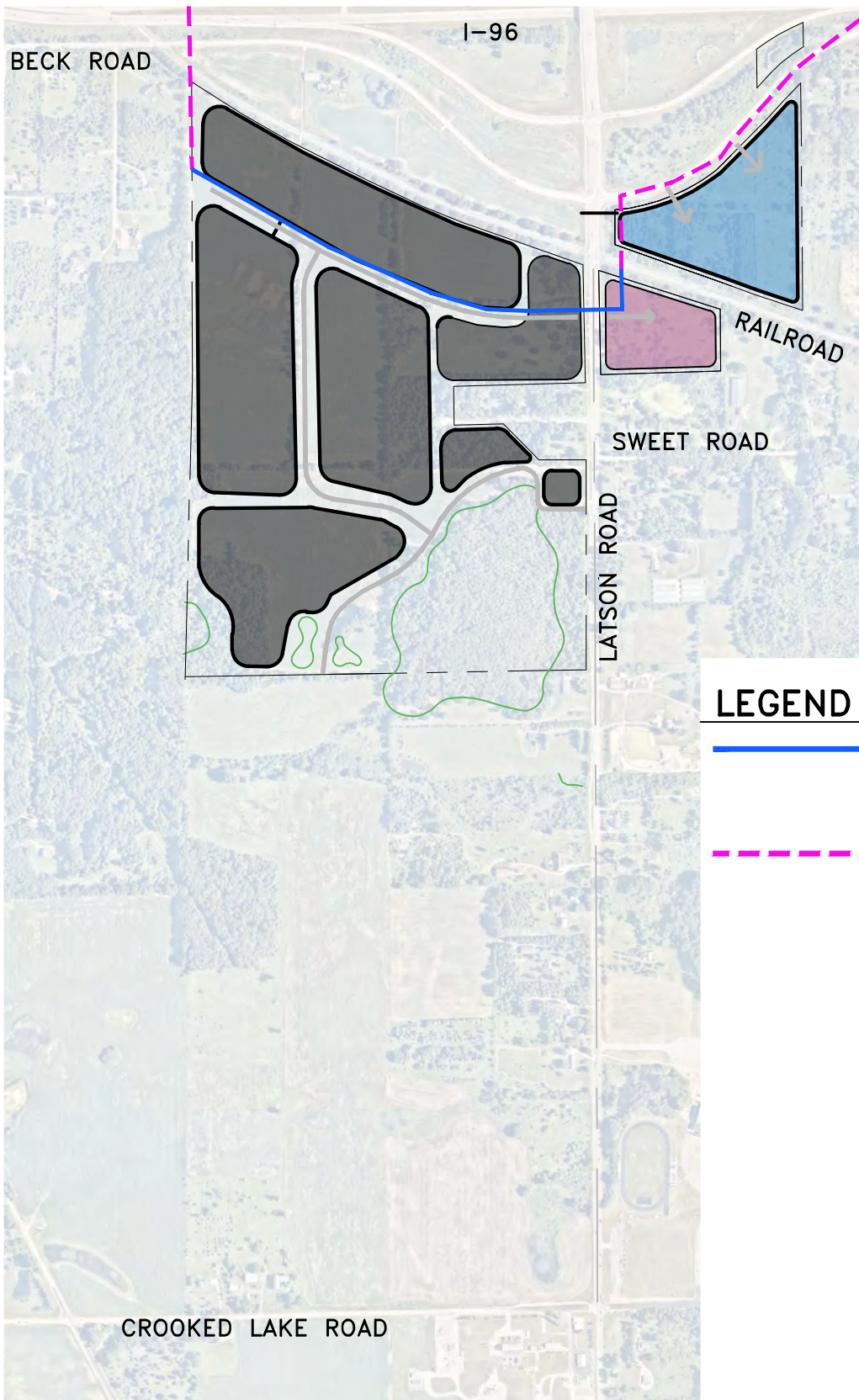
# MKSK

Landscape Architecture  
Urban Design  
Planning

462 SOUTH LUDLOW ALLEY  
COLUMBUS, OH 43215  
614.621.2796 MKSKSTUDIOS.COM

Drawing Title: <b>SITE PLAN</b>	Project #	d23103
	Date:	06.07.2024
Project: <b>COMMERCIAL PUD</b>	Scale:	1 to 200





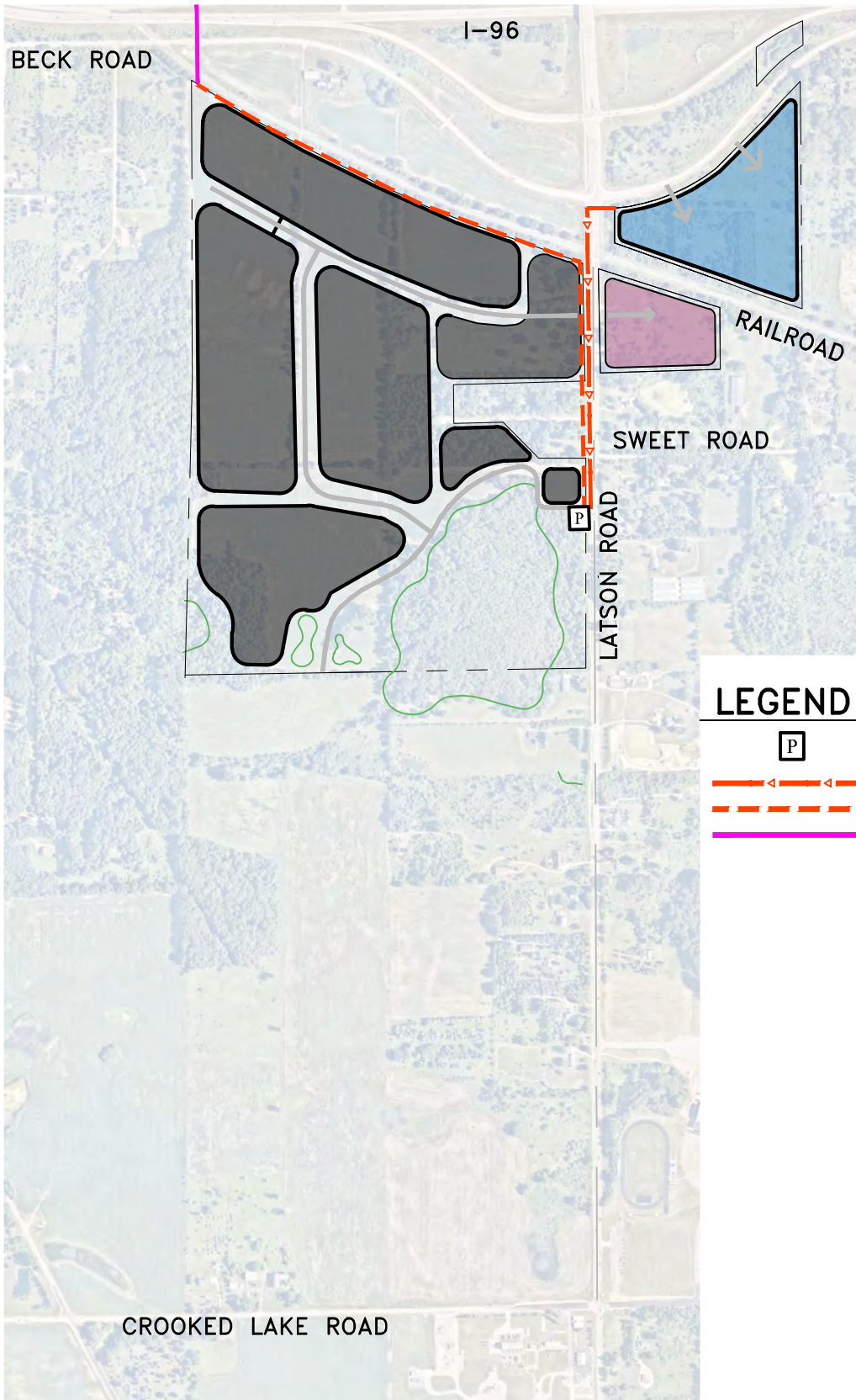
**LEGEND**

- PR WATERMAIN  
(TO BE INSTALLED  
AS PART OF THE  
LATSON ROAD PUD)
- - - - - EX WATERMAIN





**WATERMAIN CONCEPT**

NOTE:  
WATER MAIN TO BE A COMBINATION OF 8" AND  
12" IN DIAMETER

PROJECT NO.: 16001784  
DATE: 2024-01-04



**LEGEND**

-  PUMP STATION
-  PR GRAVITY SEWER
-  PR FORCE MAIN
-  EX 8" FORCE MAIN

**SANITARY SEWER CONCEPT**

PROJECT NO.: 16001784  
 DATE: 2024-01-04

NOTE:  
 SANITARY SEWER TO BE A COMBINATION OF 8"  
 AND 10" GRAVITY SEWER AND 8" FORCE MAIN



# Interchange Commercial PUD


## Parcel Exhibit Map

Genoa Township, Livingston County, Michigan



Verify accuracy of property lines. Do they own to the center of Latson Road?



 Interchange Commercial PUD

